



**777 LAUREL STREET, CAMBRIDGE ONTARIO
PUBLIC MEETING
OFFICIAL PLAN & ZONING BY-LAW AMENDMENT**

September 5, 2023

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PROJECT TEAM

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PROJECT LOCATION

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LAND USE CONTEXT

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PROPOSED DEVELOPMENT

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CONCLUSION



Figure 1
Location Map

LEGEND

 Subject Lands

Date: June 2023

Scale: 1:5,000

File: 16222N

Drawn: PL



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777 Laurel St and 308 Dolph St N
City of Cambridge
Region of Waterloo

Source: Region of Waterloo 2022 Satellite Imagery



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE



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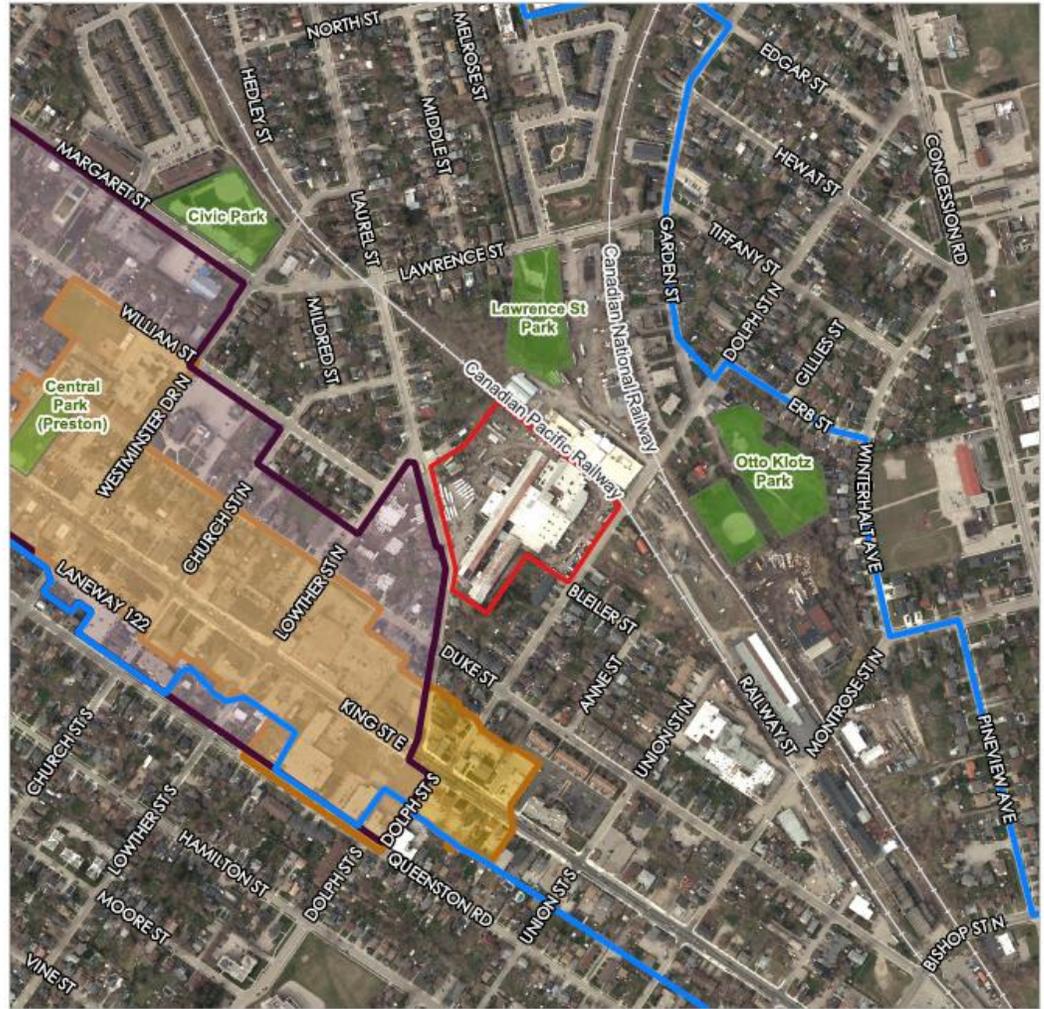


Figure 3
Site Context Plan

LEGEND	
	Subject Lands
	Planned Preston Major Transit Station Area
	Community Core Area (Preston Towne Centre)
	Regeneration Area

Date:	June 2023
Scale:	1:6,000
File:	16222N
Drawn:	PL



777 Laurel St and 306 Dolph St N
City of Cambridge
Region of Waterloo

Notes:
- Source: Region of Waterloo 2022 Satellite Imagery
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- Apartment buildings – 7 to 14 storeys
- Structured parking, with limited surface parking
- Shared indoor and outdoor amenity spaces
- One access from Laurel and one access from Dolph
- 1,046 units (one, two and three bedroom units) and 1,046 parking spaces



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View from Laurel Street

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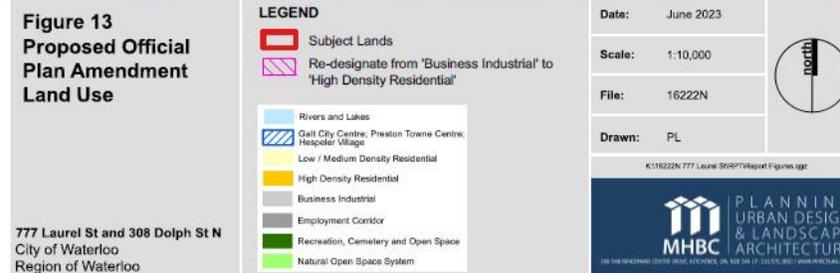
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OFFICIAL PLAN AMENDMENT

- Lands located within a **Regeneration Area**
- **Re-designation** from Business Industrial **to High Density Residential**
- **Apply Site Specific Special Policy Area** to permit a maximum height of 14 storeys and a maximum density of 3.0 Floor Space Index (FSI)
- Location
 - Transit (planned and existing)
 - Walkable (Preston Core)
 - Range of commercial, institutional and open space uses in area
 - Area intended to transition away from industrial uses
 - Redevelopment of industrial property with residential uses

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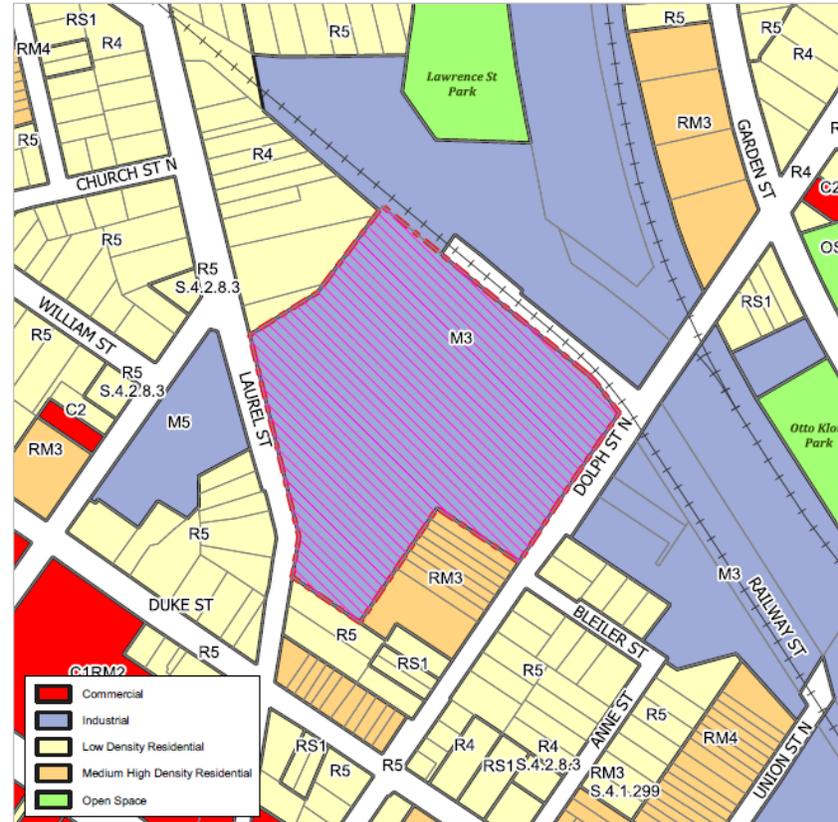


Figure 16
Proposed Zoning
By-law Amendment

LEGEND

- Subject Lands
- Rezoned from M3 to RM1 and establish new Site-Specific Exception (4.2.XX) to:
 - o Permit a maximum building height of 7 storeys within 10 metres of Laurel Street, 12 storeys within 10 metres of Dolph Street North, and 14 storeys on the balance of the lands
 - o Permit a maximum density of 3.0 Floor Space Index
 - o Permit a minimum amenity area requirement of 15m² per unit
 - o Permit a minimum parking requirement of 1.0 spaces per unit, being 0.9 spaces per dwelling unit and 0.1 spaces for visitors

777 Laurel St and 308 Dolph St N
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Region of Waterloo

Date: June 2023
Scale: 1:2,500
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MHBC PLANNING URBAN DESIGN & LANDSCAPE ARCHITECTURE

ZONING BY-LAW AMENDMENT

- Currently Zoned Business Industrial (M3)
- Rezone the lands to the RM1 zone with site specific provisions to permit residential uses
- Site specific provisions:
 - Permit a maximum building height of 7 storeys within 10 metres of Laurel Street
 - Permit a maximum building height of 12 storeys within 10 metres of Dolph Street and a maximum height of 14 storeys on the balance of the lands.
 - Permit a maximum density of 3.0 Floor Space Index
 - Permit a minimum amenity area of 15 square metres per unit
 - Permit a minimum parking requirement of 0.9 spaces per unit plus 0.1 spaces per unit for visitors

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Noise and Vibration Feasibility Study

- Report concludes that the development is subject to noise from road and stationary sources, and **recommends applicable Noise Warning Clauses**
- No specific mitigation required for institutional, industrial, or commercial uses in the area

Functional Servicing Report

- **Municipal services available** to support the development
- Grading designed to match external grades around property
- Stormwater controlled with underground system

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Traffic Impact Study

- Most intersections are expected to operate with a Level of Service of "C" or better, without capacity or operational issues
- Laurel Street/Duke Street intersection expected to operate with a Level of Service of "D" (P.M. peak hour), given its location between King Street and site access at Laurel Street
 - Recommends conversion of intersection to an all-way stop controlled intersection, ultimately improving to Level of Service "B"
- Study concludes development can be supported from a transportation perspective

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Parking Study

- Applying peak parking rates for a similar land use to proposed development resulted in significant surplus of 283 parking spaces
(as per Institute of Transportation Engineers Parking Generation – ParkGen 5th Ed.)
- Proposed lower parking rate supported by viable transportation alternatives (existing GRT Routes and planned ION light rail)
- **Recommended Transportation Demand Management (TDM) measures:**
 - Provision of pedestrian and cyclist facilities
 - Transit incentives
 - Information package for new/prospective residents – available alternative travelling options

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OVERALL, THE PROPOSED DEVELOPMENT:

- **Is consistent with the Provincial Policy Statement**
- **Conforms to the Growth Plan**
- **Conforms to the Regional Official Plan**
- **Represents an opportunity to contribute towards a 15-minute neighbourhood, with transit, shopping, jobs, schools, and close proximity to the subject lands**
- **Provides for a range of residential unit types and sizes, three bedroom units within an area planned for growth that is served by commercial uses, transit and active transportation**
- **Provides for substantial setbacks from streets to allow for separation, transition and enhanced landscaping**
- **Is transit supportive, with a mix of land uses in close proximity existing GRT bus routes and stops, and approximately 900 from the planned Preston ION Transit Station**