



# 777 LAUREL STREET, CAMBRIDGE ONTARIO PUBLIC MEETING OFFICIAL PLAN & ZONING BY-LAW AMENDMENT

September 5, 2023



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2 PROJECT LOCATION

3 LAND USE CONTEXT

4 PROPOSED DEVELOPMENT

5 PLANNING POLICY

6 ZONING AMENDMENT


7 SUPPORTING STUDIES

8 CONCLUSION



Figure 1  
Location Map

LEGEND

 Subject Lands

Date: June 2023

Scale: 1:5,000

File: 16222N

Drawn: PL



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777 Laurel St and 308 Dolph St N  
City of Cambridge  
Region of Waterloo

Source: Region of Waterloo 2022 Satellite Imagery



PLANNING  
URBAN DESIGN  
& LANDSCAPE  
ARCHITECTURE

MHBC  
PLANNING  
URBAN DESIGN  
& LANDSCAPE  
ARCHITECTURE

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ANNIVERSARY



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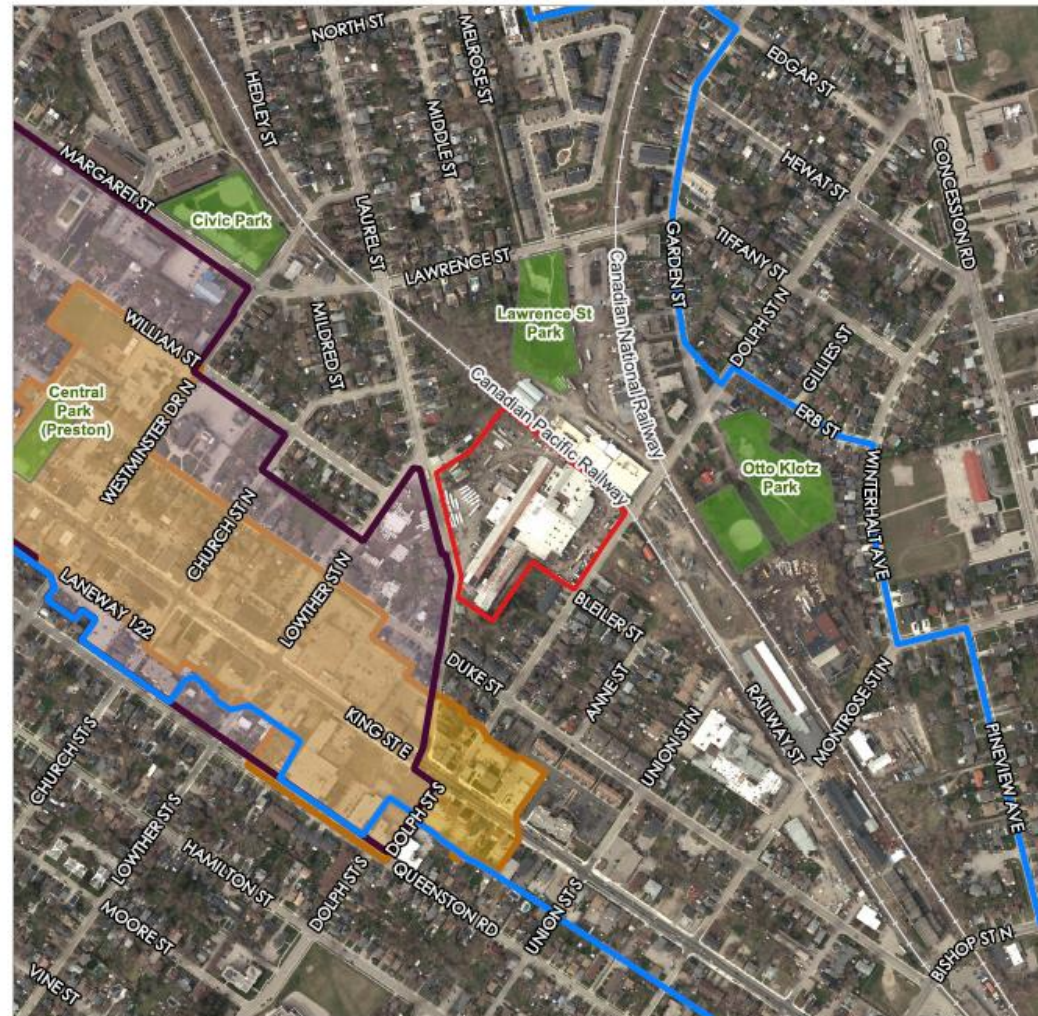
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**Figure 3**  
**Site Context Plan**

**LEGEND**

- Subject Lands
- Planned Preston Major Transit Station Area
- Community Core Area (Preston Towne Centre)
- Regeneration Area

Date: June 2023

Scale: 1:6,000

File: 16222N

Drawn: PL



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777 Laurel St and 306 Dolph St N  
City of Cambridge  
Region of Waterloo

Notes:  
- Source: Region of Waterloo 2022 Satellite Imagery  
- Contains information licensed under the Open Government Licence - City of Cambridge & Region of Waterloo



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- Apartment buildings – 7 to 14 storeys
- Structured parking, with limited surface parking
- Shared indoor and outdoor amenity spaces
- One access from Laurel and one access from Dolph
- 1,046 units (one, two and three bedroom units) and 1,046 parking spaces





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## OFFICIAL PLAN AMENDMENT

- Lands located within a **Regeneration Area**
- Re-designation** from Business Industrial **to High Density Residential**
- Apply Site Specific Special Policy Area** to permit a maximum height of 14 storeys and a maximum density of 3.0 Floor Space Index (FSI)
- Location
  - Transit (planned and existing)
  - Walkable (Preston Core)
  - Range of commercial, institutional and open space uses in area
  - Area intended to transition away from industrial uses
  - Redevelopment of industrial property with residential uses



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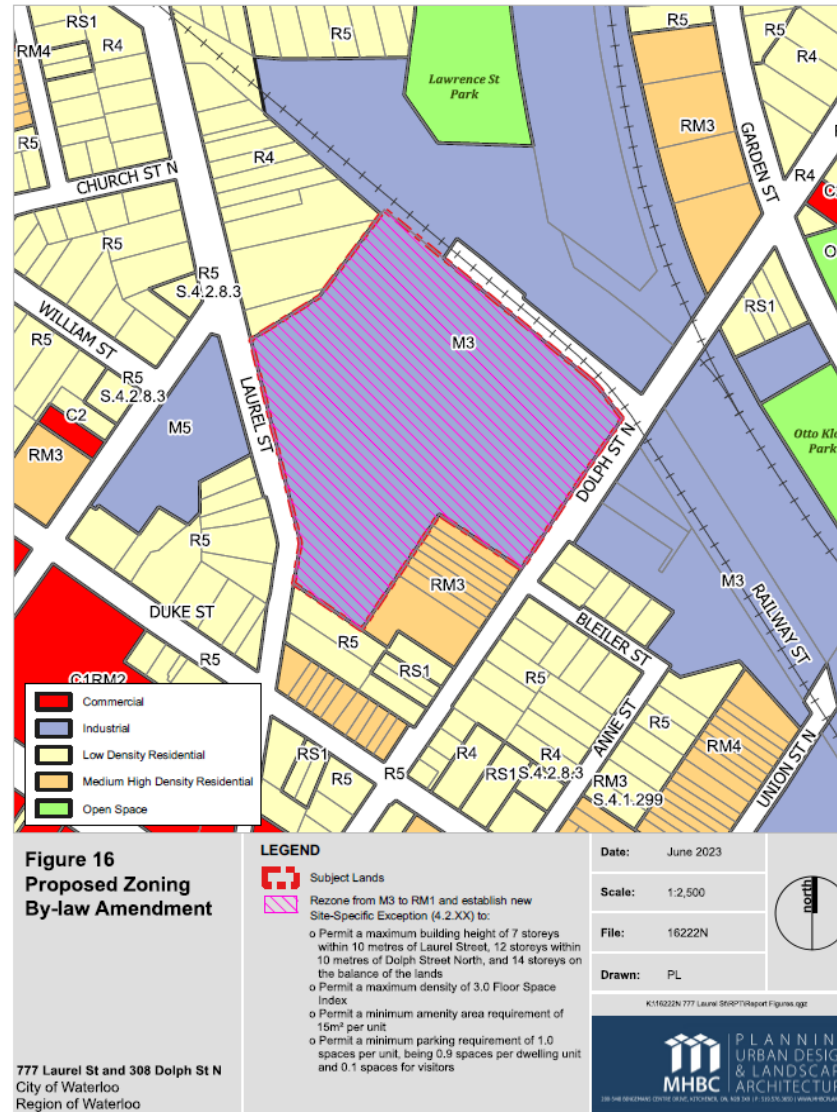
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## ZONING BY-LAW AMENDMENT

- Currently Zoned Business Industrial (M3)
- Rezone the lands to the RM1 zone with site specific provisions to permit residential uses
- Site specific provisions:
  - Permit a maximum building height of 7 storeys within 10 metres of Laurel Street
  - Permit a maximum building height of 12 storeys within 10 metres of Dolph Street and a maximum height of 14 storeys on the balance of the lands.
  - Permit a maximum density of 3.0 Floor Space Index
  - Permit a minimum amenity area of 15 square metres per unit
  - Permit a minimum parking requirement of 0.9 spaces per unit plus 0.1 spaces per unit for visitors



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## Noise and Vibration Feasibility Study

- Report concludes that the development is subject to noise from road and stationary sources, and **recommends applicable Noise Warning Clauses**
- No specific mitigation required for institutional, industrial, or commercial uses in the area

## Functional Servicing Report

- **Municipal services available** to support the development
- Grading designed to match external grades around property
- Stormwater controlled with underground system



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## Traffic Impact Study

- Most intersections are expected to operate with a Level of Service of "C" or better, without capacity or operational issues
- Laurel Street/Duke Street intersection expected to operate with a Level of Service of "D" (P.M. peak hour), given its location between King Street and site access at Laurel Street
  - Recommends conversion of intersection to an all-way stop controlled intersection, ultimately improving to Level of Service "B"
- Study concludes development can be supported from a transportation perspective



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## Parking Study

- Applying peak parking rates for a similar land use to proposed development resulted in significant surplus of 283 parking spaces  
*(as per Institute of Transportation Engineers Parking Generation – ParkGen 5<sup>th</sup> Ed.)*
- Proposed lower parking rate supported by viable transportation alternatives (existing GRT Routes and planned ION light rail)
- **Recommended Transportation Demand Management (TDM) measures:**
  - Provision of pedestrian and cyclist facilities
  - Transit incentives
  - Information package for new/prospective residents – available alternative travelling options



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## OVERALL, THE PROPOSED DEVELOPMENT:

- **Is consistent with the Provincial Policy Statement**
- **Conforms to the Growth Plan**
- **Conforms to the Regional Official Plan**
- **Represents an opportunity to contribute towards a 15-minute neighbourhood, with transit, shopping, jobs, schools, and close proximity to the subject lands**
- **Provides for a range of residential unit types and sizes, three bedroom units within an area planned for growth that is served by commercial uses, transit and active transportation**
- **Provides for substantial setbacks from streets to allow for separation, transition and enhanced landscaping**
- **Is transit supportive, with a mix of land uses in close proximity existing GRT bus routes and stops, and approximately 900 from the planned Preston ION Transit Station**