

**To:** **COUNCIL**

**Meeting Date:** **07/27/2021**

**Subject:** **City Initiated River Rd Secondary Plan and Zoning By-law Amendment – Recommendation Report**

**Submitted By:** **Deanne Friess, Manager of Planning Policy, MCIP, RPP**

**Prepared By:** **Bryan Cooper, Senior Policy Planner MCIP, RPP**

**Report No.:** **21-165(CD)**

**File No.:** **OR04/21**

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## Recommendations

THAT report 21-165(CD) - City Initiated River Rd Secondary Plan and Zoning By-law Amendment – Recommendation Report - be received for information;

AND THAT Cambridge Council recommends approval of Official Plan Amendment No. 47 (River Road Secondary Plan) to the City of Cambridge Official Plan;

AND THAT Cambridge Council amends the Zoning By-law (Attachment No. 2 – Recommended Zoning By-law Amendment) for the River Rd. Neighbourhood;

AND THAT Cambridge Council amends the site plan control by-law to include properties fronting on River Rd;

AND THAT Cambridge Council direct staff to consider the interim road improvements to River Rd. through the next available capital budget process;

AND THAT Council direct staff to initiate a cultural heritage landscape study of the River Rd. area in 2022;

AND THAT Cambridge Council is satisfied that a subsequent public meeting in accordance with subsection 34(17) of the Planning Act is not required;

AND FURTHER THAT the attached By-laws are passed.

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## Executive Summary

### Purpose

- This report recommends approval of City initiated amendments to the Official Plan and Zoning By-law for the River Rd. neighbourhood. The Official Plan Amendment is in the form of a formal secondary plan.
- The purpose of the amendments is to provide updated land use policy and zoning regulations to guide new development in this area of the City.

### Key Findings

- A new secondary plan and zoning by-law amendment is proposed for the River Rd. neighbourhood.
- Servicing (sanitary, water, storm water) and improvements (pedestrian/cycling facilities) to the River Rd. road allowance are recommended.
- The City's site plan control by-law is recommended to be amended to apply to new development (single detached, semi-detached and triplex dwellings) along River Rd.
- River Rd. may qualify as a cultural heritage landscape (CHL) area. It is recommended that the City initiate a CHL study in 2022.
- Two draft zoning by-law amendments have been prepared for Council consideration – Option 1 – Recommended Zoning By-law Amendment (Attachment No. 2, permitting single detached, semi-detached and triplex dwellings along River Rd) and Option 2 – Alternative Zoning By-law Amendment (Attachment No. 3), restricting River Rd. to single detached dwellings only).
- Staff is recommending that Council enact Option 1 – Recommended Zoning By-law Amendment (Attachment No. 2).

### Financial Implications

- The full reconstruction and servicing of River Rd is estimated to be \$6,060,000.
- An interim improvement to River Rd. which would include a pedestrian sidewalk and swales for stormwater management but not sanitary and water service is estimated to be \$1,560,000. Staff is recommending that the interim road improvements be considered in the next budget cycle.
- A new municipal park within the area is identified. The size of land required for a viable park in this area necessitates that the City will need to purchase 0.33 ha of



land which is over and above what can be acquired through parkland dedication under the Planning Act. A high-level cost estimate (2021 dollars) is between \$1,250,000 to 1,450,000 plus ancillary costs (e.g. legal fees, surveys, appraisals, environmental site assessments etc.).

- Cultural Heritage Landscape Study – the study recommends that the City complete a Cultural Heritage Landscape for the area. A preliminary estimate to complete this work is \$75,000.

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## Background

In recent years the River Rd. neighbourhood has seen growth pressure through infill redevelopment of existing large residential lots. The City's Official Plan identifies the area for urban development; however, the area is predominantly rural with limited municipal infrastructure to support the development permitted by the Official Plan.

Through the consultation process on site specific development applications the City has heard concerns that new infill development needs to be considerate of the surrounding rural and lower density character and that the infrastructure (sanitary services, transportation etc.) needs to be properly planned to accommodate new growth.

In response to these concerns, City Council enacted interim control by-law 19-100 on June 19, 2019. The purpose of the interim control by-law is to temporarily "freeze" development permissions within the study area while the City completes a land use and servicing study. In September 2020 the interim control by-law was extended until June 18, 2021. The interim control by-law was further extended until September 23, 2021. The boundary of the study area is shown in red below.

- Location – Bounded by Townline Rd to the east; River Rd and the Speed River to the west and north; Melran Dr. to the south;
- Area is 19. ha (47 acres).



Figure 1 River Rd Study area

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## Analysis

### Strategic Alignment

PEOPLE To actively engage, inform and create opportunities for people to participate in community building – making Cambridge a better place to live, work, play and learn for all.

Goal #1 - Community Wellbeing

Objective 1.4 Promote, facilitate and participate in the development of affordable, welcoming and vibrant neighbourhoods.

The proposed River Rd Secondary Plan and Zoning by-law amendment will provide up to date land use policy and regulations to guide future infill development within this area of the City.

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## Comments

### Proposed Secondary Plan Official Plan Amendment:

The City of Cambridge Official Plan is recommended to be amended to create site-specific policies and mapping changes for the River Rd. neighbourhood. These changes are proposed in the form of a secondary plan which is a formal amendment to the Official Plan. The purpose of the secondary plan is to guide future development and redevelopment in this area as well as municipal works such as servicing and road design of River Rd. In addition to the policies of the secondary plan (Attachment No. 1) a number of detailed supporting studies (e.g. Functional Servicing Assessment, Cultural Heritage Assessment) form part of the plan and will be utilized in the review of future development applications and municipal works.

The following is a summary of the main components of the secondary plan:

New Residential Policies:

- The residential policies propose a new site-specific density for the area. Currently, the land is designated low/medium density residential in the City's Official Plan. This permits up to a maximum density of 40 units per hectare, which is significantly higher than the current built form in the study area. The draft secondary plan includes a new density range of a minimum of 12 units per hectare and a maximum density of 16 units per hectare (measured over the entire study area). Note that accessory dwelling units are exempt from this overall density limit.



## New Residential Designations:

- Two new residential designations are proposed in the draft secondary plan. These are identified on Schedule 4 – Land Use Structure (see Attachment No. 1)

### A. Village Residential:

The Village Residential designation applies to the lands that front onto River Rd. This designation is proposed to permit low rise ground-oriented housing including single detached, semi-detached and triplexes on large lots (e.g. 30 m/98.4 ft frontage). The target density is 9 units per hectare to a maximum density of 13 units per hectare. Accessory dwelling units are permitted but are not included in the density calculation. The density allocation for the Village Residential area has increased slightly from the proposed 8 units per hectare at the time of the public meeting report. This update is based on additional analysis of development scenarios. In addition to larger lot frontage and area requirements, site specific standards to require averaging of front yard setbacks, larger side yard setbacks, and a maximum building height of two storeys is proposed. The intent of this policy and development standards is to maintain the existing character of River Rd which is currently comprised of low-rise housing on large rural-like lots.

The policies and zoning for the Village Residential area (River Rd fronting properties) has been revised since the April 27, 2021 public meeting to eliminate fourplexes as a permitted use. This change was made in response to public feedback which indicated that less intensive development is preferred along River Rd. In addition, an accessory dwelling unit within a detached structure (e.g. “coach houses”) is recommended to be permitted (along River Rd. and throughout the study area) and site-specific zoning regulations are included in the recommended by-law for this use (described further below). The purpose of permitting detached accessory dwelling units is to allow for a modest level of intensification on existing lots which would still allow for the preservation of existing homes. This use is further described in the zoning section of this report.

### B. Neighbourhood Residential:

The Neighbourhood Residential designation applies to lands located in the interior/central area of the study and are considered to be able to accommodate more medium density format development than the land fronting on River Rd. The Neighbourhood Residential land use designation is drafted to permit single detached, semi-detached, townhouse and stacked townhouse dwellings to a maximum density of 40 units per

hectare for the interior/central area which fits into the new density range of a minimum of 12 units per hectare and a maximum density of 16 units per hectare (measured over the entire study area) referred to above. Building heights are proposed to be a maximum of three storeys. The rationale for permitting development at a greater intensity within the interior/central area of the plan is to ensure that the secondary plan provides for a range of housing options which is a required objective by local and Provincial policy. This area of the study also comprises larger landholdings that are more feasible for development than what exists on River Rd.

- Urban Design Policies:

The secondary plan includes policies to guide the physical design of new development. These include:

- Policies to require new homes to address the street through consistent front yard setbacks, front entry elements such as porches and direction for a variety of architectural styles.
- Policies to minimize the visibility of garages and driveways, particularly along River Rd.
- Design criteria for priority lots such as buildings facing River Rd, corner lots and terminating view lots in new development.

- Transportation and Pedestrian Network:

A new local street system is proposed through the extension of municipal roads from McMeeken Dr. and Alderson Dr. to River Rd. Only one vehicular connection is proposed to intersect with River Rd. This will provide municipal frontage to the interior/central area of the plan and facilitate new street fronting development and provide public access to a proposed new municipal park. A private road to serve the development lands at 442/448 River Rd is also shown, which is consistent with the private initiated development (file R04/19) that is currently under review by the City. A pedestrian connection over these lands to River Rd. is also identified. The secondary plan includes a policy that requires an easement to be conveyed to the City to secure this access.

The secondary plan proposes improvements be made to River Rd. An interim improved road design is planned which could be accommodated within the current 14.0 m (46 ft) road allowance (Attachment No. 8). This design proposes a 1.8 m (5.9 ft) pedestrian sidewalk and bioswale to provide stormwater management.

A mid to long-term design of River Rd. is also proposed (Attachment No. 9). This design is based on a full 20.0 m (65.6 ft) road cross section and includes a 3.5 m (11.5 ft) pedestrian/bicycle path and larger landscape/bioswale strip and lighting.

The intent of both the interim and ultimate road design is to maintain a rural road type but provide for safe pedestrian facilities. In order to implement the mid to long-term design of River Rd, the City will need to acquire road widenings which typically are received through the redevelopment process or expropriation. Therefore, it may be many years until the full reconstruction of River Rd. is completed. On this basis staff is recommending that the interim improved road design be considered through the next budget cycle since this design can be accommodated within the current River Rd. road allowance.

- **Natural & Recreational Open Space System:**

The existing natural open space system comprised of wetlands, the Speed River and flood plain is recognized in the proposed plan and policies. The schedules of the Secondary Plan identify a special policy overlay on the development land comprised of 442/448 River Rd, since portions of this land is within a conceptual 30.0 m (98.4 ft) buffer area of an adjacent wetland. The policy includes a requirement that the proponent must submit an Environmental Impact Study as part of a development application (submitted and under review) and provides that if it is demonstrated that a natural heritage feature or associated buffer may be reduced or removed, the proponent shall incorporate portions of the feature into development plans, where feasible, to the satisfaction of the City of Cambridge and the Grand River Conservation Authority (GRCA) and that should the feature and/or buffer be reduced in size, the adjacent land use (residential) designation shall apply.

Based on feedback from City staff, a minimum 0.6 ha (1.48 acre) park is required to serve this area of the City. Under the Planning Act it is estimated that approximately 0.33 hectares/0.8 acres in parkland dedication may be acquired through the development approval process and therefore the City would be required to acquire the balance of 0.27 hectare (0.66 acres). The location of this park is conceptually shown on the demonstration plan within the Secondary Plan Report prepared by Fotenn Planning + Design (Attachment No. 11). A future trail is identified over this parkland to provide a pedestrian connection through the wooded area of Red Wildfong Park through to Melran Drive and McMeeken Dr.

- **Heritage:**

A Cultural Heritage Resource Assessment was prepared as part of the secondary plan study. This assessed properties within the study area and identified properties that have potential heritage merit. The assessment recommends the following:

- The City of Cambridge should evaluate all properties identified as having potential cultural heritage resources under Ontario Regulation 9/06 and the City's Cultural Heritage Value Criteria to ensure that all potential heritage resources that merit designation are conserved.
- If the City is unable to evaluate potential heritage resources, any planning application on or adjacent to an identified heritage resource or any demolition should require a heritage impact assessment.
- River Rd. merits recognition as a City of Cambridge Scenic Heritage Road (and that the City should develop local policies for Scenic Roads) and as a potential cultural heritage landscape (CHL) under the Region of Waterloo's evaluation criteria. A CHL would require further evaluation and the Heritage report identifies a proposed boundary which includes all properties within the secondary plan study area that have frontage on River Rd (Attachment No. 12). These recommendations are contained within the Secondary Plan. Staff is recommending that Council direct that this study be initiated in 2022.

The City's Municipal Heritage Advisory Committee (MHAC) has been circulated the draft material for information. Any property that may be proposed to be listed on the City's heritage registrar or designated would require consultation with MHAC. Should Council direct staff to pursue the CHL study this would also involve consultation with MHAC.

- Servicing:

Currently sanitary sewer services are not available along River Rd. The long-term servicing plan is to provide gravity sanitary service along River Rd. This would enable existing homes on private septic to connect to City services (when feasible). Existing homes on Melran Dr. that are served by private grinder pumps would also be able to connect to a proposed City owned lift (pump) station which would enable these homes to disconnect from private grinder pumps. The cost of plumbing alterations on the private properties would be the responsibility of each property owner. Homes on the recently approved development at 340-360 River Rd, as well as the proposed development over 442/448 River Rd would ultimately be able to connect to gravity sewers within the River Rd road allowance upon extension of gravity sewers along River Rd.

It should be noted that there are a number of existing low-lying properties on the Speed River side of River Rd which may not be able to connect to the future sanitary sewer via gravity connection. This is due to the existing homes being situated much lower than the road. When municipal sewers are extended on River Road in the future, the City will not force these properties to connect to municipal services if a gravity connection is not feasible.

Water service is proposed to be extended along River Rd. and through the extension of McMeeken Dr. and Alderson Dr.

Stormwater management is proposed through a combination of lot level infiltration controls, improved storm service along River Rd. and an end of pipe storm water management facility within the central area of the plan.

The Functional Servicing Assessment includes a preliminary cost estimate of \$6,060,000 for the full reconstruction of River Rd.

An interim improvement to River Rd. which would include a pedestrian sidewalk and swales for stormwater management but not sanitary and water service extension is estimated to be \$1,560,000. Staff is recommending that the interim road improvements be considered in the next budget cycle.

### **Zoning By-law Amendment:**

In order to implement the policies of the secondary plan, a recommended zoning by-law amendment has been prepared (Attachment No. 2). The following is a summary of the main components of the by-law amendment:

- Properties fronting along River Rd. are proposed to be zoned a site-specific version of R2 – Residential, and would permit single detached, semi-detached, and triplex buildings on large lots (e.g. 30 m/98.4 ft frontage, 900 m<sup>2</sup>/9,687 ft<sup>2</sup> lot area). The permission for fourplexes has been eliminated based on public feedback received. Site specific standards to require averaging of front yard and larger than typical side yard setbacks (e.g. 5.0 m/16 ft) and a maximum building height of two storeys is proposed (currently there is no height limit). A site-specific standard to reduce the projection of a front yard garage and regulations to limit the impact of front yard parking (e.g. parking for a triplex is required to be located behind the building so that the front yard area remains landscaped) is also proposed.
- Staff acknowledge that many residents would prefer that the zoning be further restricted to single detached dwellings only along River Rd. Staff believe that the proposed regulations for new development will maintain the important characteristics of the River Rd. area yet also allow for a modest level of intensification and range of housing options which are important land use planning principles. The character of River Rd. can be maintained by the large lot frontage/area requirements, setbacks and garage/driveway regulations as well as the requirement for site plan approval for new development (described further below in this report). The staff recommended zoning by-law amendment is contained within Attachment No. 2.



- Although staff and the consultant recommend that the zoning by-law be amended to permit single detached, semi-detached and triplexes along River Rd, a draft amending by-law to more restrictively zone River Rd. to permit single detached residential only is included in Attachment No. 3 in the event that Council wishes to pass such a by-law.
- The central/interior (north of Alderson Dr. west of McMeeken Dr., east of River Rd and south of 442/448 River Rd) area off the proposed western extension of McMeeken Dr. is to be zoned for low rise multiple residential and would permit townhouses, stacked townhouses, back-to-back and stacked back-to-back townhouses. Site specific development standards are proposed, including a minimum front yard setback of 5.0 m (16.4 ft) and requirement to recess garages 1.0 m (3.2 ft) behind the front wall of the building. A maximum coverage of 60% is proposed and a maximum building height of three storeys is also included.
- Land located west of the northern extension of Alderson Dr. and 442/448 River Rd is proposed to be zoned a site-specific version of R5. This would permit single, semi-detached and triplexes dwellings with 12.0 m (39 ft) frontages and 270 m<sup>2</sup> lot area with recessed garages. A maximum lot coverage of 50% is proposed and maximum building height of three storeys is required.
- 240 McMeeken Dr. is proposed to be zoned Holding-Multiple Residential – (H)RM4. This property is identified as a road connection (extension of McMeeken Dr.) and the Holding would restrict development until it is confirmed whether this road is required to be extended.
- Since the public meeting, the proposed zoning has been updated to allow for an accessory dwelling unit within detached structures (e.g. a coach house). The addition of this permission is to allow for affordability and a more “gentle” form of intensification, while still retaining existing housing stock. This use is consistent with recent amendments to the Planning Act, passed in 2019 (More Homes and More Choice Act, 2019).

### **Site Plan Control:**

In addition to recommended policy and zoning by-law changes for the area, the Secondary Plan recommends that the City amend the site plan control by-law to apply site plan control to properties fronting on River Rd. Typically, single detached, semi-detached and triplex dwellings are not subject to site plan control. The amending site plan control by-law is found in Attachment No. 4.

### **Options:**

- This report summarizes staff’s recommended amendments to the Official Plan and Zoning By-law. Staff is recommending that Council approve the Official Plan

amendment contained in Attachment No.1 and the Zoning By-law amendment in Attachment No. 2. The recommended Zoning By-law amendment would permit single detached, semi-detached and triplex dwellings fronting along River Rd.

- Staff have included for Council consideration an alternative amendment to the Zoning By-law (Attachment No. 3). This alternative amendment would zone River Rd. to only permit single detached dwellings. Although staff is not recommending this option, it is provided in the event that Council wishes to more restrictively zone the River Rd. fronting properties.
- If Council does not support any of the amendments and recommendations, then the existing land use planning policy and zoning framework for this area of the City will remain in place. This will also mean that the City will not pursue any of the implementation measures of the study such as a cultural heritage landscape study.

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## Existing Policy/By-Law

### City of Cambridge Official Plan:

The River Road neighbourhood is currently designated as “Low/Medium Density Residential” in the City of Cambridge Official Plan and Natural Open Space System. The Low/Medium Density Residential land use designation permits single detached, townhouses and/or walk up apartments up to a maximum density of 40 units per hectare.

Policy 10.2 of the Official Plan provides the basis for developing Secondary Plans. This policy states that Secondary Plans may be prepared for specific areas of the City to provide more detailed planning objectives and policies to direct and guide development. Criteria to consider in the preparation of a secondary plan includes (but is not limited to):

- Patterns of land use;
- Mix and range of housing types and densities;
- Provision for trails, parks and open spaces;
- Development of a transportation network that facilitates the efficient movement of vehicular and pedestrian traffic including active transportation considerations; and,
- Municipal services.

## **Zoning By-law 150-85**

The area is currently primarily zoned (H)R4 - Holding – Residential 4 and R2 – Residential. The Holding provision restricts development until servicing is provided. R2 permits large lot single detached residential.

## **Provincial Policy Statement, 2020**

Section 3 of the Planning Act requires that decisions affecting planning matters shall be consistent with policy statements issued under the Act. The PPS provides policy direction on matters of provincial interest related to land use planning and development, including the protection of resources of provincial interest, public health and safety, and the quality of the natural and built environment.

The River Road Secondary Plan has been developed to ensure the efficient use of land and infrastructure, to support active transportation and the provision of parkland, and the provision of an appropriate mix of housing forms, including affordable housing options, in a contextually appropriate way for this unique area of the City.

The Secondary Plan is appropriate and consistent with the PPS.

## **A Place to Grow - Growth Plan for the Greater Golden Horseshoe**

The Growth Plan focusses on directing new development to existing settlement areas in order to support the development of complete communities that contain a diverse mix of uses and housing options in order to optimize existing infrastructure and public transit. The Growth Plan contains development targets for urban areas (referred to as Built-Up areas). The River Road Secondary Plan area is located within a settlement area and is considered a Built-Up area in the Growth Plan. The proposed secondary plan and implementing by-law allows for an appropriate level of intensification for this area of the City and conforms to the Growth Plan for the Greater Golden Horseshoe.

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## **Financial Impact**

- The full reconstruction and servicing of River Rd is estimated to be \$6,060,000.
- An interim improvement to River Rd. which would include a pedestrian sidewalk and swales for stormwater management but not sanitary and water service extension is estimated to be \$1,560,000. Staff is recommending that the interim road improvements be considered in the next budget cycle.
- A new municipal park within the area is identified. The size of land required for a viable park in this area necessitates that the City will need to purchase 0.33 ha of land which is over and above what can be acquired through parkland dedication under the Planning Act. A high-level estimate (2021 dollars) is between \$1,250,000 to 1,450,000 plus ancillary costs (e.g. legal fees, surveys, appraisals, environmental site assessments etc.).

- Cultural Heritage Landscape Study – the study recommends that the City complete a Cultural Heritage Landscape for the area. A preliminary estimate to complete this work is \$75,000.

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## Public Input

Public consultation has been an important component in the secondary plan study. This has included the following:

- An in-person neighbourhood meeting – Spring 2019
- Virtual neighbourhood meeting - Fall 2020
- Small resident liaison meeting – Winter 2020
- Statutory public meeting – Spring 2021
- Small resident liaison meeting – Spring 2021

Public submissions are included in Attachment No. 5. Attachment No. 6 includes a summary of comments received and a staff response. This attachment also includes summaries of public feedback received at the neighbourhood meetings held in Spring and Fall 2020.

### April 27, 2021 Statutory Public Meeting

A statutory public meeting under the Planning Act took place on April 27, 2021. This was an opportunity to formally introduce the project to Council and the public. The main comments received included the desire for single detached dwellings only along River Rd, questions on the timing of road improvements and questions related to existing homes on private services. Following the statutory public meeting, a (second) virtual resident working session was held on May 4, 2021 to discuss input received at the statutory public meeting. Comments received during the working session included: preference for single detached dwellings and implementation of the ultimate cross section along River Road; there should be no parking on River Rd; questions regarding need for River Rd residents to connect to municipal services when available; preference for no road connection at McMeeken Drive and River Rd; City should consider extending the Blackridge Cultural Heritage Landscape boundary to encompass River Rd; and, preservation of existing trees is important especially in the east side of the study area between McMeeken Drive and Olivewood Crescent.

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## Internal/External Consultation

The Secondary Plan is being led by Development Engineering staff in consultation with City Planning and Transportation staff. External agencies such as the Region of

Waterloo and Grand River Conservation Authority (GRCA) have been circulated the material for comment.

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## Conclusion

Planning staff is recommending approval of the River Rd Secondary Plan and Zoning By-law amendment. The amendments are consistent with the Provincial Policy Statement, 2020 and conforms to the Growth Plan (2019), the Regional Official Plan and City of Cambridge Official Plan and represents good planning.

The proposed River Rd Secondary Plan and Zoning by-law amendment will provide up to date land use policy and regulations to guide future infill development within this area of the City which aligns well with Goal #1 – Community Wellbeing of the City's Strategic Plan.

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## Signature

### Division Approval

N/A

Reviewed by the CFO

Reviewed by Legal Services

**Name:**

**Title:**

### Departmental Approval



**Name: Hardy Bromberg**

**Title: Deputy City Manager, Community Development**

### City Manager Approval



**Name: David Calder**

**Title: City Manager**

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## Attachments

Attachment No. 1 - Official Plan Amendment No. 47 River Rd Secondary Plan

Attachment No. 2 – Recommended Zoning By-law Amendment

Attachment No. 3 – Alternative Zoning By-law Amendment

Attachment No. 4 – Amendment to Site Plan Control By-law 20-60

Attachment No. 5 – Public Submissions

Attachment No. 6 - Public Submission Response Table, Consultant Summary of  
Neighbourhood Meetings 1 & 2

Attachment No. 7 – Excerpt from the April 27, 2021 Public Meeting

Attachment No. 8 – River Road Cross Section – Interim

Attachment No. 9 – River Road Cross Section – Ultimate

Attachment No. 10 – Planning Report Prepared by Fotenn Planning + Design

Attachment No. 11 – Conceptual Demonstration Plan

Attachment No. 12 – Potential Cultural Heritage Landscape Study Boundary



By-law No. XX-21

of the

City Of Cambridge

Being a By-law of the Corporation of the City of Cambridge to adopt Amendment No. 47, to the City of Cambridge Official Plan (2012) as amended for lands known as the River Rd Secondary Plan

WHEREAS Council of the City of Cambridge has the authority pursuant to Section 17 and 22 of the Planning Act, R.S.O. 1990, c. P. 13, as amended to adopt an Official Plan and made amendments thereto;

NOW THEREFORE BE IT RESOLVED THAT the Corporation of the City of Cambridge enacts as follows:

1. THAT Amendment No. 47 to the City of Cambridge Official Plan (2012) applies to lands described as the River Rd Secondary Plan
2. THAT Amendment No. 47 to the City of Cambridge Official Plan (2012) as amended, consisting of the text and attached map, is hereby adopted;
3. THAT the Clerk is hereby authorized and directed to make application to the Regional Municipality of Waterloo for approval of the aforementioned Amendment No. 47 to the City of Cambridge Official Plan (2012), as amended;
4. THAT this By-law shall come into full force and effect up on the final passing thereof.

READ A FIRST, SECOND AND THIRD TIME, ENACTED AND PASSED this \_\_\_\_\_  
day of \_\_\_\_\_, A.D. 21.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



**AMENDMENT NO. 47 TO THE  
CITY OF CAMBRIDGE OFFICIAL PLAN  
RIVER ROAD SECONDARY PLAN**

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# AMENDMENT NO. 47 TO THE OFFICIAL PLAN OF THE CITY OF CAMBRIDGE

## **SECTION 1 – TITLE AND COMPONENTS**

This amendment shall be referred to as Amendment No. 47 to the Official Plan of the City of Cambridge. This amendment is comprised of Sections 1 to 4 inclusive and Schedules 1-4.

## **SECTION 2 – PURPOSE OF THE AMENDMENT**

The purpose of the Official Plan Amendment is to:

1. Amend the Official Plan to include the River Road Secondary Plan which will introduce a policy framework to guide development in parts of the River Road Community;

## **SECTION 3 – BASIS OF THE AMENDMENT**

### **3.1 Location and Description of Area**

The River Road Neighbourhood is located in the Hespeler community and is generally bounded by: Townline Road to the east; River Road and the Speed River to the west and north; and Melran Drive to the south.

The River Road Secondary Plan area includes a series of lands along River Road between the Brewster Trail (Flynn Avenue) and River Road, to Townline Road and River Road. The total area of the Secondary Plan area is approximately 19.0 hectares (47 acres) and is shown in Figure 1. Although the area is currently described as rural / suburban, it is designated for urban development according to the Low/Medium Density Residential land use

**Figure 1**



designation in the City's Official Plan. This designation permits single detached, townhouses and walk up apartment buildings to a maximum density of 40 units per hectare which is a higher density form of development than what currently exists in this part of the City.

### **3.2 Background**

Through the consultation process on site specific development applications, the City has heard concerns that new infill development needs to be considerate of the surrounding rural and lower density character and that the infrastructure (sanitary services, transportation etc.) needs to be properly planned to accommodate new growth.

In response, City Council enacted an interim control by-law in accordance with the Planning Act in June of 2019 to restrict development in the area for a period of one year to allow for studies to be completed to address long term planning and servicing for the area. In the Fall of 2019, the River Road Secondary Plan, Zoning and Servicing Study was initiated to evaluate potential servicing alternatives and to develop an updated policy and regulatory framework to guide new development through a Secondary Plan and Zoning By-law Amendment for the area. The Study also included a review of area stormwater management, traffic/transportation and cultural heritage.

The study also involved the identification and consideration of opportunities and constraints, including:

<b>Opportunities</b>	<b>Constraints</b>
<ul style="list-style-type: none"><li>• vacant/undeveloped properties</li><li>• potential for improved vehicular, cycling and/or pedestrian connections</li><li>• improving streetscapes</li><li>• improving views and connections to river and natural features</li><li>• incorporating existing heritage homes into the new street and block pattern for the community</li></ul>	<ul style="list-style-type: none"><li>• limitations on future connections</li><li>• lands adjacent to the river and wetlands, and/or located in the GRCA floodplain</li><li>• land adjacent to butternut trees and associate buffers</li><li>• significant grade changes and/or slope erosion hazards</li></ul>

Council extended the interim control by-law to September 23, 2021 to permit the proper completion of the study.

### **3.3 Existing Planning Policy Framework**

The River Road Secondary Plan area is located in the Built-Up Area in the City's Official Plan, and is designated Low/Medium Density Residential and Natural Open Space System. The Low/Medium Density Residential designation permits development of single detached dwellings, townhouses, and walk-up apartments to a maximum density of 40 units per hectare, where municipal water supply and wastewater systems are available. Compatible community facilities such as schools, parks, places of worship and convenience commercial establishments are also permitted.

The City's Official Plan directs that secondary plans may be prepared for specific areas of the City to provide more detailed planning objectives and policies to direct and guide development.

### **3.4 Official Plan Amendment Description and Rationale**

The City of Cambridge Official Plan is hereby amended by adding the attached "Chapter 18: River Road Secondary Plan".

#### **3.4.1 Secondary Plan**

The purpose of the River Road Secondary Plan is to guide the future planning and development of the River Road Secondary Plan lands. The Secondary Plan supports and builds on the policies of the City of Cambridge Official Plan, and provincial and regional policies and plans, with respect to orderly development of the area.

The Secondary Plan establishes a vision and the principles for the design and development of the area. It also establishes the general land use patterns and conceptual locations of parks and trails, roads and infrastructure. The Secondary Plan includes goals, general policies and land use policies that include four schedules.

#### **3.4.2 Compliance with Provincial Legislation and Policy**

##### **Provincial Policy Statement (2020)**

Section 3 of the Planning Act requires that decisions affecting planning matters shall be consistent with policy statements issued under the Act. The Provincial Policy Statement (PPS) was issued under the authority of Section 3 of the Act. The PPS provides policy direction on matters of provincial interest related to land use planning and development, including the protection of resources of provincial interest, public health and safety, and the quality of the natural and built environment.

Section 1.1.1 of the PPS states, in part, that “healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs”.

Section 1.1.3.2 states, in part, that “land use patterns within settlement areas shall be based on:

- densities and a mix of land uses which: a) efficiently use land and resources; b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; and,
- land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated”.

Section 1.1.3.3 states that “planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs”.

Section 1.1.3.4 states that “appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety”.

Section 1.4.3 states, in part, that “planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

- b) permitting and facilitating: 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and 2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;
- c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;
- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and
- f) establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety”.

Section 1.5.1 of the PPS states that healthy, active communities should be promoted by: planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity; and planning and providing for a full range and equitable distribution of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources.

The River Road Secondary Plan has been developed to ensure the efficient use of land and infrastructure, to support active transportation and the provision of parkland, and the provision of an appropriate mix of housing forms, including affordable housing options, in the community.

Infrastructure, which includes transportation corridors and sewage and water systems, shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected needs (Section 1.6.1). In addition, planning for infrastructure shall be coordinated and integrated so that they are available to meet current and projected needs. Policy 1.6.6.2 states that the extension of existing municipal water and wastewater systems are the preferred form of servicing in settlement areas. Section 1.6.4 states that “infrastructure and public service facilities should be strategically located to support the effective and efficient delivery of emergency management services”.

Section 1.6.7.1 states that “transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs” and section 1.6.7.2 states that efficient use should be made of existing and planned infrastructure, where feasible.

The River Road Secondary Plan has been developed based on a vision that includes streets designed to enable safe access for all users. The objectives of the transportation network include promoting the efficient movement of traffic and incorporating improved active transportation facilities within the neighbourhood. Planning for the River Road Secondary Plan area is based on using existing infrastructure efficiently.

Section 2.1, Natural Heritage, of the PPS states that natural features and areas shall be protected for the long term and development and site alteration shall not be permitted in significant wetlands (2.1.4), and significant woodlands “unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions” (2.1.5). Also, development and site alteration are not permitted on lands adjacent to significant wetlands and significant woodlands “unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions” (2.1.8). In regard to ‘other natural heritage features’, “negative impacts” is defined in the PPS as “degradation that threatens the health and integrity of the natural features or ecological functions for which an area is identified due to single, multiple or successive development or site alteration activities”.

Policy 2.6.1 of the PPS states that significant built heritage resources and significant cultural heritage landscapes shall be conserved and Section 2.6.3 states that “planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved”.

The cultural heritage policies of the River Road Secondary Plan have been developed based on the Cultural Heritage Resource Assessment (CHRA) Study prepared by Archaeological Services Inc. as background to the River Road Secondary Plan and Servicing Study. Based on the results of the background research and field review, there are 18 existing and potential cultural heritage resources within or adjacent to the study area. 14 properties have also been identified as potential cultural heritage resources that may merit further evaluation to determine whether they contain cultural heritage value. Existing and potential cultural heritage resources are found within or adjacent to the study area to include:

- Two properties (CHR1 and CHR11) are designated under Part IV of the Ontario Heritage Act
- One Cultural Heritage Landscape (CHL1) identified by the City of Cambridge is adjacent to the study area.

- One Canadian Heritage River (CHR12) is adjacent to the study area.
- 14 properties (CHR2 to CHR10, and CHR13 to CHR17) are potential cultural heritage resources that merit evaluation under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria to determine whether they contain cultural heritage value.

Using guidance on Scenic Roads and Special Character Streets from the Region of Waterloo, River Road merits recognition as a City of Cambridge "Scenic Heritage Road" as a scenic route with a "moderate" scenic quality. In addition, River Road has been identified as a potential cultural heritage landscape under the Region of Waterloo's Cultural Heritage Landscape evaluation criteria.

The Secondary Plan is appropriate and consistent with the PPS.

### **Places to Grow – Growth Plan for the Greater Golden Horseshoe (2020)**

In 2019, the Province of Ontario released an update to the provincial growth plan called "A Place to Grow – Growth Plan for the Greater Golden Horseshoe" (Growth Plan). An Office Consolidation of the Growth Plan, which includes Amendment 1 (2020) was released in August of 2020. Planning applications are required to conform to Provincial plans.

The Growth Plan focusses on directing new development to existing settlement areas in order to support the development of complete communities that contain a diverse mix of uses and housing options in order to optimize existing infrastructure and public transit. The Growth Plan contains development targets (45% infill) for urban areas (referred to as Built-Up areas). The River Road Secondary Plan area is located within a settlement area and is considered a Built-Up area in the Growth Plan.

The Secondary Plan conforms to the Growth Plan for the Greater Golden Horseshoe.

### **Regional Official Plan**

The Secondary Plan area is located in the Built-up Area within the Urban Area Boundary as identified in the Region of Waterloo Official Plan. It is intended that land within the Urban Area is "to accommodate the majority of the Region's growth within the planning horizon" of the Plan (Section 2.B.2).

The Built-Up Area identifies all lands within the built boundary of the Urban Area. Area municipalities are required to establish policies in their official plans and other supporting documents to ensure that "a minimum of 45 per cent of all new residential development occurring annually within the region as a whole will be constructed within the Built-Up Area" (Section 2.C.2).

Section 2.D.1 requires planning for new development within the Urban Area that:



- b) is serviced by a municipal drinking-water supply system and a municipal wastewater system;
- c) contributes to the creation of complete communities with development patterns, densities and an appropriate mix of land uses that supports walking, cycling and the use of transit;
- d) protects the natural environment, and surface water and groundwater resources;
- e) conserves cultural heritage resources and supports the adaptive reuse of historic buildings;
- f) respects the scale, physical character and context of established neighbourhoods in areas where reurbanization is planned to occur; and,
- h) promotes building designs and orientations that incorporate energy conservation features and the use of alternative and/or renewable energy systems.

The River Road Secondary Plan was prepared based on the principles supporting the creation of complete communities, and on the availability and planning of water and wastewater infrastructure. The policy framework was developed to ensure that the character of existing neighbourhoods is respected, and that natural and cultural heritage features are appropriately protected.

The proposed Official Plan Amendment conforms to the Regional Official Plan.

### **City of Cambridge Official Plan**

The River Road Secondary Plan area is located in the Built-Up Area according to the City's Official Plan, and is designated Low/Medium Density Residential and Natural Open Space System.

Uses such as single detached dwellings, townhouses and/or walk-up apartments are permitted on lands designated Low/Medium Density Residential where municipal water supply and wastewater systems are available (8.4.6.9).

The City's Official Plan directs that secondary plans may be prepared for specific areas of the City to provide more detailed planning objectives and policies to direct and guide development (10.2.1) provided the secondary plan is in conformity with the City and Regional Official Plans (10.2.2), and in consideration of the following Section 10.2.4 criteria:

- a) patterns of land use;
- b) population and employment projections;
- c) mix and range of housing types and densities;

- d) phasing of development in an efficient manner;
- e) provision for adequate and appropriate areas for commercial, institutional and community-oriented facilities;
- f) provision for trails, parks and open space;
- g) natural features;
- h) cultural heritage resources;
- i) incorporating intensification opportunities;
- j) development of a transportation network that facilitates the efficient movement of vehicular and pedestrian traffic including active transportation considerations;
- k) municipal services;
- l) incorporating opportunities for mixed-use and higher density development in appropriate locations;
- m) designation of land; and
- n) any other matters as deemed appropriate by the City.

Section 5.2.1 states that the design of the built environment will promote sustainable, healthy, active living through:

- a) well-connected and maintained streets, paths and trails that are able to safely accommodate different modes of transportation;
- b) safe, accessible, aesthetically pleasing, well-serviced and inclusive developments;
- c) resilient natural environments that support wildlife and their habitat and are better connected to residential areas; and
- d) walkable neighbourhoods that offer a mix of uses and range and variety of housing types with convenient access to public transit.

Section 5.4 of the Official Plan speaks to preserving and enhancing views and vistas, and protecting views to natural or cultural heritage elements within the vicinity of the Speed River. The Official Plan also states that development will be “compatible in terms of massing and scale with the existing and planned streetscape; and provide appropriate transitions in height to adjacent buildings” (5.7). Energy efficiency and sustainability is encouraged in neighbourhood and building design (5.8).

Section 8.4.6.3 (a) sets a maximum residential target of 40 units per gross hectare for the Low/Medium Density Residential designation, excluding only provincially constrained environmental areas as defined in the Regional Official Plan and accessory units. However, Section 8.4.6.7 states that a lower density may be permitted, without

amendment to this Plan, where it is warranted by at least two listed criteria including: the nature of the soils, topography, geomorphology, vegetation or other environmental features of the lands proposed for development; the established character of the neighbourhood in which the lands proposed for development are located; the projected negative impact of the proposed development on adjoining environmental features or cultural heritage resources.

The River Road Secondary Plan was developed, in part, based on the criteria in Section 10.2.4 of the Official Plan. The land use designations and policies in the River Road Secondary Plan are based on an assessment of the character of the area and intensification opportunities and are intended to ensure that new development provides a mix and range of housing types and densities, community trails and parkland, and appropriate infrastructure including roads and water and wastewater services. Planned densities are within the maximum residential target of 40 units per gross hectare.

The Secondary Plan conforms to the City of Cambridge Official Plan.

### **3.5 Public Engagement**

Multiple opportunities have been provided throughout the public consultation component of the River Road Secondary Plan Study project. On February 19, 2020, the City hosted an Introductory Open House and Working Session at the Hespeler Arena to formally introduce the project to the community, provide background information and to work with attendees to refine and augment the opportunities and constraints analysis to reflect community issues and concerns. A series of questions were also presented for consideration to better understand how participants experience River Road and the Speed River, as well as, any specific views and vistas they thought should be preserved, additional connections/links, and strategies to ensure fit in the neighbourhood. Following the Open House and Working Session, the project team prepared a Phase 1 Background Report Memorandum.

On October 13, 2020, the City held a second Open House. This session was held ‘virtually’ due to the pandemic and was an opportunity to provide the community with an update on the study including findings, opportunities and constraints based on comments from the community and the ongoing technical studies. A proposed vision for the River Road Secondary Plan area was shared with the community, along with proposed key principles to guide future development in the area related to land use and built form, transportation and mobility, parks and open space, and natural heritage. On this basis, a series of possible options were presented in the form of concept plans, along with visual examples to demonstrate possible built form and design approaches for the area. A question and answer period was also held following the presentation to allow participants with a chance to ask questions and provide comments (submitted via email) on the

project, presentation and land use options presented. An online survey was also prepared to gather comments and feedback on the land use options and accompanying land use designation precedent. The survey allowed participants to provide their comments and feedback on the land use examples, as well as, provide any additional comments about that study to date.

On December 3, 2020, a virtual Resident Group Working Session was held to give residents an opportunity to provide comments and feedback on a Draft Preferred Plan prepared for the area. There were key themes identified based on the comments and suggestions provided by attendees: maintain the rural character of River Road; support for a multi-use trail in the new proposed right-of-way; concerns respecting proposed land use options for the southwest corner of River Road and Townline Road (i.e. not reflecting existing character, inappropriate density and lack of appropriate transition); concerns regarding the proposed location and design of new townhouse development; potential impacts on natural areas; and concerns with the potential road connection to Alderson Drive and impacts on existing trees in the area.

On April 27, 2021, the City held the statutory public meeting to present the proposed secondary plan and zoning by-law amendment as required under the Planning Act. City staff and the project consultant presented the proposed amendments to the Official Plan and Zoning By-law to implement the River Road Secondary Plan. Five formal delegations were received by Council. Following the statutory public meeting, a (second) virtual resident working session was held on May 4, 2021 to discuss input received at the statutory public meeting. Comments received during the working session included: preference for single detached dwellings and implementation of ultimate cross section along River Road; should be no parking on River Road; questions regarding need for River Road residents to connect to municipal services when available; preference for no road connection at McMeeken Drive and River Road; City should consider extending the Blackridge Cultural Heritage Landscape boundary to encompass River Road; and, preservation of existing trees is important especially in the east side of the study area between McMeeken Drive and Olivewood Crescent.

## **SECTION 4 – THE AMENDMENT**

Add new Chapter 18: River Road Secondary Plan.

## CHAPTER 18: RIVER ROAD SECONDARY PLAN

## **18.1 Introduction**

The River Road area is within the designated Urban Area in the City of Cambridge. The River Road Secondary Plan establishes a vision, principles and policies for the design and development of the River Road area. It also establishes the general land use patterns and conceptual location of community infrastructure such as parks and trails, roads, and services.

The lands subject to this Section of the Official Plan are identified on Schedules 1 to 4, which are attached hereto and form part of this Official Plan.

In addition to the identified Schedules and the following text, this Secondary Plan also includes the following appendices:

Appendix A – River Road Secondary Plan Report (June 2021)

Appendix B – Supporting Documents:

- Phase 1 Background Report Memorandum (June 2020)
- Cultural Heritage Resource Assessment Study – Existing Conditions Report (January 2020, updated March 2020, May 2020 and March 2021)
- River Road Secondary Plan Traffic Study (March 2021)
- River Road Neighbourhood Plan Functional Servicing Assessment (June 2021)

### **18.1.1 Purpose of the Secondary Plan**

The purpose of the River Road Secondary Plan is to provide a land use strategy to guide the detailed planning and development of the River Road Community in a manner that integrates infill development with the existing neighbourhood and adjacent rural character of River Road, while respecting and protecting surrounding natural features.

The River Road Community is planned to contribute to the achievement of a complete community and provide opportunities for a full range of housing, transportation options including active transportation, provision of a new park and protection of existing natural open space. The Secondary Plan is to provide an appropriate framework to organize the community neighbourhood structure, transportation, park, natural open space and cultural heritage elements.

Development within the River Road Secondary Plan will conform to and implement the Regional Official Plan and the City of Cambridge Official Plan. In addition to the policies of

this Secondary Plan, all other parts of the City of Cambridge Official Plan shall apply. In the event of a policy conflict, the River Road Secondary Plan will prevail.

### **18.1.2 Structure of the Secondary Plan**

The Secondary Plan consists of the following:

- a) Neighbourhood Vision in Section 18.2
- b) Neighbourhood Structure in Section 18.3
- c) Land Use Structure in Section 18.4
- e) Servicing Strategy in Section 18.5
- f) Transportation Strategy in Section 18.6
- g) Implementation in Section 18.7
- h) Schedules 1 to 4

### **18.1.3 Goals of the Secondary Plan**

The goals of the River Road Secondary Plan are to:

- a) establish a community structure and land use strategy for lands within the secondary plan area;
- b) identify a vision and design principles for the community;
- c) ensure orderly development of the community by providing direction and guidance to the review and approval of development applications and by providing a phasing strategy that provides for the logical development of the community;
- d) identify, preserve and enhance, where possible, significant natural features and areas;
- e) deliver an enhanced River Road street cross section that preserves the views of the Speed River and associated natural features and areas as well as the road's character;
- f) efficiently use infrastructure and community facilities;
- g) ensure an appropriate mix of housing types and sizes, and promote opportunities for affordable housing;
- h) ensure the conservation of identified built heritage resources (refer to Appendix B Cultural Heritage Resource Assessment Study – Existing Conditions Report (January 2020, updated March 2020, May 2020, March 2021));



- i) identify a road network and identify opportunities for transit and active transportation;
- j) identify the optimal River Road cross section to safely and comfortably accommodate for pedestrian and cycling movement while introducing traffic calming strategies;
- k) ensure compatible built form and transition in densities relative to surrounding existing development; and,
- l) enhance the existing character of the River Road corridor.

## **18.2 Neighbourhood Vision**

The River Road Secondary Plan Vision is based on the growth and development tenets found in the City of Cambridge Official Plan to:

*“Develop a community that supports and emphasizes River Road’s unique rural character and built heritage features, in a way that allows residents and visitors to better enjoy the natural environment and scenic views associated with the Speed River.*

*The community is envisioned as a sustainable and accessible neighbourhood with a range of housing, a pedestrian friendly environment and multiple recreational opportunities.”*

Furthermore, the Vision for the River Road Secondary Plan lands is intended to encourage and manage development in a manner that delivers a vibrant and diverse community. Key components of the Vision reflected in the policies herein and related schedules include:

1. The enhancement of River Road as a scenic "promenade" route along the Speed River through the implementation of a distinctive streetscape and built form character strategy fronting onto the street;
2. The preservation and enhancement of the existing River Road streetscape through the implementation of a special residential character zone that seeks to maintain the area’s character through the implementation of a River Road typical streetscape and built form character elements such as building setbacks, height and massing;
3. The development of a neighbourhood-scaled open space network that is sensitive to the site's existing natural features and provides pedestrian links to adjacent public open space amenities;
4. The development of a community with a diverse range of housing types; and,
5. The implementation of a connected street system.

### **18.3 Neighbourhood Structure**

The Secondary Plan is based on a Neighbourhood Structure organized around River Road as a key character element with a central internal neighbourhood all connected by an active transportation system, a local road network and an integrated open space network. The objectives of the Neighbourhood Structure are to:

- a) Plan for a more complete community that is walkable and can accommodate various modes of transportation by means of a more compact form and densities that contribute to achieving the density targets of the approved Regional and City of Cambridge Official Plans.
- b) Provide an open space network of integrated and connected public spaces, including a new park (minimum 0.6 ha up to 1.5 ha in size), and trail link to the Red Wildfong Park, an enhanced active transportation network along River Road, existing mid-block connectors, and existing stormwater management facilities and natural features.
- c) Locate higher density development along internal roads and on larger landholdings in the Secondary Plan area.
- d) Ensure compatibility of scale and form between new and existing development in the vicinity and provide for appropriate transitions between existing and proposed development.
- e) Promote active streetscapes.
- f) Design the built form in a manner which is sensitive to the adjacent natural heritage system and mitigates impacts on natural features, functions and linkages through buffers, development setbacks, and stormwater management and other infrastructure best practices.
- g) Promote sustainable design throughout the built environment to promote efficient use of energy, land, and infrastructure.
- h) Design spaces that provide safe living and working environments through the consideration of Crime Prevention Through Environmental Design (CPTED) principles.
- i) Coordinate the location of parks with the active transportation network including community trails, walkways and bike lanes.
- j) Coordinate traffic calming and pedestrian protection measures with the open space network and other public spaces including the trail system along River Road.

The River Road Neighbourhood Structure is shown on Schedule 1 and the main elements are:

- a) **Residential Neighbourhoods** comprised of lands designated Village Residential and Neighbourhood Residential as identified on Schedule 4 and described below:

Village Residential – Fronting onto River Road, these lands are to frame and enhance the road's character through River Road specific built form and siting parameters while accommodating new active transportation infrastructure.

Neighbourhood Residential – Larger landholdings internal to the secondary plan, these lands might be anchored by open space at its centre (pending draft plan application process) and planned to contribute to the overall housing mix and density targets of the community as a whole.

- b) **Existing Natural Open Space System** – Comprised of existing natural heritage features such as wetlands, the natural areas associated with the Speed River as well as the flood plain. The system frames the overall structure of the Plan by maintaining and conserving these areas.
- c) **Proposed Parks/Open Space** – Comprised of a centrally located proposed open space amenity and active transportation links to the adjacent existing open spaces of Red Wildfong and Sault Parks.
- d) **The Road System** – Comprised of an internal north-south and east-west public road system and a private road system intended to facilitate pedestrian, cycling and vehicular movement. The Road System extends into existing roads intersecting the study area, such as McMeeken and Alderson Drives.

### 18.3.1 Sustainable and Healthy Neighbourhood

The community design elements structuring the River Road Secondary Plan area are the physical land use planning manifestation of a strategy that seeks to achieve social, economic, and environmental sustainability.

Development in the River Road Secondary Plan is envisioned to achieve:

- a) Social sustainability by encouraging a diverse community that offers a wide range of housing mix to a wide range of people with different backgrounds, age, lifestyles and socio-economic status;
- b) Economic sustainability by continuing to encourage home occupation related employment and increasing the local population that can support Cambridge's business community; and,

- c) Environmental sustainability by:
- i. integrating active transportation infrastructure that encourages walking and cycling;
  - ii. encouraging environmentally responsible design and construction practices;
  - iii. the integration, protection and enhancement of natural features and landscapes into building and site design;
  - iv. promoting practices, which conserve water, and protect or enhance water quality;
  - v. on a site-specific basis, certain techniques such as the use of roof top gardens and the re-use of grey water will be encouraged provided that groundwater infiltration targets are not compromised; and,
  - vi. design of stormwater management should consider Low Impact Development (LID) measures, on-site infiltration on a distributed basis, adaptive management design, reduced chloride impacts, and resiliency of wetlands and resilient stormwater management strategy as outlined in the Functional Servicing Report.

### **18.3.2 Cultural Heritage**

Cultural heritage resources within the River Road Secondary Plan area shall be conserved. Development in the River Road Secondary Plan is envisioned to promote development which respects and reflects the physical and cultural identity and the heritage attributes of the area. Development that is adjacent to significant cultural heritage resources are to be of an appropriate scale and character.

The conservation of existing cultural heritage as identified in the Cultural Heritage Resource Assessment Study, Existing Conditions Report (refer to Appendix B) is encouraged in accordance with and to implement the policies of Chapter 4: Cultural Heritage Resources of this Plan. The City will consider evaluating all properties identified as potential cultural heritage resources in the Cultural Heritage Resource Assessment Study, Existing Conditions Report (refer to Appendix B) under Ontario Regulation 9/06 and the City's Cultural Heritage Value Evaluation Criteria to ensure that all potential cultural heritage resources that merit designation are conserved.

If the City does not evaluate potential cultural heritage resources under Ontario Regulation 9/06 and the City's Cultural Heritage Value Evaluation Criteria, the City may require submission of a heritage impact assessment with any Planning Act application on or adjacent to an identified existing or potential cultural heritage resource or any demolition application. The heritage impact assessment will further assess the cultural heritage value

of the identified potential cultural heritage resources under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria to ensure that the existing cultural heritage resources in the Secondary Plan area are conserved.

The following mitigation measures and/or alternative development approaches should be considered in development design to reduce the potential for adverse impacts to potential cultural heritage resources in the area:

- a) Avoidance and mitigation to allow development to proceed while retaining potential cultural heritage resources in situ and intact;
- b) Avoidance protocols to isolating development and land alterations to minimize impacts on significant built and natural features and vistas;
- c) In support of Section 4.2.1(b) and other policies of this Plan, the City encourages design approaches that:
  - i. Ensure compatible lot patterns, situating parks and storm water ponds near a heritage resource;
  - ii. Limit height and density of buildings on adjacent and nearby lots;
  - iii. Ensure infill development that is compatible with heritage resources; and,
  - iv. Use vegetation buffer zones, tree planting, and other design measures, implemented through the review and approval of plans of subdivision and site plan applications.

Where appropriate, architectural design guidelines may be required for buildings on adjacent and nearby lots to help integrate and harmonize mass, setback, setting, and materials.

#### **18.3.2.1 River Road – Potential Scenic Heritage Road and Cultural Heritage Landscape**

It has been determined that River Road merits recognition as a City of Cambridge Scenic Heritage Road as a scenic route with a 'moderate' scenic quality. The City will consider identifying River Road as a scenic heritage road upon the establishment of policies in accordance with Section 4.14 of this City Official Plan. Until such policies are created, River Road was found to meet the criteria for recognition as a potential cultural heritage landscape under the Region of Waterloo's criteria and could be recognized as such by the City.

A proposed Statement of Significance and list of attributes has been included within the Cultural Heritage Resource Assessment Study, Existing Conditions Report (refer to

Appendix B). These attributes should be considered in concert with roadway improvements intended for improving public safety within the Secondary Plan area.

### **18.3.3 Existing Natural Open Space System**

The lands designated Existing Natural Open Space System are shown on Schedule 1. The goal of the Existing Natural Open Space System is to maintain, restore and enhance existing natural features and associated ecological functions and ensure their continued existence within the urban landscape.

The Existing Natural Open Space System retains and protects all provincially, regionally and municipally designated natural features including provincially significant wetlands, river slopes erosion allowances and the regulatory floodplain associated with the Speed River.

#### **18.3.3.1 Objectives**

- a) To conserve and protect the area's natural open space system as an integral component of the community by providing clear and unobstructed visual and physical links to natural features where feasible and appropriate;
- b) To ensure a connected open space system is implemented and comprised of the existing natural features, stormwater management facilities, streets, trails, new public park with a formal and accessible trail connection through Red Wildfong Park to Melran Drive and McMeeken Drive;
- c) To encourage the conservation and integration of existing hedgerow and woodland features into the community's overall design and development.

#### **18.3.3.2 Policies**

In addition to the policies in Section 8.8, the following policies shall apply to the lands designated as Existing Natural Open Space System on Schedule 1 of this Secondary Plan:

- a) Buffers and development setback distances will address potential impacts and protect and enhance natural features. Setback distances and buffer widths vary by natural feature to ensure development does not interfere with the hydrologic function of the feature. This is to include areas within 120 metres of all provincially significant wetlands and wetlands greater than or equal to 2.0 hectares in size, and areas within 30 metres of wetlands less than 2.0 hectares in size. O. Reg. 150/06, s. 2 (1); O. Reg. 57/13, s. 1 (1-3).
- b) Development and site alteration within the buffers areas is subject to further detailed study and evaluation in a future site-specific Environmental Impact Study.

- c) Development, infrastructure and site alteration which may be considered within the buffer areas include:
  - i. Limited portions of stormwater management facilities and associated grading;
  - ii. Recreational trails and associated grading; and,
  - iii. Site alteration associated with grading incursions to address landscape; and anomalies as required to achieve sound engineering practice and design.
- d) Development or site alteration within natural features and buffers will be prohibited except as may be required for the permitted uses specified in Section 18.3.3.2(c).
- e) The Existing Natural Open Space System will maintain or enhance existing linkages within and between large natural areas, and restore linkages between natural areas where existing connectivity is degraded. Linkages are intended to provide movement areas for a range of wildlife and provide suitable dispersal corridors.
- f) The Existing Natural Open Space System will provide opportunities for ecological enhancements to improve plant and wildlife habitat.
- g) Opportunities for additional enhancements (wildlife crossing enhancements) should be considered during the detailed design of River Road and the development planning stage.
- h) Environmental Impact Studies, Hydrogeologic Assessments and Stormwater Management Plans will be required to form part of a “complete application” and as supporting information for draft plans of subdivision, plans of condominium and major zoning by-law amendment applications.

### **18.3.3.3 Regulated Environmental Features**

#### **18.3.3.3.1 Regulated Wetland/Environmental Features**

Regulated Wetland/Environmental Features on Schedules 2, 3 and 4 recognize the limits and associated minimum buffers of a Provincially Significant Wetland. The ecological and hydrologic functions of the lands designated Regulated Wetland/Environmental Features shall be protected, maintained, or, where possible, improved for the long-term, recognizing linkages between and among natural heritage features and areas.

The Regulated Wetland/Environmental Features designation is intended to:

- a) protect the health and water quality of the natural heritage feature;
- b) conserve biodiversity;
- c) protect significant natural heritage features and their associated functions; and,
- d) protect surface and ground water resources.

#### **18.3.3.3.2 Special Policy Area Overlay**

The Special Policy Area Overlay on Schedules 1, 2, 3 and 4 recognizes natural heritage features, which contribute to the Natural Heritage System.

- a) Additional environmental study is required to determine the exact boundaries and buffers to protect the features and functions to the satisfaction of the City of Cambridge, the Grand River Conservation Authority and Region of Waterloo.
- b) The proponent shall be required to submit an Environmental Impact Study as part of any future development application to demonstrate that there will be no significant negative impact on the natural heritage features or their functions.
- c) Should it be demonstrated through an Environmental Impact Study that a natural heritage feature may be reduced or removed, the adjacent land use designation shall apply.

#### **18.3.4 Open Space**

The River Road Community will provide opportunities for residents to live, work, learn and play and will contribute to the achievement of a complete Cambridge community. The proposed open space network encourages active transportation, recreation, and opportunities to connect across the neighbourhood. A new park, a trail system along River Road, a trail connection through Red Wildfong Park, existing pedestrian connections and other elements of the active transportation network (such as sidewalk and on-road cycling routes) are identified on Schedules 1, 2, 3 and 4.

##### **18.3.4.1 Policies**

A new park is envisioned to be located central to the community's larger land holdings and may serve as a community hub. The new park will also feature a trail connection through the Red Wildfong forested area to connect to Melran Drive and to McMeeken Drive.

##### **18.3.4.2 Existing Trees**

The rural character of the River Road Secondary Plan area is established by the abundance of trees. They occur throughout the study area with a large concentration located at the east edge of the plan area immediately adjacent to the existing community. Trees are an important component of the River Road Secondary Plan area as they enhance the character of the area, provide wildlife habitat, nourish and protect soils from wind and water erosion.

- a) Development of the Secondary Plan area should seek to maintain canopy and the Urban Forest Plan target. The area as a whole should meet the Urban Forest Plan canopy target (30% canopy) by protecting existing trees (in the natural area), adding



new street trees, buffer compensation plantings, and anticipated private property landscaping plans/trees in condominium blocks.

- b) A tree management plan / tree by-law permit for removals, street tree plans, as well as applicable Landscape Plans on Site Plan submissions, will be requirements for development applications.
- c) Integration of the treescape into future development plans as part of rear yards or as important element of front yards and future streets is required.
- d) Tree plantings should include a variety of native trees, appropriate to the local habitat.

#### **18.4 Land Use Structure**

The land use structure was developed on the basis of building on the rural character of River Road by generally maintaining the existing lot fabric along the roadway, while planning for a mix of low to medium density residential development east of the River Road corridor and west of existing developed areas.

The land use structure and planned densities for the River Road Secondary Plan area are based, in part, on the policies of Section 2.6.1.8 of the Official Plan directing that infill, intensification and redevelopment within existing neighbourhoods will be minor in nature and will be designed to respect existing character. While planned infill, intensification and redevelopment is more limited when compared to other areas in the City, the policies of the River Road Secondary Plan will allow for a mix of residential dwelling types and forms with new development.

Development throughout the River Road Secondary Plan area shall be encouraged to create a sense of identity and place through the use of various means including attractive built form, architectural design treatments, high quality building materials, decorative lighting, decorative street signs, boulevard treatments, and landscaping elements. The following policies are intended to apply to all new development or redevelopment within the River Road Secondary Plan area to ensure the overall design vision and objectives for the area are achieved.

##### **18.4.1 General Policies**

- a) Residential lands in the River Road Secondary Plan area are designated Village Residential and Neighbourhood Residential. The Village Residential designation applies primarily to lands located along River Road, while the Neighbourhood Residential designation applies to lands that are located more centrally in the broader River Road community (i.e., east of River Road and west of the existing residential neighbourhood). The locations of the Residential designations are

shown on Schedule 4.

- b) Lands within the River Road Secondary Plan area will be planned to achieve an overall minimum density of 12 units per hectare and a maximum density of 16 units per hectare. Accessory apartments are not included in the calculation of density. Individual development applications may have densities lower than 12 or higher than 16 units per hectare provided that the City is satisfied that the total overall density within the Secondary Plan area, at full build-out, will conform to the overall planned density range.
- c) The River Road Secondary Plan area will be planned, designed and zoned to achieve a variety of housing types, styles and lot sizes. Single detached lots will be planned to have a variety of lot widths, unless specified otherwise in this Plan.
- d) Secondary residential units and special needs housing are permitted within Village and Neighbourhood Residential designations and will be regulated by the Zoning By-law.
- e) Community gardens and other compatible forms of urban agriculture may be permitted in all residential areas.
- f) Residential streetscapes shall be designed to allow for on-street parking through creative design solutions such as varying housing types and lotting patterns.

#### **18.4.2 Affordable Housing**

The City encourages opportunities to locate affordable rental and ownership housing within the Secondary Plan Area (as defined in the Provincial Policy Statement). The City encourages a diverse range of tenure types and unit sizes scaled to meeting the City's housing needs.

#### **18.4.3 Built Form and Lotting Policies**

- a) With the exception of along River Road, a mix of lot frontages along all street blocks will be encouraged. Long blocks of similar frontages will be discouraged.
- b) A variety of building forms are encouraged. Harmonious built form typologies are encouraged within the River Road community to collectively establish a sense of place and neighbourhood character.
- c) Built form should frame intersections and provide attractive and defining entrances to the community.
- d) All building forms are encouraged to address the street in a manner that supports an active streetscape for pedestrians. Primary building entrances will be required to be oriented towards the public realm and garage walls and driveways will be required to minimize their presence on the streetscape.

- e) Consideration will be given to the location, orientation and design of Priority Lots. Through the development application review process, priority lots should be identified.
- f) Priority Lots may include: corner lots; terminating view/vista lots; lots along elbow streets; and open space lots.
- g) Buildings on corner lots will be required to articulate façades on both street frontages. Buildings on corner lots shall be oriented towards the higher-order street where possible and appropriate.
- h) Buildings at terminating vistas will be encouraged to be designed as landmarks, with architectural innovation and quality urban design that reflects the community character.
- i) The built form in close proximity to the lands designated Natural Open Space System or Open Space will be designed to be sensitive and complementary to these areas and maintain and create opportunities for views and vistas to these areas, where appropriate.
- j) Variation and excellence in building design, including architectural features, building materials, style, colour and other individual design elements, is encouraged and will be reviewed through applicable development applications. Continuous, blank facades are discouraged. Built forms may be further defined and implemented through the City's Zoning By-law and site plan approval.
- k) Design briefs are required in support of future development application and will identify priority lots such as vista, terminating view, corner lots, etc. Prior to the issuance of permits, enhanced elevations will be prepared for the identified lots in accordance with the River Road Urban Design Policies with the requirement imposed as a condition of approval.

#### **18.4.4 Land Use Policies**

##### **18.4.4.1 Village Residential**

- a) The planned intent of the Village Residential designation is to complement River Road's historic development pattern and scale in the form of ground-oriented housing in a low-rise format.
- b) The Village Residential land use designation will permit single detached, semi-detached, and triplex dwellings with a target density of 9 units per hectare and a maximum density of 13 units per hectare. Accessory dwelling units are also permitted but are not included in the calculation of density.
- c) The maximum building height in the Village Residential land use designation will be 2 storeys and will be regulated by the Zoning By-law.

- d) In addition to the built form, landscape and vegetation are an important character element of River Road. Generous naturalized front “meadows” are intended to ensure the area’s distinctive rural character is maintained as it accommodates for new development in accordance with Section 18.4.5.7.2.

#### **18.4.4.2 Neighbourhood Residential**

- a) The planned intent of the Neighbourhood Residential designation is to provide opportunities for a range of residential typologies in a medium density format that deliver on the diversity of dwelling types and densities encouraged to support the City’s range of accommodation options.
- b) The Neighbourhood Residential land use designation will permit single detached, semi-detached, triplex, fourplex, townhouse, stacked townhouse, back-to-back townhouse and stacked back-to-back townhouse dwellings to a maximum density of 40 units per hectare. Accessory units are also permitted but are not included in the calculation of density.
- c) Building heights will be a maximum of 3 storeys and will be regulated by the Zoning By-law with specific consideration being given to maximum building height and compatibility relative to existing development in the immediate vicinity to ensure appropriate transition between existing and proposed development.

### **18.4.5 Urban Design Policies**

#### **18.4.5.1 Urban Design Vision**

The Urban Design Vision for the River Road Secondary Plan builds on the overall Vision and objectives for the River Road Secondary Plan, and is based on the City of Cambridge’s Official Plan and aims to deliver a new community that:

*“(...) supports and emphasizes River Road’s unique rural character and built heritage features, in a way that allows residents and visitors to better enjoy the natural environment and scenic views associated with the Speed River.*

*(...) Is envisioned as a sustainable and accessible neighbourhood with a range of housing, a pedestrian friendly environment and multiple recreational opportunities.”*

The urban design vision encourages and manages development in a manner that delivers a vibrant and diverse community. Integral to this vision is the design and integration of the Village Residential and Neighbourhood Residential areas, each with a variety of residential housing options. In addition, the urban design vision delivers policies to ensure a high-quality, sustainable and accessible public realm and private realm.

#### **18.4.5.2 Development Block Design**

Block length and design define and structure neighbourhoods, influencing development opportunities, movement options and neighbourhood character. Each guideline may not be appropriate for all instances and an alternative may be better suited for a specific development. Therefore, variations to these provisions may be considered by City staff and where applicable Council, when it is demonstrated that an exception may be appropriate, provided that adjustments to block lengths maintain and facilitate pedestrian movement throughout the community through the provision of clear and safe pedestrian connectors and walkways.

The following are design guidelines for block design in the River Road Secondary Plan area:

- a) To maximize connections and permeability for both vehicular and active transportation modes, the street should be based on a grid pattern that is modified in response to natural open space, built heritage or existing street conditions.
- b) Block lengths should generally range between 60 to 100 metres to promote walkability.
- c) New developments that are adjacent to existing built form should pursue opportunities to integrate with or connect to existing streets. The number of connections should be maximized to increase permeability.
- d) Blocks longer than 100 metres may be considered on an individual basis where other block alternatives are not feasible. In such cases, publicly accessible pedestrian walkaways should be provided to facilitate pedestrian circulation and access.
- e) Draft plans shall be designed to ensure that residents are generally located within a 200 to 400 metre radius (2.5-to-5-minute walk) to a parkette or to other passive recreational elements such as trail heads, neighbourhood park, community park, or school.
- f) Where possible, existing hedgerows should be conserved and integrated into the neighbourhood's local street and block pattern.
- g) Where rear lanes are used, the maximum lane length should be no more than 180 metres in order to provide for a maximum 90 metre hose length from fire hydrants located on road connections. Where the 180-metre length is exceeded, an on-street hydrant location and fire hose access to the rear lane shall be provided.
- h) The use of cul-de-sacs should be minimized, except where necessary due to grading and topography or at view terminus sites. Where cul-de-sacs are used a

pedestrian and/or bicycle through-connection should be provided to promote active transportation.

- i) Significant hedgerows within public open space or parks and/or rear backyards should be protected where possible and feasible. Any grading activities around the hedgerows should be minimized.

### **18.4.5.3 Public Realm Improvement**

#### **18.4.5.3.1 Streetscape General Guidelines**

Where appropriate and feasible, collector, local and private roads streetscape design will include sidewalks on at least one side of the street, on-road or separated bicycle lanes and trail connections, subject to the following guidelines:

- a) The careful evaluation of the alignment of streets adjacent to existing hedgerows and wooded areas to ensure the site's existing natural features are visible and part of the community's open space network;
- b) Construction of Low Impact Design Stormwater Management Galleries can be implemented along the proposed roadway boulevards to provide both quality and quantity control for the proposed development. Subject to groundwater levels and soil conditions, these could consist of bio-swales and/or sub-surface infiltration galleries or sub-surface storage cells. (Refer to Appendix B: River Road Neighbourhood Plan Functional Servicing Assessment);
- c) Streets should have a relatively consistent building edge in order to provide spatial definition and containment to the street. A minimum and maximum front and exterior setback zone is defined in the accompanying Zoning By-law; and,
- d) A utility strategy where utilities are clustered or grouped where possible to minimize visual impact should be developed as part of the draft plan process.

#### **18.4.5.3.2 Street Tree Guidelines**

The combination of both public realm improvements as well as the design guidelines for the private realm are intended to increase the overall tree canopy along River Road and support the overall City-wide target for a 30 percent tree canopy. To support the achievement of the target, the City will:

- a) Endeavour to create an extensive tree canopy over all pedestrian connections in the Secondary Plan Area;
- b) Ensure street trees are spaced every 12 meters to continue the treed streetscapes of the surrounding neighbourhood as well as meet the standards of the Engineering Development Manual; and,

- c) Promote the use of innovative systems which allow for best practices in arboricultural maintenance.

#### **18.4.5.3.3 Existing Trees**

The conservation of trees aims to provide the best conditions for growth, to encourage stand renewal and to improve the health, diversity and appearance of the landscape, through three basic approaches: cleaning, thinning and stand improvement.

- a) Cleaning is done in young stands to regulate composition and favour valuable species.
- b) Thinning can remove up to 35% of the medium age (25 years) stand to allow the remaining quality trees more room and light to grow fuller and faster.
- c) Stand improvement harvests large mature trees for sale and removes defective, crooked and leaning specimens and inferior species. A few large dead trees and snags are left for wildlife habitat.

#### **18.4.5.4 Open Space**

The River Road Secondary Plan area is envisioned to include a centrally located park as well as a system of interconnected pedestrian walkways and trail facilities (refer to Figure 2: Open Space Structure). The Natural and Recreation Open Space Systems are consistent with the Official Plan designations (Section 8.8) and permitted uses.

#### **18.4.5.5 Active Transportation**

##### **18.4.5.5.1 Public Pedestrian Network**

New active transportation connections are illustrated on Figure 3. The network is comprised of a system of pedestrian street sidewalks, walkways and trails with the intent of providing pedestrians with options to get to their destination using the most direct route. The following active transportation connections have been identified:

- a) **Sidewalks along all public and private streets** – As integral components of the community open space system and active transportation network, sidewalks are required along all streets in the community.
- b) **Multi Use Trail along River Road** – Envisioned as a pedestrian and cycling multi use facility on the east side of the road, the River Road multiuse trail is intended to improve the road conditions by introducing a much needed safe and comfortable pedestrian and cycling route.

- c) **Existing pedestrian walkway connecting Skipton Crescent to River Road** - Located in the north side of the secondary plan area, this pedestrian connection runs east-west and is to be maintained as development along River Road occurs.
- d) **Future pedestrian link at Laneway 211** – This pedestrian connection is intended to provide a pedestrian connection to River Road through the future proposed private road to Olivewood Way. An easement for public pedestrian access over the private road to Olivewood Way will be conveyed to the City as a condition of any development.
- e) **Existing pedestrian and cycling connection between Olivewood Way and McMeeken Drive** – Located on the easterly edge of the secondary plan area, this connection is to remain as development occurs to ensure a pedestrian mid-block connector is maintained and transverses the long block defined by Skipton Crescent to the east and River Road to the west.
- f) **Future trail connections to Red Wildfong Park** – The new park will provide a trail connection through the wooded area of Red Wildfong Park through to Melran Drive and McMeeken Drive.

In order to encourage walkability within the neighbourhood, and improve circulation and pedestrian access, the following guidelines are intended to create accessible, safe and inviting pedestrian connections:

- a) All public streets are to provide for sidewalks on both sides of the street with the exception of River Road where a multi-use trail on the east side of the road is to be located.
- b) All private streets are to provide for a sidewalk on one side of the street.
- c) In order to accommodate the needs of persons with disabilities, and the elderly, sidewalks should be designed to applicable municipal and AODA standards with a preferred 1.8 metres minimum width and should be continuous throughout the community.
- d) Pedestrian walkways providing a link across blocks should be 6.0 metres wide to avoid creating narrow pedestrian alleys.
- e) Trails will be 3 metres wide and asphalt to conform with the Trails Master Plan. To minimize disturbance in natural features (e.g. Red Wildfong Park forested area) the City may consider a narrower trail width of 2 metres to minimize disturbance to the natural features while still maintaining the intent of the Trail Master Plan and compliance with the Accessibility for Ontarians with Disabilities Act (AODA) regulation.
- f) Crime Prevention Through Environmental Design (CPTED) principles should be used in the design of public pedestrian walkways, such as:



- i. Adequate lighting;
- ii. Clear sight lines, allowing view from one end of the walkway to the other;
- iii. Provide appropriate landscaping, but avoid landscaping that might create blind spots or hiding places;
- iv. Adequate fencing and fenestration adjacent to public walkways to provide opportunities for casual surveillance; and,
- v. Integrating activity generating uses such as seating areas or other amenities.

#### **18.4.5.5.2 Cycling Network**

On-street and multi-use trails have been proposed for the Secondary Plan area. Where possible, new bicycle lanes will be provided in accordance with the Bike Your City: Cycling Master Plan (2020) and link with the existing cycling network in Cambridge. The following are guidelines for the cycling network:

- a) Where cycling routes intersect major intersections, route signage should be provided.
- b) Access to cycling routes should be clear and unobstructed.

#### **18.4.5.6 Natural Heritage System**

A Natural Heritage System (NHS) has been established, which is comprised of a Provincially Significant Wetland and Speed River Waterway. In addition, the Regional Official Plan identifies Regionally Significant Core Environmental Features and Significant Valleys in the Secondary Plan Area. The designated areas are subject to further Environmental Impact Studies that must be reviewed and approved by the Region and City in consultation with the Grand River Conservation Authority. The following guidelines are for land designated in the NHS:

- a) Integrate the NHS as a key structural element of the neighbourhood by providing appropriate views, vistas and connections, as well as providing a range of development interfaces that offer opportunities for public visual and physical access while also limiting access where necessary.
- b) Physical barriers, such as lot fencing, should be reviewed and considered in areas where access and encroachment are to be restricted. Fencing should be coordinated as a condition of draft plan approval to ensure consistent fencing design is established.
- c) For homes adjacent to the NHS, homeowner education and stewardship should be encouraged through the distribution of a homeowner's pamphlet as a condition of draft approval. Information in the pamphlet may include conservation practices and

protection of the surrounding NHS, which include the identification of native plant species appropriate to the existing ecosystem, and the avoidance of organic waste dumping.

- d) Carefully consider the design and location of parking areas, three stream receptacle bins or street furniture to ensure that the edge of the NHS is not degraded. This can be achieved through larger setbacks or landscaped buffers with private and decorative fencing.

#### **18.4.5.7 Private Realm Guidelines**

New residential developments within the River Road Secondary Plan will integrate with the existing built form and scale of its context. It is essential that future community and architectural design solutions address matters of scale, siting and massing through the implementation of the design best practices contained in the Secondary Plan. These Urban Design policies aim to reinforce the structure and character of River Road and provide general direction for housing typology, housing mix, building form and landscaping.

To ensure an attractive streetscape is delivered, architectural controls shall be developed to address detailed building design aspects such as: massing, grading differentials, elevation articulation, garage articulation, materials colour and quality, roof design as well as the proposed siting strategy. An Architectural Control process can be implemented as part of the Draft Plan of Subdivision conditions and agreement phase to be coordinated between City staff and the proponent. In addition, site plan control should apply to any development on lands along the River Road frontage in accordance with the policies of Section 18.7

##### **18.4.5.7.1 Housing and Mix Diversity**

A variety of residential typologies and architectural styles, elements, and material detailing should be considered to create distinctive and complementary character, as well as provide visual interest.

Universally accessible housing options should be provided to accommodate the widest spectrum of people to live in the community, regardless of age, ability and socio-economic status. Lifecycle housing options should be provided to support a variety of age groups, including houses designated especially for seniors (ex: bungalows).

##### **18.4.5.7.2 Street Interface**

The interface between the public realm of streets and the private realm of developments is a transitional space that should be designed to promote and enhance walking and a sense of personal safety and comfort throughout this community. The following guidance pertains to the treatment of these interfaces.

- a) Houses should be designed to frame the street edge with a consistent front setback, and have front doors, windows, verandas, and entry features facing the road.
- b) Front entry elements should be articulated through the use of framing materials, colour and built form including porches, arches or articulated front steps.
- c) Porches should be designed to be functional and useable. Porches are permitted to encroach into the front yard setback.
- d) Front porches should not be enclosed as primarily open structures, not solid walls. Solid, low fences can be used if made of local stone. House entry features should be articulated through detailing and/or a variation of materials such as porticos and/or decorative treatments around the main door including stone, mill work, sidelights, accent lighting, or house number sign.
- e) Decorative fencing around front and/or exterior side yards adjacent to the dwelling should not block the view of the sidewalk from the house. Their height should be limited to 1.2 metres and they should offer a level of transparency as primarily open structures, not solid walls. Solid, low fences can be used if made of local stone.
- f) No two adjacent buildings should exhibit the same architectural style.

### **Meadows and Lawns**

Maintaining generous front setback areas in a naturalized manner is intended to ensure the area's distinctive rural character is maintained as it accommodates new development. The following are some environmentally friendly, neighbourhood character building guidelines for front lawn design and maintenance.

- a) Minimize lawn areas and substitute with naturalized, meadow like areas for wildlife.
- b) In the process of designing naturalized front lawns include visible design cues of human intention such as:
  - i. Mow the edges along paths such as sidewalks or trails as a clear cue to onlookers that the property is actively managed.
  - ii. Cluster plants to create rhythm.
  - iii. Introduce crisp edges and bold patterns as strong cues of vegetation management. One way to achieve this is to add both high (like large shrubs and trees) and low (like flowering annuals and grasses) plants.

- c) For lots fronting onto River Road a combination of trees (to provide further shade to the proposed multi use trail), as well as a combination of native vegetation and ground cover planting is encouraged.
- d) Avoid herbicide sprays. If absolutely necessary, use selective herbicides and selectively spot spray the largest and most obnoxious weeds.
- e) Use drought-resistant grasses. They require little watering and do not brown during dry spells.
- f) Cut grass to a minimum of 2" high. This reduces stress on the grass and shades bare patches to discourage weeds.
- g) Sow in a clover mix to cover 1/4 to 1/3 of the lawn area. Clover provides needed nitrogen to the lawn, adds colour and scent and out-competes weeds.
- h) Sow wildflowers into the lawn as was common in medieval times and known as "flowery medes." Low-growing flowers that co-exist with grass include English daisies, clover and chamomile.
- i) Sow small bulbs such as crocuses or scilla into the lawn for spring showing.
- j) Plant native plant species that provide continuity with the naturalized landscapes of the rural character of River Road.
- k) Locate non-native exotic plants around the residence and separated from native plants.

#### **18.4.5.7.3 Garages and Driveways**

The design of garages and driveways can have a major impact on the visual character of the individual dwelling and the collective streetscape. These guidelines intend to enable a cohesive streetscape where attached garages and driveways complement instead of dominating the streetscape.

#### **Front Garages**

The following guidelines shall be applied in order to minimize the presence of attached and detached garage buildings accessed from the front of the lot:

- a) Vehicular driveways and garage wall for residential buildings fronting onto River Road may be located offside streets or a rear laneway. When driveway and front-loaded garages are proposed, their presence on the landscape should be minimized through the consolidation of driveways and the setback of garage walls.
- b) The front elevation of the house shall be designed so that its front entrance design and architectural elements reduce the visual dominance of the garage and the front driveway. Garages shall not protrude beyond the main front wall of the dwelling unit.

- c) Garages must be a natural extension of the design, massing, and material of the main dwelling.
- d) For buildings within the Village Residential designation, garages should be set behind the main building face notwithstanding the presence of a porch feature.
- e) Garages should be paired to allow for more substantial front yard green space.
- f) A variety of garage door configurations and styles should be provided.
- g) Tandem garage designs are encouraged to help minimize the impact of garage width on the elevation and in turn on the streetscape.

### **Lane-Accessed Garages**

Garages that are accessed from a laneway can either be detached or attached to the main dwelling at the rear. Attached garages can either be set into the house with access at the rear, or they can be attached to the main dwelling through a breezeway, which forms a side courtyard for amenity space.

- a) Where possible, garages should be paired to allow for increased rear yard, or an outdoor parking pad to accommodate resident parking.
- b) Accessory units located above detached garages are encouraged on wider corner units.

### **Driveways**

- a) Driveways should be designed to reduce the amount of pavement on front yards and enhance the visibility of the street.
- b) Driveway widths should generally be no larger than the interior width of the garage.
- c) Driveways off River Road should, when feasible, be paved with gravel as an appropriate rural character material. Avoid the use of asphalt paving as it represents an urban character material.
- d) The use of semi-permeable or permeable paving materials and treatment is encouraged.
- e) Driveways are encouraged to be paved with light-coloured or permeable materials to reduce storm water run off and reduce heat island effect.
- f) Driveways should be located as far as possible from parks, open space features, public walkways, and intersections.

#### **18.4.5.7.4 Priority Lots**

Priority Lots have high public exposure and include: buildings facing and flanking River Road; buildings adjacent to parks and open space; corner lots; gateway corner lots; and

T-intersection / key view terminus lots. The identification and treatment of Priority Lots should be determined through the Draft Plan Subdivision process and Site Plan Agreement. The following provides guidance on the allocation of such lots.

### **Buildings Facing and Flanking River Road**

- a) Units facing or flanking onto River Road should be given special consideration in architectural design, massing, orientation, and siting. Materials should be of high architectural quality.
- b) For dwellings flanking River Road, the main front door should be visible from, and oriented to, the exterior side elevation of the house with access to the sidewalk. The entries should be articulated through the use of entry features such as projecting porches facing the street.
- c) Side elevations flanking River Road should be consistent with the front elevation in terms of materials, fenestration style and detailing.
- d) Façades should be highly articulated through coordinated fenestration, masonry detailing, accent gables, dormers, and/or other special treatment.

### **Buildings Adjacent to Parks and Open Space**

- a) Front, side and rear elevations exposed to public spaces should be well articulated. Articulation of buildings may also include changes in material colours and texture, changes in building plane, and variation of roof lines. The rear portions of buildings may benefit from rear yard covered porches. A combination of fenestration, bay windows, material changes and dormers may be used in addition to other design elements to achieve the objective.
- b) Side and rear elevations should have a similar design and use materials that are consistent in architectural design and material with those used on front elevations. Architectural detailing such as corbelling should continue from front to side elevations, where visible to the public.
- c) For units flanking onto public spaces, a highly articulated side face is encouraged. Architectural design and material should match the front facade's quality. Side main entrances are an alternate means to achieve this.
- d) The location of porches, windows, and entry doors for units should maximize opportunities for overview and safety.
- e) Projecting porches should emphasize the entrance as well as to reduce the presence of the garage.
- f) Driveways of adjacent homes should be located as far away as possible from public space.

- g) It is encouraged that medium to high density housing typologies are located adjacent or nearby open space so as to provide those residents with access to green space as well as increase the opportunities for social surveillance.

### **Corner Lots**

- a) Side or rear elevations visible from the street should have windows, materials, and other architectural treatments equal in quality to the front elevation of the house.
- b) Corner windows and wrap-around porches should be included to emphasize a corner location. Where possible, the entry door should be located on the exterior side elevation of the house with direct access to the sidewalk.
- c) Townhouse units should be encouraged as bookends to residential blocks.

### **Gateway Corner Lots**

Gateway corner units are typically dwellings that are located at the entry to the community from adjacent areas. These units should be designed with the following principles in mind:

- a) Gateway dwellings should be given special consideration in architectural design, massing, orientation, siting and materials, and shall be of high architectural quality.
- b) Entry elements and porches are encouraged to produce interest in the façade as well as to help define the entrance to the neighbourhood.
- c) Pairing of similar model dwellings on lots directly opposite each other is encouraged to establish and enhance a gateway condition.
- d) It is encouraged that landscaping and landscape features are provided as a means to accentuate the unit and create an inviting atmosphere.

### **T Intersections / Key View Terminus**

T intersections occur when one road terminates at right angles to another. Consideration should be given to homes at the top of the T intersection and the last two lots on either side of the road that terminates at the intersection.

- a) Architecture on lots at the end of T intersections should have facade designs that utilize elements such as coordinated fenestration, masonry detail, and entry elements.
- b) Pairing of side yards is encouraged to form a landscaped area at the terminus of the T intersection.

- c) Buildings sited at the end of the view corridor should be designed with architectural elements that address these views.

#### **18.4.5.8 Green Infrastructure and Building**

The following green infrastructure and building best practices are a compilation of suggested sustainable strategies highly encouraged in the design of River Road's new community.

##### **18.4.5.8.1 Energy Efficiency**

- a) All new developments are encouraged to be built adhering to the following requirements for sustainability:
  - i. Grade related (3 storeys or less) residential buildings achieve a minimum performance level that is equal to ENERGY Star U or equivalent standards; and,
  - ii. Mid-rise (3 storeys) residential buildings shall be designed to achieve greater energy efficiency than the Model National Energy Code for Building.
- b) Where feasible, alternative community energy systems such as geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy should be provided.
- c) Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect.
- d) To minimize energy consumption and encourage the integration of passive building systems, buildings should be oriented to maximize the potential for sunlight and natural ventilation.
- e) The strategic use of deciduous trees is strongly encouraged as part of a free cooling strategy to help with evapotranspiration and shading.
- f) Building design is encouraged to utilize opportunities associated with large expanses of roof areas to implement solar thermal, photovoltaic systems, green roofs or water harvesting systems.
- g) Energy Star compliant or equivalent appliances are encouraged to be provided in cases where the developer provides appliances.
- h) Alternative energy sources such as solar thermal, photo voltaic panels and/or geothermal technologies or the inclusion of "roughed in" facilities to accommodate such alternative energy sources are encouraged to be provided.



#### **18.4.5.8.2 Water Efficiency and Stormwater Management**

- a) All new buildings are encouraged to achieve a 20% or greater water conservation efficiency than that required by the Ontario Building Code.
- b) Irrigation of all public open spaces/structures should implement a rainwater harvesting program, and can include the use of rain barrels, and/or infiltration trenches provided water balance objectives are met.
- c) Grade-related residential unit driveways should be paved with permeable materials to reduce stormwater run off where supported by agencies.
- d) At site-plan level, developments with five or more dwellings should be encouraged to increase the level of perviousness in order to promote at-source stormwater management, reduce peak flows and lessen the dependence on end-of-pipe facilities such as stormwater management ponds. Pervious areas can include landscaped areas and/or areas containing permeable paving.
- e) In order to reduce the volume of run-off into the storm drainage system, surface water run-off flows should be directed to landscaped areas and the use of hard surfaces should be minimized.
- f) Innovative sustainable technologies in the capture, conveyance, and treatment of storm run-off to reduce potential pollutants/ contaminants are encouraged.
- g) New residential units should be designed to incorporate gray water pipe infrastructure, where permitted, in accordance with the Ontario Building Code and health regulations.
- h) Indoor water usage should be minimized in new buildings through the installation and use of water-efficient fixtures, fittings and appliances, such as dual-flush toilets, faucets, and shower heads.
- i) Low maintenance and drought-resistant landscaping is encouraged.
- j) To conserve and protect the area's natural heritage features, stormwater runoff from all new developments shall be collected and treated to an Enhanced standard (as per Ministry of Environment, Conservation and Parks guidelines).
- k) In order to avoid the negative impacts of flooding in the downstream watershed, all new development post development peak stormwater flows shall be attenuated to pre-development conditions for up to and including the 100 year return period design storm event.
- l) All downspouts from residential development shall discharge to grade.
- m) Adequate and safe minor and major storm conveyance systems shall be required for all new development.

- n) Low Impact Development methods will be strongly encouraged to be utilized in this new community in conjunction with traditional stormwater management techniques.
- o) The target is to capture, detain, utilize, or infiltrate all stormwater runoff from all storms up to the 25mm design storm event where soil conditions permit.
- p) Increasing the size and capacity of the major and minor storm systems, and the associated stormwater management systems and facilities, should be considered in order to increase the community's level of resilience against the impacts of climate change (i.e., severe storm events).

#### **18.4.5.8.3 Material Resources and Solid Waste**

- a) A minimum of 25% recycled/reclaimed materials is encouraged to be used for new infrastructure including roadways, parking lots, sidewalks, unit paving, stormwater management facilities, sanitary sewers, and/or water pipes.
- b) All buildings should utilize best practices for design and construction techniques in order to reduce the amount of construction waste produced.
- c) Where feasible, streetscape and building materials should be chosen and determined by suitability, durability, ease of maintenance, and cost effectiveness.
- d) Green building materials should be used to reduce impacts on the environment.
- e) Building materials should be purchased and/ or obtained from responsible, ethical, and whenever possible, local sources.
- f) The use of recycled and reclaimed material for new buildings is encouraged in order to reduce the negative environmental effects of extracting and processing materials.
- g) Waste volumes should be reduced through the provision of recycling and/or re-use and compost stations as well as drop off points for potentially hazardous waste.

#### **18.4.5.8.4 Lighting**

- a) Lighting should be downcast to reduce light pollution and address night sky condition.
- b) Exterior lighting spill-over onto the street or adjacent properties should be minimized.
- c) High efficiency street lighting should be used so as to reduce energy consumption by at least 15% of the baseline annual energy use.
- d) For residential buildings, external lighting should incorporate lighting controls that use motion sensors and/or timers to improve energy efficiency.

#### **18.4.5.8.5 Sustainable Program**

Sustainable programs should encourage opportunities for community-based initiatives such as car pooling, community composting, waste reduction, educational, and stewardship programs. Some examples of such programs are as follows:

- a) Education Packages
  - i. Owner/tenant education packages regarding household activities to conserve household energy and water resources, access to transit, recycling and composting programs, and depots should be provided at the time of purchase or rental.
  - ii. Encourage homeownership afford-ability for low-income families through the provision of sites for non-profit or charitable Homeownership Programs.
- b) Mobility Programs
  - iii. Carpooling or car share programs are encouraged for residents of multi dwelling residential buildings. Dedicated parking spaces for carpooling or car share programs should be provided with clear signage.

#### **18.4.5.8.6 Innovative Design**

In order to encourage exemplary performance above the requirements set out in this document, innovative design is strongly encouraged and should be recognized.

Innovative elements and performance should identify the intent of the proposed innovation, while demonstrating and describing the design approach and strategies utilized to achieve sustainable performance measures that exceed those set out in this document.

### **18.5 Servicing Strategy**

#### **18.5.1 Water and Sanitary Servicing**

All development in the River Road Secondary Plan area shall occur on full municipal water and wastewater services. As part of the implementation of this Secondary Plan, the City will work with the Region of Waterloo to ensure that there is adequate water and sanitary servicing capacity to accommodate the long term planned development for the River Road Secondary Plan Area. The River Road Neighbourhood Plan Functional Servicing Assessment (June 2021) completed in conjunction with this Secondary Plan has provided a long-term servicing strategy for providing water servicing and gravity sanitary sewer outlets for developments within the study area.

### **18.5.2 Municipal Servicing**

As part of the implementation of this Secondary Plan, and the policies of Chapter 6 of this Official Plan, the City will undertake an analysis of local infrastructure to ensure that adequate servicing is in place to accommodate the planned growth for the area. The River Road Neighbourhood Plan Functional Servicing Assessment (June 2021) has provided both interim and long-term servicing strategies for the study area.

### **18.5.3 Development Applications and Servicing Requirements**

The City may also require development applications to be supported by site-specific servicing studies.

### **18.5.4 Sustainable Storm Water Management**

The municipality encourages innovative measures to help reduce the impacts of urban run-off and maintain base groundwater flow. Such measures may include bioswales, permeable pavers, rain barrels, green roofs and green buildings.

### **18.5.5 Coordination of Public Work**

The City will work with the Region to ensure that planned public works for the area are coordinated to minimize the impacts of construction on the residents within the Secondary Plan Area. Coordination efforts will consider the phasing of streetscape improvements, any future road works and maintenance, as well as any upgrades to water and sanitary networks.

## **18.6 Transportation Strategy**

### **18.6.1 Street / Active Transportation Structure**

The vision for the Street / Active Transportation Structure on Schedule 3 is to deliver complete streets designed to enable safe access for all users with particular emphasis on vulnerable road users in accordance with City standards with a clear road hierarchy based on a connected modified grid network that accommodates for all modes of travel fundamental in the implementation of an identifiable, great community.

### **18.6.2 Objectives**

The objectives of the transportation network are to:

- a) Develop a well-connected integrated network of streets, paths and trails that enhances connectivity and safely accommodate various modes of active transportation, including walking and cycling.

- b) Design the community at the pedestrian scale to promote social interaction, active streetscapes and a walkable neighbourhood;
- c) Preserve River Road's character through the implementation of a special land use character (Village Residential) zone and a specific road solution based on a modified functional rural cross section;
- e) Incorporate traffic calming measures, where warranted, and pedestrian crossings within the neighbourhood and along River Road;
- f) Facilitate the implementation of an interconnected road network extending to adjacent neighbourhood areas;
- g) Distribute traffic to the surrounding road network;
- h) Minimize traffic impacts on adjacent communities; and
- i) Minimize impacts on the surrounding natural features.

### **18.6.3 General Policies**

- a) The planning, design and construction of all roads and walkways within the River Road Neighbourhood will have regard to the conservation of significant natural and cultural heritage features and contribution to the overall streetscape design.
- b) The design of the transportation network will provide for all forms of travel including walking, cycling, public transit and the automobile through:
  - i. The creation of a modified-grid street network with short walk-able blocks that are generally not to exceed 250m in length;
  - ii. The use of site planning and urban design techniques that foster attractive and safe pedestrian-friendly streetscapes and built form;
  - iii. The implementation of a River Road unique functional rural cross section that includes active transportation amenities in accordance with Figures 2 and 3;
  - iv. The provision of continuous pedestrian systems (sidewalks) and linkages to community trails and open spaces; and
  - v. The provision of multi-use pathways including cycling facilities, improved public and private pedestrian amenities, well planted greenways, and uses which encourage walking.
- c) The implementation of Transportation Demand Management measures, to reduce the reliance on vehicle usage and encourage active transportation, shall be considered as part of every application for new development or redevelopment within the Secondary Plan area.

- d) Development applications may be required to provide Transportation Impact Studies to the satisfaction of the City and Region.

#### **18.6.4 Streetscape Policies**

Streetscapes throughout the River Road community are important components of the public realm. Streetscapes will be designed to enhance community character and sense of place. Streetscape design shall be focused along the community's public and private streets to include: River Road, McMeeken Drive, Alderson Drive and any other new local public and/or private streets.

- a) A high-quality public realm along streets will be achieved by coordinating the design of individual site elements, the overall transportation network and individual development applications along the streetscape.
- b) Backlotting along the three primary streets (River Road, McMeeken and Alderson Drives) will be prohibited. On corner lots, the main building face shall be oriented towards the higher-order street.
- c) Residential streetscapes shall be designed to ensure the provision of sufficient on-street parking through creative design solutions such as varying housing types and lotting patterns.
- d) Streets shall be designed to be more pedestrian friendly through various means, including human scale massing of built form along the street, pedestrian-scale lighting, landscape treatments and street furniture. Streetscape plans for all public and private streets shall be required as a condition of draft plan approval.
- e) Local streets shall be designed to encourage on-street cycling safety consistent with the City's Cycling Master Plan.

##### **18.6.4.1 River Road**

As one of the most iconic scenic routes in the City of Cambridge, River Road is envisioned as a future pedestrian and cyclist friendly corridor. Its design is to:

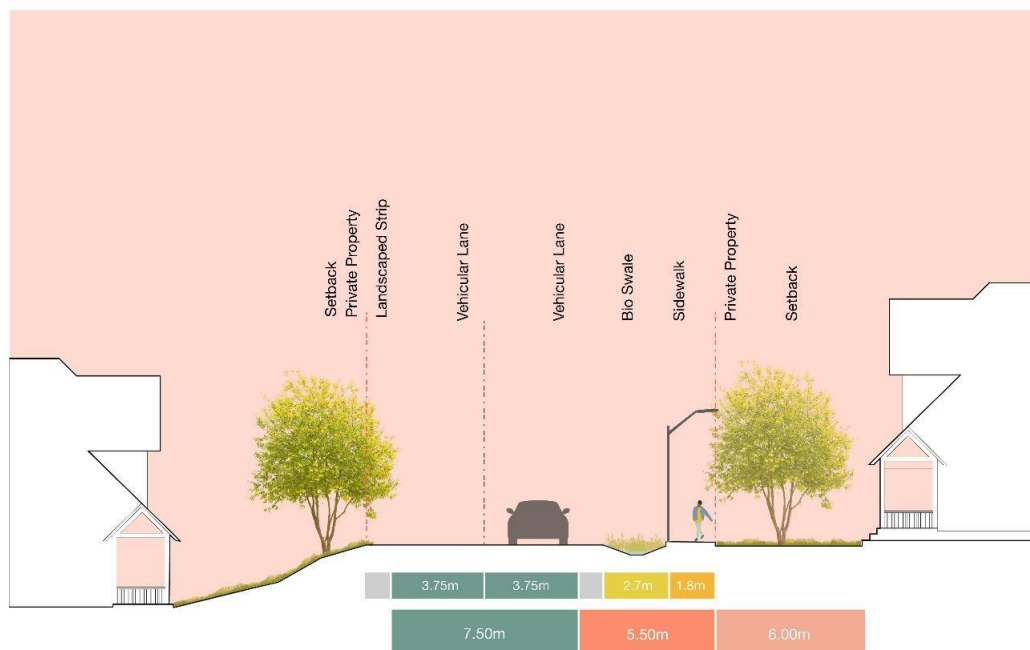
- a) Accommodate for an east multiuse trail accessible to pedestrians and cyclists;
- b) Explore the implementation of a rural cross section with deciduous trees and swales as an essential component of its engineering and its streetscape character;
- c) Be framed by a relatively consistent building edge in order to provide spatial definition and containment to the street consistent with historic lotting patterns as per Zoning By-law directives; and,
- d) Implement a utility strategy where utilities are clustered or grouped where possible to minimize visual impact.

The preliminary design of River Road's future cross section is based on the intent of preserving the road's rural scale while ensuring a sustainable storm water management strategy, servicing upgrades (as needed) and a safe and comfortable active transportation route (i.e., pedestrian and cycling trail) are part of the road's ultimate design. The preliminary road solution is based on a modified functional rural cross section to be implemented as development occurs. Future road enhancements will require an average of an additional six (6.0) metres to be added to the right of way.

### Phase 1, Short Term (current 14.0m right of way) - Figure 2: River Road Interim Cross Section

- a) Maintain the road's rural character (i.e., slopes shoulders) and a 7.5m pavement width.
- b) Any upgrades to River Road should include the replacement of the 3-cable guiderail system along the embankment.
- c) Ensure sustainable water management measures are introduced as part of the interim road upgrades such as the illustrated bioswale.
- d) Introduce a 1.8m pedestrian (AODA complaint) sidewalk on the east side of the road.
- e) The proposed interim upgrades (i.e., sidewalk and bioswale) are important visual traffic calming elements. Further opportunities to buffer pedestrians from travelling vehicles should be explored to include the planting of native grasses (along the bioswale) and/or street trees where feasible.

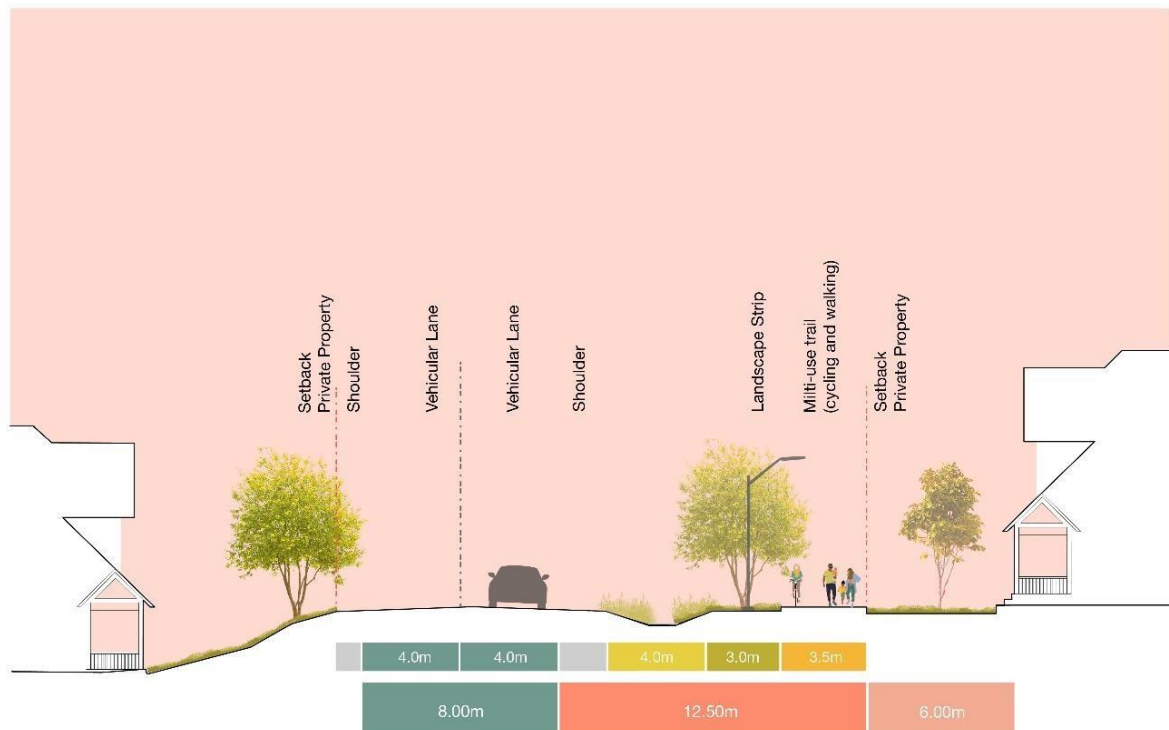
**Figure 2: River Road Interim Cross Section**



## Phase 2, Mid to Long Term (20m right of way) - Figure 3: River Road Ultimate Cross Section

- a) Maintain the road's rural character (i.e., sloped shoulders) and widen pavement to 8m.
- b) Ensure sustainable water management measures remain as part of the road design solution by widening the interim bioswale to comply to City standards (refer to 20m rural cross section City standard C108).
- c) Introduce a 3.5m combined pedestrian and cyclist trail on the east side of the road.
- d) The proposed ultimate upgrades (i.e., multiuse trail and bioswale) are important visual traffic calming elements. Further opportunities to buffer pedestrians from travelling vehicles should continue to be explored to include the planting of native grasses (along the bioswale) and street trees immediately adjacent to the multiuse trail as further pedestrian and cyclist protection.
- e) Introduce a street lighting strategy primarily focused on illuminating the multi-use trail. The future street lighting strategy will need to ensure no light spills over the westerly side of the road along the wetland areas.

**Figure 3: River Road Ultimate Cross Section**





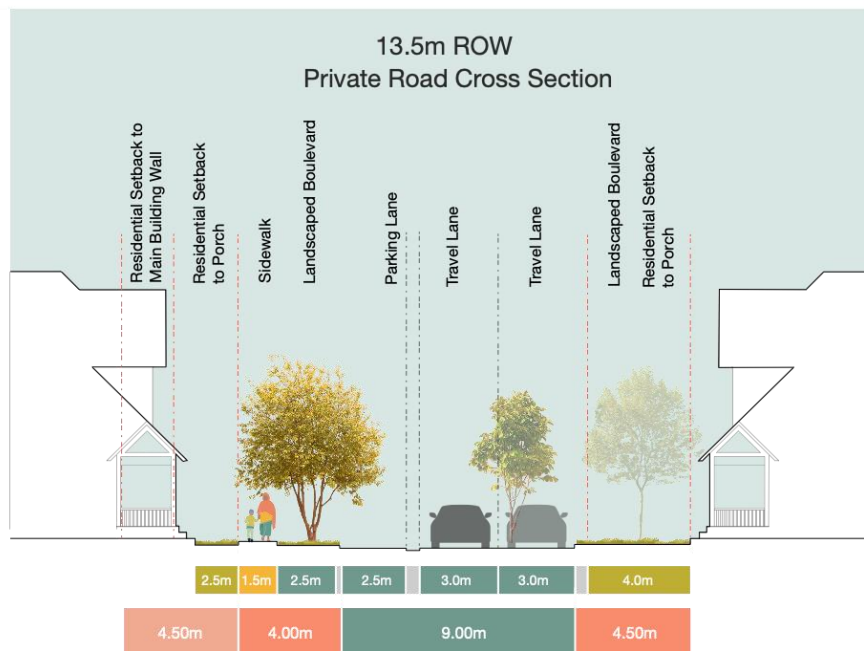
### 18.6.5 Local Roads

- a) The local street network should facilitate vehicle, cycling and pedestrian connections throughout the neighbourhood.
- b) The local street network should be based on a modified grid form or other road pattern that results in a high level of connectivity. A discontinuous street pattern and dead-end streets are discouraged.
- c) All local roads will have a minimum width of 18.5 metres.
- d) Pedestrian sidewalks shall be provided on both sides of all local streets.
- e) On-street parking is encouraged on one side of the street.

### 18.6.6 Private Roads

- a) Private roads are permitted in specific conditions subject to City's standards and to the approval of the City of Cambridge.
- b) Private roads should facilitate vehicle and cycling connections and must provide for pedestrian links throughout the neighbourhood.
- c) When present, private roads are an integral component of a neighborhood by providing for a continuous street system. A discontinuous street pattern and dead-end streets are discouraged.
- d) An on-street parking lane is not necessarily required if suitable dedicated visitor parking areas are provided within the development site to the City's satisfaction.

**Figure 4: Private Road Cross Section**



### **18.6.7 Active Transportation**

- a) A network of bicycle routes and multi-use trails will be provided throughout the community.
- b) Conceptual multiuse trail and on street routes and linkages are shown in Schedule 3.
- c) River Road should provide for an off-road pedestrian and cycling route. Multiuse trail facilities should be designed to be consistent with the City of Cambridge Bikeway Network plan.
- d) The local road network should provide for on road cycling routes. On road cycling facilities should be designed to be consistent with the City of Cambridge Bikeway Network Plan.
- e) Sidewalks will be provided on both sides of all streets located internal to the community.
- f) Priority pedestrian crossings will be further defined and implemented through the plan of subdivision approval process.
- g) Traffic calming measures should be coordinated with pedestrian crossings and other elements of the active transportation network as shown in Schedule 2. Traffic calming measures should be shown on a consolidated plan in support of draft plans of subdivision with special emphasis given to pedestrian connections to planned and existing open space amenities as well as a safe pedestrian crossing.

## **18.7 Implementation**

### **18.7.1 General Implementation**

The River Road Secondary Plan shall be implemented through a variety of tools, including but not limited to:

- a) the planning and development application process, through tools such as site plan approval, plans of subdivision and condominium, and land severances;
- b) The City shall amend the site plan control by-law to include new development on properties fronting River Road.
- c) The City shall acquire drainage easements over existing stormwater outlets to the Speed River
- d) the City of Cambridge Transportation Master Plan;
- e) the City of Cambridge Zoning By-law;
- f) the City's Parks Master Plan (to be completed);
- g) the City's Cycling Master Plan; and

h) other tools as described in this Chapter.

### **18.7.2 Municipal Works**

All future municipal works undertaken by the City of Cambridge within the Secondary Plan Area shall be consistent with the policies of this Plan and the Region of Waterloo's Official Plan.

### **18.7.3 Amendments**

Unless otherwise stated in this Plan or the City's Official Plan, applications for development which do not align with the Policies or Maps of this Plan shall require an Official Plan Amendment. Amendments to the Official Plan shall be subject to policies of the Official Plan and shall require a planning justification report, along with any other supporting studies identified through the pre-consultation process.

### **18.7.4 Zoning By-law**

#### **18.7.4.1 Alignment with Zoning By-law**

The City will update its zoning by-law to ensure that the land use and design policies for this Secondary Plan are reflected in the City's zoning by-law.

#### **18.7.4.2 Amendments to Zoning By-law**

Applications for development within the Secondary Plan Area shall be subject to the policies of this Secondary Plan and the City's Official Plan (where applicable). Amendments to the zoning by-law shall be subject to policies of the Official Plan/Secondary Plan and shall require a planning justification report, along with any other supporting studies identified through the pre-consultation process.

### **18.7.5 Site Plans, Plans of Subdivision and Condominium and Severances**

Applications for site plan review, plans of subdivision, plans of condominium and land severances to sever shall be consistent or conform with Provincial policies, and the policies of this Secondary Plan, City of Cambridge's Official Plan and the Regional Official Plan. All forms of development fronting on River Rd. shall require site plan approval and the City shall amend the existing site plan control by-law.

### **18.7.6 Supporting Studies**

Prior to site plan approval, the completion and implementation of various studies may be required as conditions of a site plan or subdivision agreement depending on the location and nature of the development proposal. Studies may include but are not limited to: Scoped Environmental Impact Statements; Tree Management Plans; Hydrogeological

Studies; Chloride Impact Studies and Salt Management Plans; Spill Prevention, Response and Contingency Plans; Stormwater Management Reports; Traffic Impact Studies; Functional Servicing; and/or Stationary Noise Studies. Any required on-site implementation measures identified in the study will be secured as part of a site plan agreement with the City.

#### **18.7.7 Coordination with the Region**

The City will work with the Region of Waterloo, who is the approval authority for this Plan, to ensure that the policies of this Plan are implemented, including any opportunities to implement the urban design and public realm improvements through any future Regional works. The City also encourages the Region of Waterloo to consider opportunities for affordable housing development within the Secondary Plan area.

#### **18.7.8 Phasing and Financial Tools**

##### **18.7.8.1 Municipal Capital Improvements**

The City will prepare a phasing strategy to assist with the implementation of this Secondary Plan and more particularly to any streetscape and/or servicing improvements to River Road. The Phasing Strategy should consider the following:

- a) The expecting timing of development, including the expected built-out of vacant lands and redevelopment of existing areas;
- b) The timing and funding approach for River Road's infrastructure and public realm improvements; and,
- c) Any other projects or initiatives which may impact the timing of development.

##### **18.7.8.2 Development Charges**

The City will include any growth-related infrastructure identified in this plan as part of the next Development Charges By-law update in accordance with the City's local service policy.

##### **18.7.8.3 Parkland Dedication**

The new park as noted in the maps, will be acquired through parkland dedication, purchase of lands, cash-in-lieu of parkland dedication, and/or expropriation if necessary. A minimum park size of 1 ha is generally pursued but given constraints in the area, and the size and location of the preferred park site, the City will accept an approximately 0.7 ha park. Cash-in-lieu of parkland dedication will be required from all other Secondary Plan subject sites as they develop.

### **18.7.9 Monitoring**

The City will prepare a monitoring program to track the implementation of the Secondary Plan and report on the progress of its implementation. The monitoring program should identify development statistics for residential intensification and the status of the various actions identified in this Plan (such as the upgrades to River Road).

The Secondary Plan monitoring should be undertaken in conjunction with other monitoring activities described in the Secondary Plan chapter of the Official Plan.

### **18.7.10 Updating the Plan**

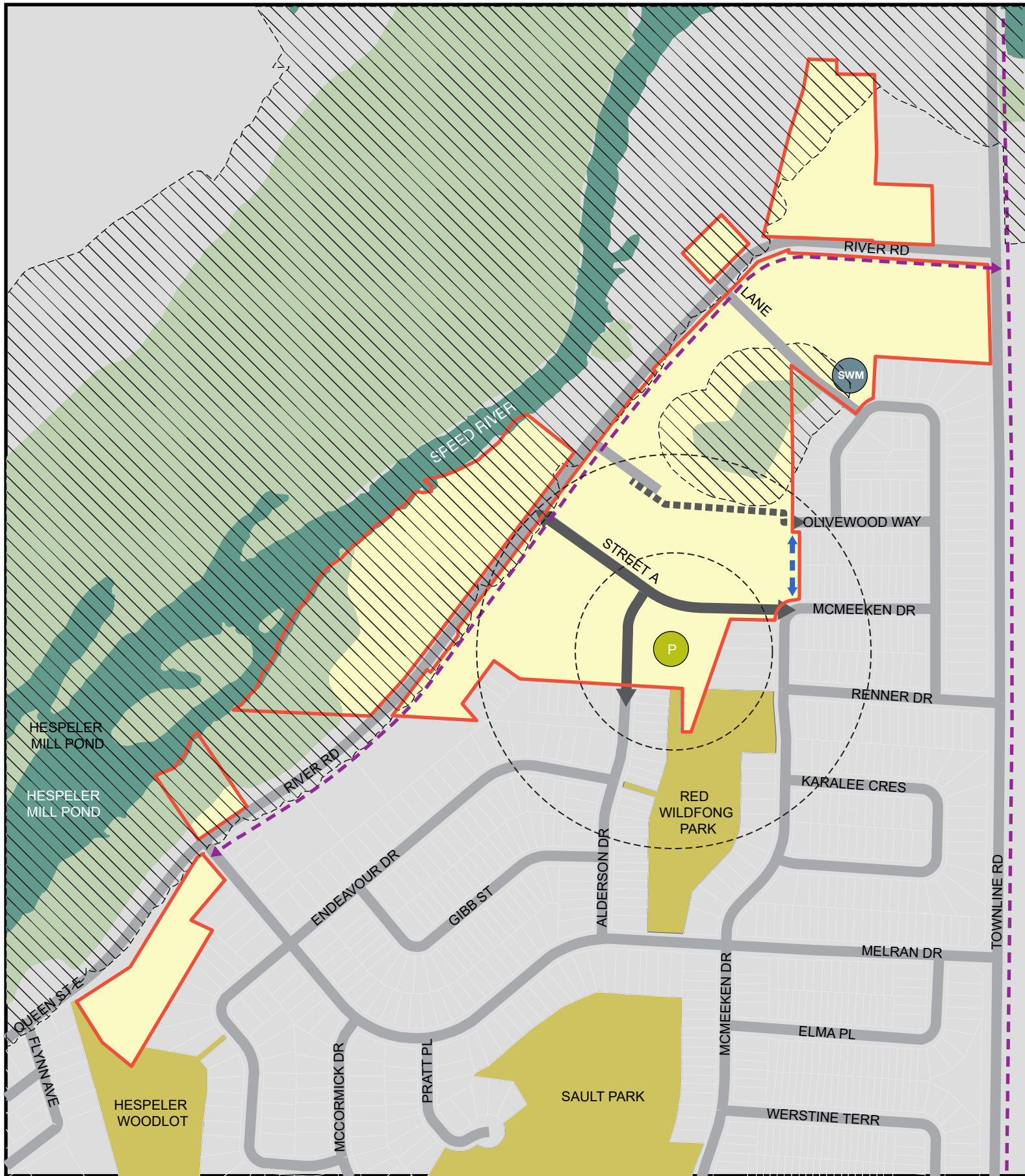
The City will comprehensively review the policies of this Secondary Plan at the 10-year review of the City's Official Plan. Depending on the outcomes of the review, the City may decide to update the Plan.

### **18.7.11 Conflicts with Official Plan**

In the event of a conflict between the City of Cambridge's Official Plan and this Secondary Plan, the policies of the Secondary Plan shall prevail.

### **18.7.12 Boundaries**

The boundaries shown on the Secondary Plan Maps are approximate, except where they meet with existing roads, river valleys or other clearly defined physical features. Where the general intent of this Secondary Plan is maintained to the satisfaction of the Municipality.

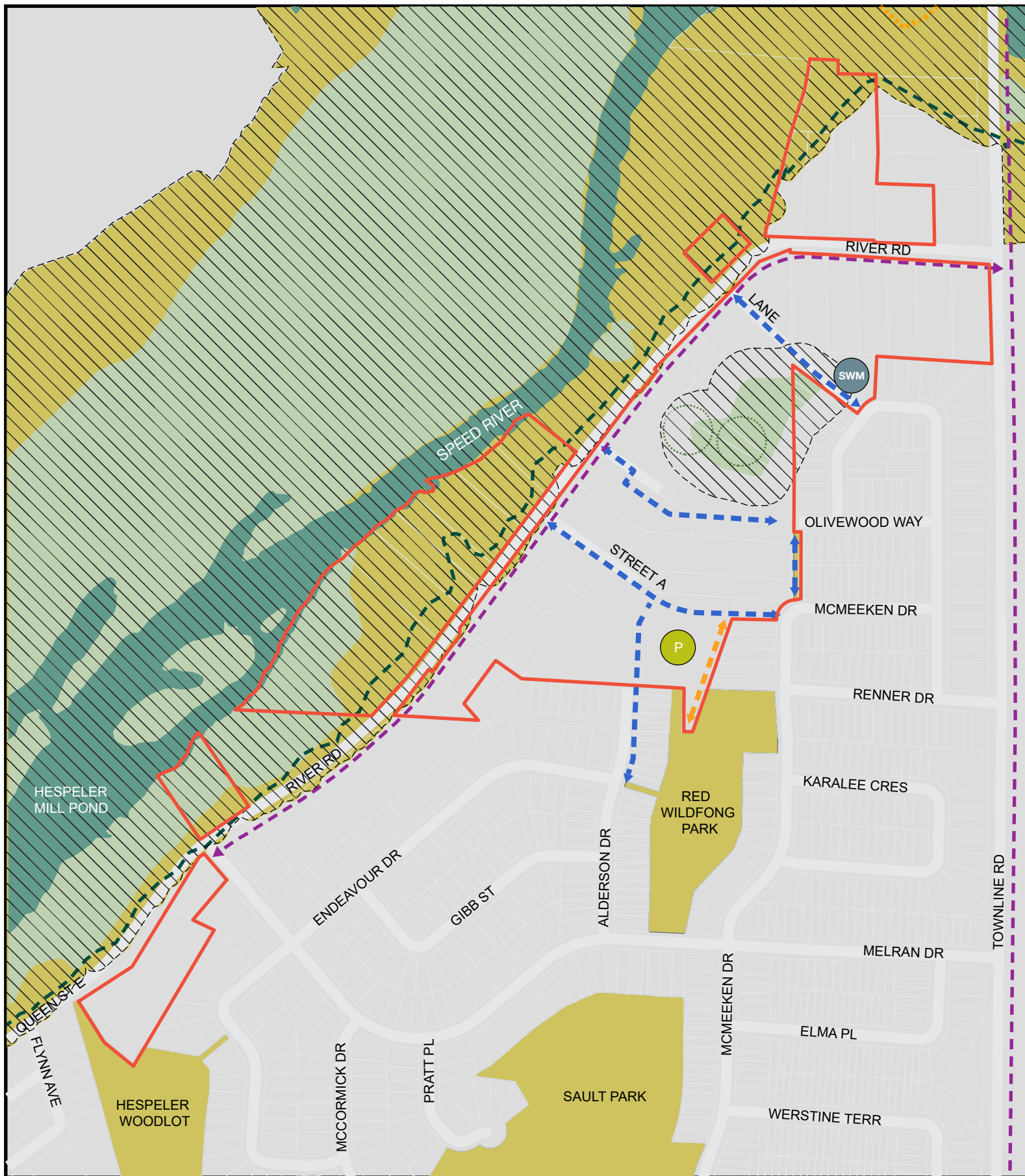


## Schedule 1 Neighbourhood Structure



1:5,000

- Residential Neighbourhoods
- Existing Natural Open Space System
- Existing Recreational, Parks/Open Space
- Proposed Open Space/Park
- Stormwater Management
- Proposed Multi-Use Trail Network
- Existing Roads
- Proposed Local Municipal Road
- Proposed Private Road
- Pedestrian Connection
- Walking Radius (200-400m)
- River Road Secondary Plan Boundary
- Special Policy Area



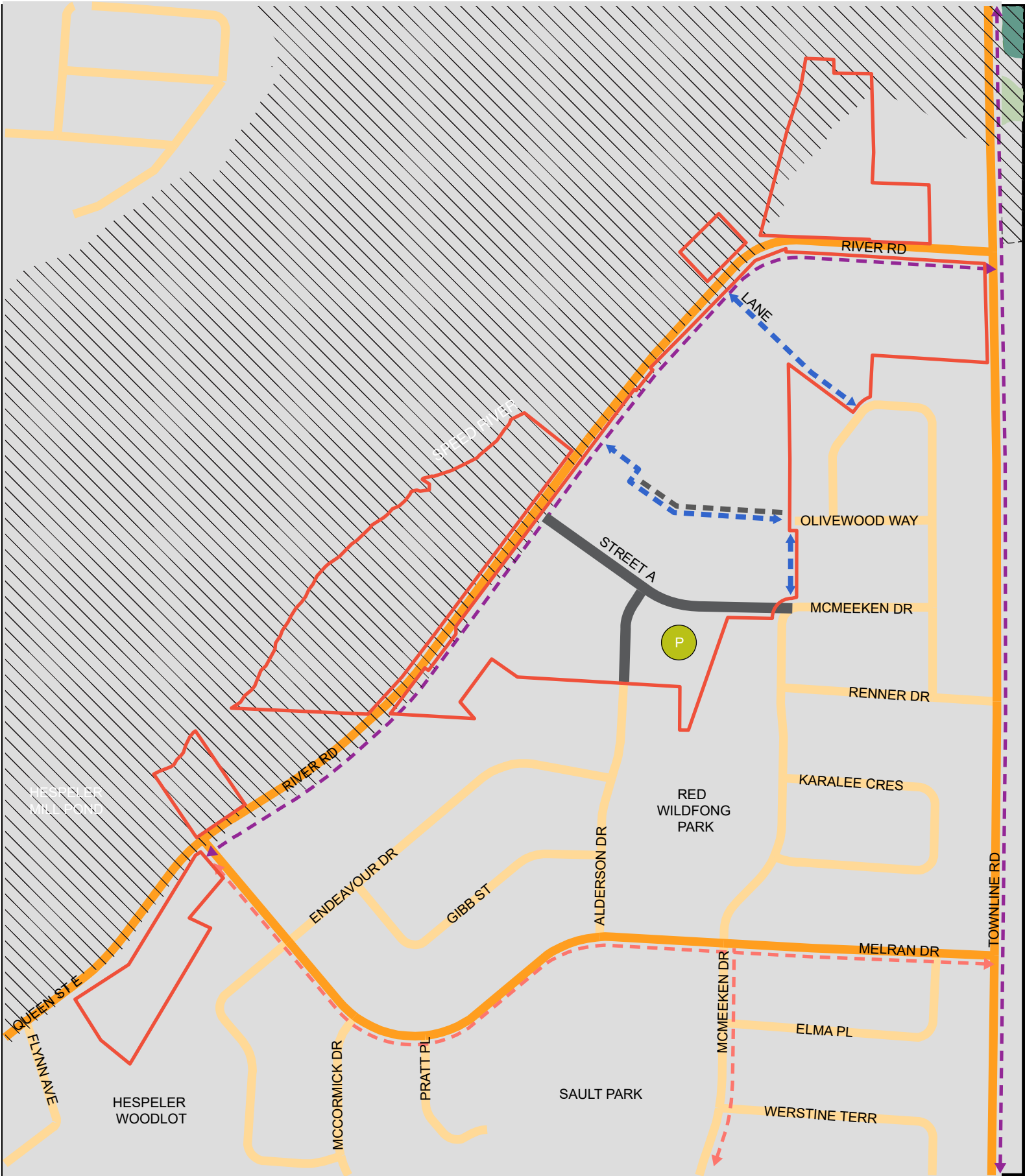
## Schedule 2 Open Space Structure



1:5,000

- Regulated Wetland/Environmental Feature(s)
- Regulatory Storm Floodplain (GRCA)
- Regulated Watercourse (GRCA)
- Special Policy Area Overlay
- Butternut Tree Buffer (25.0m buffer)
- Proposed Multi-Use Trail Network

- Existing Open Space/Parkette
- Proposed Open Space/Park
- Pedestrian Connection
- Trail Connection
- Stormwater Management
- Significant Valley Lands (ROP)
- River Road Secondary Plan Boundary



## Schedule 3

### Street/ Active-Transportation Structure



1:5,000

- Collector Road
- Local Road
- Proposed Local Municipal Road
- Proposed Private Road
- Proposed Bikeway Network
- Proposed Open Space/Park

- Proposed Multi-Use Trail Network
- Proposed Pedestrian Connections
- /// Special Policy Area
- Butternut Tree Buffer (25.0m buffer)
- Regulated Wetland/Environmental Feature
- River Road Secondary Plan Boundary





Attachment No. 2 – Recommended Zoning By-law Amendment

## **AMENDMENT TO THE CITY OF CAMBRIDGE ZONING BY-LAW**

### **PURPOSE AND EFFECT**

The River Road Neighbourhood is located in the Hespeler community and is generally bounded by: Townline Road to the east; River Road and the Speed River to the west and north; and Melran Drive to the south. The River Road Secondary Plan area includes a series of lands along River Road between the Brewster Trail (Flynn Avenue) and River Road, to Townline Road and River Road. The total area of the Secondary Plan area is approximately 19.0 hectares (47 acres).

The River Road area is within the designated Urban Area in the City of Cambridge. The River Road Secondary Plan establishes a vision, principles and policies for the design and development of the River Road area. It also establishes the general land use patterns and conceptual location of community infrastructure such as parks and trails, roads, and services.

The purpose of the Zoning By-law Amendment is to implement the River Road Secondary Plan. New land use designations, specific to the River Road Secondary Plan area, are proposed including Village Residential and Neighbourhood Residential designations.

The subject lands are presently zoned R2 (Residential Type 2) and (H)R4 (Residential Type 4 with a Holding Zone) by the City of Cambridge Zoning By-law 150-85. A schedule showing the zoning is attached. The zoning will implement the proposed official plan amendments. The following zones are proposed: R2, R5 and RM4.

**BY-LAW NO. \_\_\_\_\_-21**

**of the**

**CITY OF CAMBRIDGE**

Being a by-law of the Corporation of the City of Cambridge to amend the City of Cambridge Zoning By-Law pursuant to Sections 34 and 36 of the Planning Act (R.S.O. 1990, c. P.13, as amended)

WHEREAS the City of Cambridge Official Plan provides for the lands affected by this by-law to be zoned as set forth in this by-law;

AND WEREAS this By-law conforms to the City of Cambridge Official Plan; and,

AND WHEREAS Council deems that adequate public notice of the public meeting was provided and adequate information regarding the amendment was presented at the public meeting held April 27, 2021, and that further public meeting is not considered necessary in order to proceed with this amendment.

NOW THEREFORE THE MUNICIPAL COUNCIL OF THE CORPORATION OF THE CITY OF CAMBRIDGE enacts as follows:

1. THAT this by-law shall apply to the lands outlined in heavy black line on Schedule "A" to By-law **XX-21**, in the City of Cambridge, Regional Municipality of Waterloo
2. THAT the City of Cambridge Zoning By-Law, being Schedule A to By-Law No. 150-85, is hereby amended by changing the zoning classification of the lands shown outlined by a heavy black line on Schedule A hereto attached from R2 and (H)R4 to R2, R5, RM4, and RM4(H).
3. THAT the City of Cambridge Zoning By-law, being Schedule A to By-law 150-85, is hereby further amended by adding the notation "(s.4.1.415)" to the R2, R5 and RM4 zone symbols;
4. AND FURTHER THAT the aforesaid City of Cambridge Zoning By-law, as amended, is hereby further amended by adding the following subsection to section 4.1 thereof:

**"4.1.415 River Road**

1. Notwithstanding the provisions of sections 2.1.15, 2.2.4.5, 3.1.1.4A, 3.1.1.10, 3.1.2.1, 3.1.2.2, 3.1.2.3, 3.1.2.5 of this by-law and in addition to section 1.1.1, the following regulations shall apply to the lands in those R2, R5 and RM4 zone classifications to which reference to "(s.4.1.415)" is made on Zoning Map C14 attached to and forming part of this by-law:

## Definitions

Stacked, back-to-back townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit,

- (a) is divided both vertically and horizontally from another dwelling unit by a common wall;
- (b) has independent external access; and,
- (c) shares a common rear exterior wall.

Stacked townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit is divided both horizontally and vertically from another dwelling unit by a common wall and in which each dwelling unit has an independent external access.

Back-to-back townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit,

- (a) is divided vertically from another dwelling unit by a common wall;
- (b) has independent external access; and,
- (c) shares a common rear exterior wall.

Uses Permitted <sup>(A)</sup>	In Zone		
	R2	R5	RM4
Detached dwelling	♦	♦	
Semi-detached dwelling	♦	♦	♦
Triplex dwelling	♦	♦	♦
Semi-detached duplex dwelling			♦
Linear row house dwelling			♦
Stacked townhouse dwelling			♦
Back-to-back townhouse dwelling			♦
Stacked back-to-back townhouse dwelling			♦
Residential special care facility	♦	♦	
A use permitted in any zone in accordance with section 2.1.1	♦	♦	♦

Uses Permitted <sup>(A)</sup>	In Zone		
	R2	R5	RM4
An accessory use, building or structure in accordance with section 2.1.11	♦	♦	♦

(A) No building or structure, except for a single detached dwelling and/or accessory structure, shall be constructed unless serviced by full municipal water and sewer services.

ZONE STANDARDS		R2 Detached and Triplex	R2 Semi- Detached
Minimum Lot Frontage	m	30	15/30 <sup>(A)</sup>
Maximum Lot Frontage	m	35	20/35
Minimum Corner Lot Frontage	m	30	20/35 <sup>(A)</sup>
Maximum Corner Lot Frontage	m	35	20/35 <sup>(A)</sup>
Minimum Lot Area	m <sup>2</sup>	900	450/900 <sup>(A)</sup>
Minimum Corner Lot Area	m <sup>2</sup>	900	600/1,050 <sup>(A)</sup>
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre	
Minimum Front Yard to House	m	(B)	(B)
Maximum Front Yard to House	m	(C)	(C)
Minimum Interior Side	m	5.0	5.0
Minimum Exterior Side Yard	m	7.0	7.0
Minimum Rear Yard	m	7.5	7.5
Maximum Building Height	m	9	
Maximum Number of Storeys		2	

ZONE STANDARDS		R2 Detached and Triplex	R2 Semi- Detached
Minimum Gross Floor Area	m <sup>2</sup>	120	75
Maximum Lot Coverage	% of lot area	40	40
Minimum Landscaped Open Space		35	35

- (A) One attached Dwelling Unit / Two Attached Dwelling Units
- (B) The Minimum Front Yard to House shall be 1.5 metres less than the average of the Minimum Front Yard Setback of dwellings on adjacent lots located within 60 metres along the same side of the road, but shall not be less than 6 metres.
- (C) The Maximum Front Yard to House shall be 1.5 metres greater than the average of the Maximum Front Yard Setback of dwellings on adjacent lots located within 60 metres along the same side of the road, but shall not be greater than 12 metres.

ZONE STANDARDS		R5 Detached	R5 and RM4 Semi- Detached and Triplex
Minimum Lot Frontage	m	12	9/18 <sup>A</sup>
Minimum Corner Lot Frontage	m	17	13/22 <sup>A</sup>
Minimum Lot Area	m <sup>2</sup>	270	270
Minimum Corner Lot Area	m <sup>2</sup>	450	450
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre	
Minimum Front Yard to House	m	6.0	6.0
Minimum Interior Side Yard	m	1.2	1.2

ZONE STANDARDS		R5 Detached	R5 and RM4 Semi- Detached and Triplex
Minimum Exterior Side Yard	m	6.0	6.0
Minimum Rear Yard	m	7.5	7.5
Maximum Building Height	m	11 (3 storeys)	
Minimum Gross Floor Area	m <sup>2</sup>	75	75
Maximum Lot Coverage	% of lot area	50	50
Minimum Landscaped Open Space		30	30

(A) One attached Dwelling Unit / Two Attached Dwelling Units

ZONE STANDARDS		RM4 Linear Row	RM4 All Types of Townhouse
Minimum Lot Frontage Rear	m	5.5	N/A
Minimum Lot Frontage Front Garage	m	6.0	30
Minimum Corner Lot Frontage	m	11.6	N/A
Minimum Lot Area	m <sup>2</sup>	165	165 per unit
Minimum Corner Lot Area	m <sup>2</sup>	285	N/A
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre	N/A
Minimum Front Yard to House	m	5.0	5.0



ZONE STANDARDS		RM4 Linear Row	RM4 All Types of Townhouse
Minimum Front Yard to House Rear Garage Condition	m	3.0	3.0
Minimum Rear Setback to Rear	m	1.0	1.0
Minimum Interior Side	m	1.5 or 3.0 <sup>A</sup>	3.0
Minimum Exterior Side Yard	m	4.5	4.5
Minimum Rear Yard	m	7.5	N/A
Minimum Rear Yard between Rear Garage and Dwelling	m	6.0	N/A
Maximum Building Height	m	11 (3 storeys)	
Minimum Gross Floor	m <sup>2</sup>	70 per unit	60 per unit
Minimum private amenity area per unit	m <sup>2</sup>	N/A	30
Maximum number of attached units		6	N/A
Maximum Lot Coverage	% of lot area	60	60
Minimum Landscaped Open Space		30	30

- (A) Minimum interior side yard increased to 3.0 metres if such interior side yard abuts an R-class or RS-class zone

GENERAL PROVISIONS	
Permitted Encroachments – open or covered unenclosed porches, patios, decks not exceeding 3 metres in height	<p>1.5 metres into the required front yard on an interior lot and 1.5 metres into the minimum required front and exterior side yards on a corner lot</p> <p>2.5 metres into the minimum rear yard including eaves and</p>

GENERAL PROVISIONS	
	cornices
Setback of access driveway from the intersection of two street	6.0 m
A private garage shall not project beyond the front of a porch	
<p>The following regulations shall apply to the dwellings fronting onto River Road:</p> <ul style="list-style-type: none"> <li>a) Unless the garage is located in the rear yard, the maximum width of a garage on a lot larger than 15.0 metres, measured from outside walls shall be 25% of the front wall of the building.</li> <li>b) Unless the garage is located in the rear yard, the maximum width of a semi-detached garage, measured from outside walls shall be 35% of the front wall of the building.</li> <li>c) The width of an attached garage or carport for a detached one-family dwelling, or a semi-detached one-family dwelling may not exceed 20% of the width of the lot measured at the front building wall and parallel to the front lot line to a maximum width of 7.0 metres and the inside of an attached garage or carport shall have a minimum size of 2.9 m by 5.5 m.</li> <li>d) A driveway and its widening shall not exceed 50% of the lot width, or 6 metres, whichever is lesser.</li> <li>e) Garage and parking areas for triplexes shall be located at the rear of the property (behind main building).</li> <li>f) A maximum of one driveway with one access point from a street or lane shall be permitted on a lot.</li> <li>g) A driveway shall have a minimum width of 2.6 metres.</li> <li>h) A driveway that is longer than 6.0 metres shall not exceed 3.5 metres in width within 6.0 metres of a street line.</li> </ul>	

GENERAL PROVISIONS – Secondary Dwelling Units	
Permitted Secondary Dwelling Units	<p>Maximum of:</p> <ul style="list-style-type: none"> <li>– 1 secondary dwelling unit in a single detached or semi-detached, and</li> <li>– 1 secondary unit in an accessory structure</li> </ul>
Floor Area Maximum	<p>Secondary dwelling unit in a principal dwelling unit:</p> <ul style="list-style-type: none"> <li>– 40% of the total floor area of the</li> </ul>

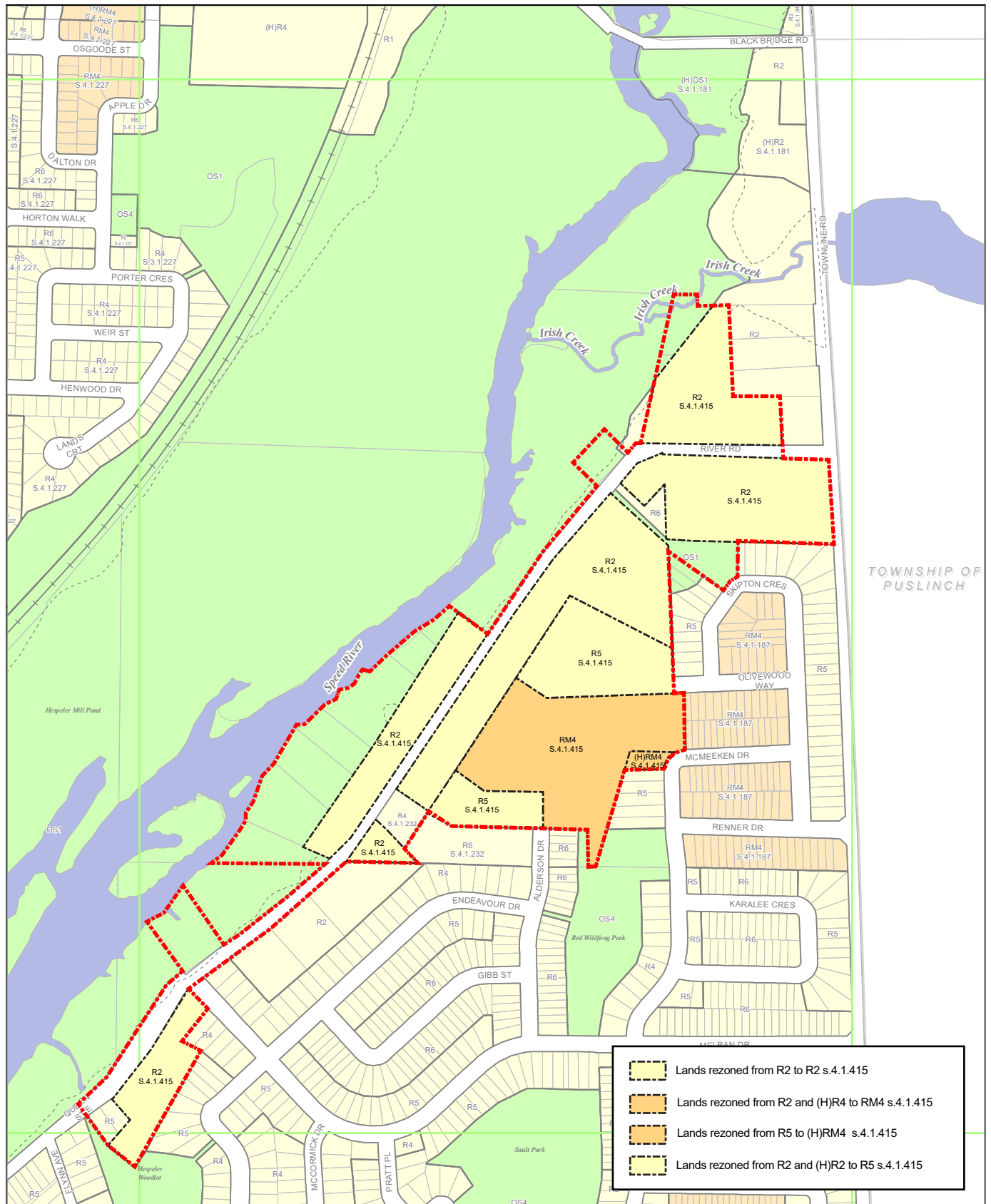
<b>GENERAL PROVISIONS – Secondary Dwelling Units</b>	
	<p>principal dwelling including the basement.</p> <p>Secondary dwelling unit in an accessory structure:</p> <ul style="list-style-type: none"> <li>- the lesser of 40% of the total floor area of the principal dwelling excluding the basement or 80 m<sup>2</sup></li> </ul>
Entrance	Direct or shared entrance to the secondary dwelling unit may be provided.
Access	No separate access to a municipal road is permitted.
Connection to City Services	The principal dwelling unit and secondary dwelling unit on the same lot must be connected to municipal sewer and water services of adequate size where municipal services are available.
Maximum Number of Bedrooms in a Secondary Dwelling Unit	2
Minimum Interior Side Yard	<p>Secondary dwelling unit in:</p> <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 3.0 m</li> </ul>
Minimum Exterior Side Yard	No closer than exterior side yard for main building
Minimum Rear Yard	<p>Secondary dwelling unit in:</p> <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 3.0 m</li> </ul>
Minimum distance between principal dwelling unit and accessory structure with secondary dwelling unit	7.5 m
Minimum Distance from Interior Side Lot Line to Secondary Dwelling Unit Entrance or Staircase	1.2 m
Minimum Distance from Exterior Side Lot Line to Secondary Dwelling Unit Entrance or Staircase	6.0 m

<b>GENERAL PROVISIONS – Secondary Dwelling Units</b>	
Minimum Parking spaces required for Secondary Dwelling Unit	<ul style="list-style-type: none"> <li>- Bachelor or 1 bedroom secondary dwelling unit: 1</li> <li>- 2 bedroom secondary dwelling unit: 2</li> </ul>
Front Yard Parking	In addition to the provision of 2.2.2.3(c) and subject to 3.1.1.4(b) and other provisions of this By-law, required parking for a secondary dwelling unit may be permitted between the regulatory building line or the established building line and the street line.
Maximum Height	Secondary dwelling unit in <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 4.5 m and 1 storey</li> </ul>
Maximum Lot Coverage	Secondary dwelling unit in <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 10%</li> </ul>

The holding provision, as shown by the symbol “H” on Schedule -21 of this By-law, may be removed when a servicing and road connections linking the River Road Secondary Plan area to the adjacent neighbourhood have been established.

5. AND THAT this By-law shall come into force and effect on the date it is enacted subject to Official Plan Amendment No. 47 coming into effect pursuant to subsection 24(2) of the Planning Act, R.S.O, 1990, c.P.13, as amended

READ A FIRST, SECOND AND THIRD TIME, ENACTED AND PASSED THIS \_\_\_\_ DAY OF \_\_\_\_\_, A.D, 2021



**GENERALIZED ZONING CATEGORIES**

- AGRICULTURAL
- COMMERCIAL
- INDUSTRIAL
- INSTITUTIONAL
- LOW DENSITY RESIDENTIAL
- MEDIUM HIGH DENSITY RESIDENTIAL
- OPEN SPACE

- Regulatory Flood Line See Section 2.1.8
- Municipal Boundary



## Attachment No.3 – Alternative Zoning By-law Amendment

## **AMENDMENT TO THE CITY OF CAMBRIDGE ZONING BY-LAW**

### **PURPOSE AND EFFECT**

The River Road Neighbourhood is located in the Hespeler community and is generally bounded by: Townline Road to the east; River Road and the Speed River to the west and north; and Melran Drive to the south. The River Road Secondary Plan area includes a series of lands along River Road between the Brewster Trail (Flynn Avenue) and River Road, to Townline Road and River Road. The total area of the Secondary Plan area is approximately 19.0 hectares (47 acres).

The River Road area is within the designated Urban Area in the City of Cambridge. The River Road Secondary Plan establishes a vision, principles and policies for the design and development of the River Road area. It also establishes the general land use patterns and conceptual location of community infrastructure such as parks and trails, roads, and services.

The purpose of the Zoning By-law Amendment is to implement the River Road Secondary Plan. New land use designations, specific to the River Road Secondary Plan area, are proposed including Village Residential and Neighbourhood Residential designations.

The subject lands are presently zoned R2 (Residential Type 2) and (H)R4 (Residential Type 4 with a Holding Zone) by the City of Cambridge Zoning By-law 150-85. A schedule showing the zoning is attached. The zoning will implement the proposed official plan amendments. The following zones are proposed: R2, R5 and RM4.

**BY-LAW NO. \_\_\_\_\_-21**

**of the**

**CITY OF CAMBRIDGE**

Being a by-law of the Corporation of the City of Cambridge to amend the City of Cambridge Zoning By-Law pursuant to Sections 34 and 36 of the Planning Act (R.S.O. 1990, c. P.13, as amended)

WHEREAS the City of Cambridge Official Plan provides for the lands affected by this by-law to be zoned as set forth in this by-law;

AND WEREAS this By-law conforms to the City of Cambridge Official Plan; and,

AND WHEREAS Council deems that adequate public notice of the public meeting was provided and adequate information regarding the amendment was presented at the public meeting held April 27, 2021, and that further public meeting is not considered necessary in order to proceed with this amendment.

NOW THEREFORE THE MUNICIPAL COUNCIL OF THE CORPORATION OF THE CITY OF CAMBRIDGE enacts as follows:

1. THAT this by-law shall apply to the lands outlined in heavy black line on Schedule "A" to By-law **XX-21**, in the City of Cambridge, Regional Municipality of Waterloo
2. THAT the City of Cambridge Zoning By-Law, being Schedule A to By-Law No. 150-85, is hereby amended by changing the zoning classification of the lands shown outlined by a heavy black line on Schedule A hereto attached from R2 and (H)R4 to R2, R5, RM4, and RM4(H).
3. THAT the City of Cambridge Zoning By-law, being Schedule A to By-law 150-85, is hereby further amended by adding the notation "(s.4.1.415)" to the R2, R5 and RM4 zone symbols;
4. AND FURTHER THAT the aforesaid City of Cambridge Zoning By-law, as amended, is hereby further amended by adding the following subsection to section 4.1 thereof:

**"4.1.415 River Road**

1. Notwithstanding the provisions of sections 2.1.15, 2.2.4.5, 3.1.1.4A, 3.1.1.10, 3.1.2.1, 3.1.2.2, 3.1.2.3, 3.1.2.5 of this by-law and in addition to section 1.1.1, the following regulations shall apply to the lands in those R2, R5 and RM4 zone classifications to which reference to "(s.4.1.415)" is made on Zoning Map C14 attached to and forming part of this by-law:



## Definitions

Stacked, back-to-back townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit,

- (a) is divided both vertically and horizontally from another dwelling unit by a common wall;
- (b) has independent external access; and,
- (c) shares a common rear exterior wall.

Stacked townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit is divided both horizontally and vertically from another dwelling unit by a common wall and in which each dwelling unit has an independent external access.

Back-to-back townhouse dwelling means a building containing three or more dwelling units in which each dwelling unit,

- (a) is divided vertically from another dwelling unit by a common wall;
- (b) has independent external access; and,
- (c) shares a common rear exterior wall.

Uses Permitted <sup>(A)</sup>	In Zone		
	R2	R5	RM4
Detached dwelling	♦	♦	
Semi-detached dwelling		♦	♦
Triplex dwelling		♦	♦
Semi-detached duplex dwelling			♦
Linear row house dwelling			♦
Stacked townhouse dwelling			♦
Back-to-back townhouse dwelling			♦
Stacked back-to-back townhouse dwelling			♦
Residential special care facility	♦	♦	
A use permitted in any zone in accordance with section 2.1.1	♦	♦	♦

Uses Permitted <sup>(A)</sup>	In Zone		
	R2	R5	RM4
An accessory use, building or structure in accordance with section 2.1.11	♦	♦	♦

(A) No building or structure, except for a single detached dwelling and/or accessory structure, shall be constructed unless serviced by full municipal water and sewer services.

ZONE STANDARDS		R2 Detached
Minimum Lot Frontage	m	30
Maximum Lot Frontage	m	35
Minimum Corner Lot Frontage	m	30
Maximum Corner Lot Frontage	m	35
Minimum Lot Area	m <sup>2</sup>	900
Minimum Corner Lot Area	m <sup>2</sup>	900
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre
Minimum Front Yard to House	m	(A)
Maximum Front Yard to House	m	(B)
Minimum Interior Side	m	5.0
Minimum Exterior Side Yard	m	7.0
Minimum Rear Yard	m	7.5
Maximum Building Height	m	9
Maximum Number of Storeys		2

ZONE STANDARDS		R2 Detached
Minimum Gross Floor Area	m <sup>2</sup>	120
Maximum Lot Coverage	% of lot area	40
Minimum Landscaped Open Space		35

- (A) The Minimum Front Yard to House shall be 1.5 metres less than the average of the Minimum Front Yard Setback of dwellings on adjacent lots located within 60 metres along the same side of the road, but shall not be less than 6 metres.
- (B) The Maximum Front Yard to House shall be 1.5 metres greater than the average of the Maximum Front Yard Setback of dwellings on adjacent lots located within 60 metres along the same side of the road, but shall not be greater than 12 metres.

ZONE STANDARDS		R5 Detached	R5 and RM4 Semi-Detached and Triplex
Minimum Lot Frontage	m	12	9/18 <sup>A</sup>
Minimum Corner Lot Frontage	m	17	13/22 <sup>A</sup>
Minimum Lot Area	m <sup>2</sup>	270	270
Minimum Corner Lot Area	m <sup>2</sup>	450	450
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre	
Minimum Front Yard to House	m	6.0	6.0
Minimum Interior Side Yard	m	1.2	1.2

ZONE STANDARDS		R5 Detached	R5 and RM4 Semi- Detached and Triplex
Minimum Exterior Side Yard	m	6.0	6.0
Minimum Rear Yard	m	7.5	7.5
Maximum Building Height	m	11 (3 storeys)	
Minimum Gross Floor Area	m <sup>2</sup>	75	75
Maximum Lot Coverage	% of lot area	50	50
Minimum Landscaped Open Space		30	30

(A) One attached Dwelling Unit / Two Attached Dwelling Units

ZONE STANDARDS		RM4 Linear Row	RM4 All Types of Townhouse
Minimum Lot Frontage Rear	m	5.5	N/A
Minimum Lot Frontage Front Garage	m	6.0	30
Minimum Corner Lot Frontage	m	11.6	N/A
Minimum Lot Area	m <sup>2</sup>	165	165 per unit
Minimum Corner Lot Area	m <sup>2</sup>	285	N/A
Minimum Front Yard to Garage	m	Equal to the front yard setback of the house plus 1.0 metre	N/A
Minimum Front Yard to House	m	5.0	5.0

ZONE STANDARDS		RM4 Linear Row	RM4 All Types of Townhouse
Minimum Front Yard to House Rear Garage Condition	m	3.0	3.0
Minimum Rear Setback to Rear	m	1.0	1.0
Minimum Interior Side	m	1.5 or 3.0 <sup>A</sup>	3.0
Minimum Exterior Side Yard	m	4.5	4.5
Minimum Rear Yard	m	7.5	N/A
Minimum Rear Yard between Rear Garage and Dwelling	m	6.0	N/A
Maximum Building Height	m	11 (3 storeys)	
Minimum Gross Floor	m <sup>2</sup>	70 per unit	60 per unit
Minimum private amenity area per unit	m <sup>2</sup>	N/A	30
Maximum number of attached units		6	N/A
Maximum Lot Coverage	% of lot area	60	60
Minimum Landscaped Open Space		30	30

- (A) Minimum interior side yard increased to 3.0 metres if such interior side yard abuts an R-class or RS-class zone

GENERAL PROVISIONS	
Permitted Encroachments – open or covered unenclosed porches, patios, decks not exceeding 3 metres in height	<p>1.5 metres into the required front yard on an interior lot and 1.5 metres into the minimum required front and exterior side yards on a corner lot</p> <p>2.5 metres into the minimum rear yard including eaves and cornices</p>

<b>GENERAL PROVISIONS</b>	
Setback of access driveway from the intersection of two street	6.0 m
A private garage shall not project beyond the front of a porch	
<p>The following regulations shall apply to the dwellings fronting onto River Road:</p> <ul style="list-style-type: none"> <li>a) Unless the garage is located in the rear yard, the maximum width of a garage on a lot larger than 15.0 metres, measured from outside walls shall be 25% of the front wall of the building.</li> <li>b) Unless the garage is located in the rear yard, the maximum width of a semi-detached garage, measured from outside walls shall be 35% of the front wall of the building.</li> <li>c) The width of an attached garage or carport for a detached one-family dwelling, or a semi-detached one-family dwelling may not exceed 20% of the width of the lot measured at the front building wall and parallel to the front lot line to a maximum width of 7.0 metres and the inside of an attached garage or carport shall have a minimum size of 2.9 m by 5.5 m.</li> <li>d) A driveway and its widening shall not exceed 50% of the lot width, or 6 metres, whichever is lesser.</li> <li>e) Garage and parking areas for triplexes shall be located at the rear of the property (behind main building).</li> <li>f) A maximum of one driveway with one access point from a street or lane shall be permitted on a lot.</li> <li>g) A driveway shall have a minimum width of 2.6 metres.</li> <li>h) A driveway that is longer than 6.0 metres shall not exceed 3.5 metres in width within 6.0 metres of a street line.</li> </ul>	

<b>GENERAL PROVISIONS – Secondary Dwelling Units</b>	
Permitted Secondary Dwelling Units	<p>Maximum of:</p> <ul style="list-style-type: none"> <li>– 1 secondary dwelling unit in a single detached or semi-detached, and</li> <li>– 1 secondary unit in an accessory structure</li> </ul>
Floor Area Maximum	<p>Secondary dwelling unit in a principal dwelling unit:</p> <ul style="list-style-type: none"> <li>– 40% of the total floor area of the principal dwelling including the</li> </ul>

<b>GENERAL PROVISIONS – Secondary Dwelling Units</b>	
	<p>basement.</p> <p>Secondary dwelling unit in an accessory structure:</p> <ul style="list-style-type: none"> <li>- the lesser of 40% of the total floor area of the principal dwelling excluding the basement or 80 m<sup>2</sup></li> </ul>
Entrance	Direct or shared entrance to the secondary dwelling unit may be provided.
Access	No separate access to a municipal road is permitted.
Connection to City Services	The principal dwelling unit and secondary dwelling unit on the same lot must be connected to municipal sewer and water services of adequate size where municipal services are available.
Maximum Number of Bedrooms in a Secondary Dwelling Unit	2
Minimum Interior Side Yard	<p>Secondary dwelling unit in:</p> <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 3.0 m</li> </ul>
Minimum Exterior Side Yard	No closer than exterior side yard for main building
Minimum Rear Yard	<p>Secondary dwelling unit in:</p> <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 3.0 m</li> </ul>
Minimum distance between principal dwelling unit and accessory structure with secondary dwelling unit	7.5 m
Minimum Distance from Interior Side Lot Line to Secondary Dwelling Unit Entrance or Staircase	1.2 m
Minimum Distance from Exterior Side Lot Line to Secondary Dwelling Unit Entrance or Staircase	6.0 m

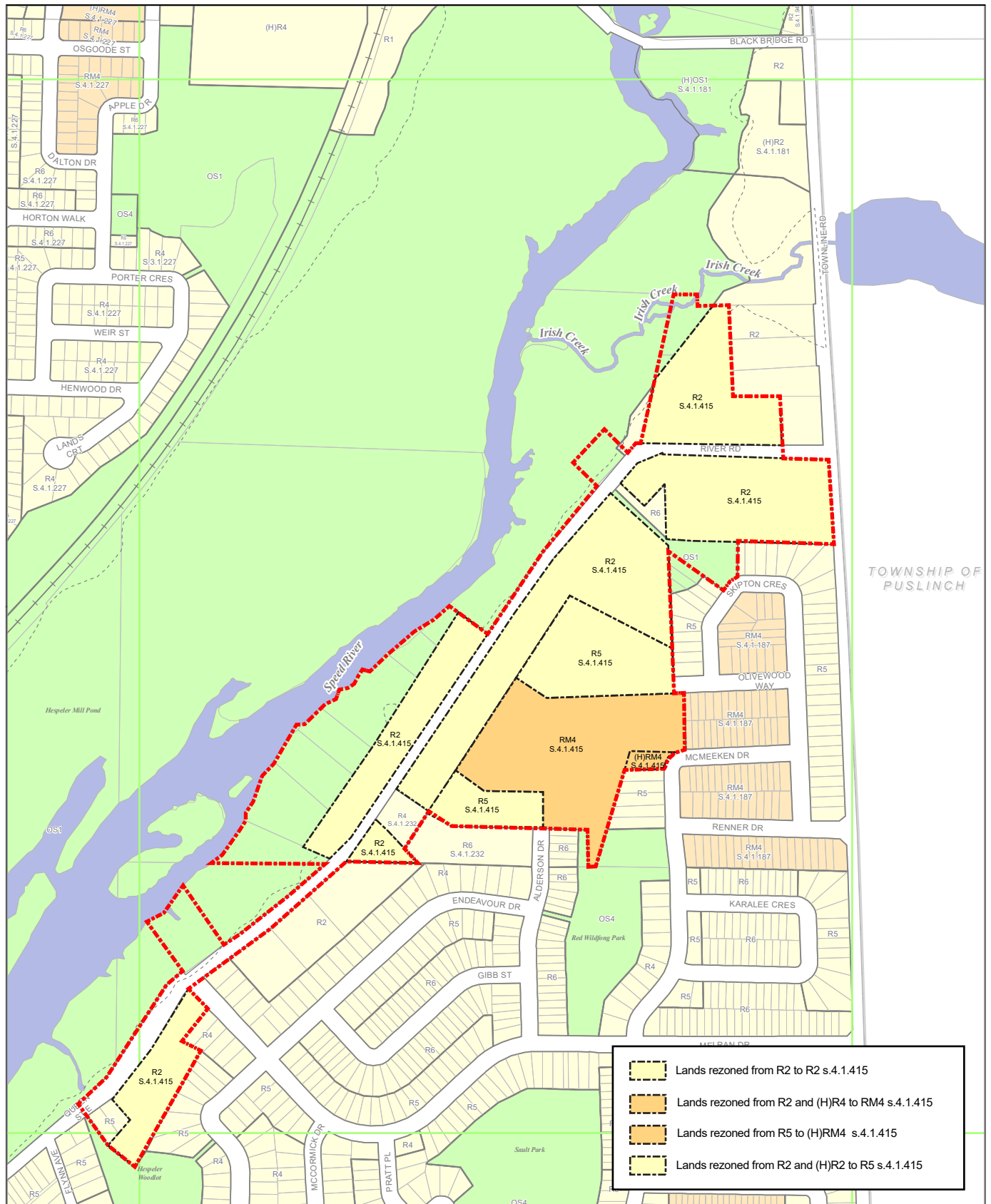
<b>GENERAL PROVISIONS – Secondary Dwelling Units</b>	
Minimum Parking spaces required for Secondary Dwelling Unit	<ul style="list-style-type: none"> <li>- Bachelor or 1 bedroom secondary dwelling unit: 1</li> <li>- 2 bedroom secondary dwelling unit: 2</li> </ul>
Front Yard Parking	In addition to the provision of 2.2.2.3(c) and subject to 3.1.1.4(b) and other provisions of this By-law, required parking for a secondary dwelling unit may be permitted between the regulatory building line or the established building line and the street line.
Maximum Height	Secondary dwelling unit in <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 4.5 m and 1 storey</li> </ul>
Maximum Lot Coverage	Secondary dwelling unit in <ul style="list-style-type: none"> <li>- principal dwelling unit: refer to requirement for principal dwelling in applicable zone</li> <li>- accessory structure: 10%</li> </ul>

The holding provision, as shown by the symbol “H” on Schedule -21 of this By-law, may be removed when a servicing and road connections linking the River Road Secondary Plan area to the adjacent neighbourhood have been established.

5. AND THAT this By-law shall come into force and effect on the date it is enacted subject to Official Plan Amendment No. 47 coming into effect pursuant to subsection 24(2) of the Planning Act, R.S.O, 1990, c.P.13, as amended

READ A FIRST, SECOND AND THIRD TIME, ENACTED AND PASSED THIS \_\_\_\_ DAY OF \_\_\_\_\_, A.D, 2021





**GENERALIZED ZONING CATEGORIES**

- AGRICULTURAL
- COMMERCIAL
- INDUSTRIAL
- INSTITUTIONAL
- LOW DENSITY RESIDENTIAL
- MEDIUM HIGH DENSITY RESIDENTIAL
- OPEN SPACE

- Regulatory Flood Line See Section 2.1.8
- Municipal Boundary





CORPORATION OF THE CITY OF CAMBRIDGE

BY-LAW 21-XXXX

Being a by-law to amend Site Plan Control By-law 20-60.

**Whereas** the City of Cambridge has completed a study of the River Rd. neighborhood;

**AND WHEREAS** a recommendation of the study is that the site plan control by-law be amended to require site plan control for single detached, semi-detached and triplex dwellings fronting on River Rd. to further regulate development;

**AND WHEREAS** the Council for the Corporation of the City of Cambridge has deemed it necessary and expedient to pass such a by-law.

**NOW THEREFORE BE IT RESOLVED THAT** the Corporation of the City of Cambridge enacts as follows:

1. By-law 20-60 is amended by adding the following Schedule 'D' and 'D-1'

Read a First, Second and Third Time

**PASSED AND ENACTED** this 27<sup>th</sup> day of July, 2021, A.D.

---

MAYOR

---

CLERK

SCHEDULE "D"  
TO BY-LAW NO. 20-60  
OF THE  
CORPORATION OF THE CITY OF CAMBRIDGE

Notwithstanding Schedule "A" the following classes of development may not be undertaken without the approval of the plans referred to in subsection 3 a) – b) and 4 of By-law No. 20-60; for the lands delineated on Schedule "D-1"

1. Detached dwellings.
2. Semi-detached dwellings.
3. Detached triplexes.

## Schedule "D-1"



Map 1



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## Attachment No.5 – Public Submissions

## Bryan Cooper

---

**From:** Marcel O'Gorman [REDACTED]  
**Sent:** Monday, May 10, 2021 11:24 AM  
**To:** Bryan Cooper  
**Cc:** clerks  
**Subject:** [External] Update on Planning for River Road

Dear Bryan (if I may),

I recently moved into a house at [REDACTED] Road, in the midst of all the new development that was recently discussed at a council meeting. I received a letter in the mail about the plans, in brief, but I would like to know more about how this is going to impact my property. My house is close to River Road but below a retaining wall. I am concerned about the impact of construction on that wall and the mature trees that have grown into it. I am also curious about water and sewer service in the area and what it is going to mean for my property.

Finally, I would like to remain informed about future plans. I see from this letter that I am supposed to make a written request to the City Clerk for this purpose. Can this be considered my written request?

Thank you. I look forward to having a discussion. I can be reached at [REDACTED]

Best,  
m.

--

Professor Marcel O'Gorman, PhD

I acknowledge that I work and teach on the traditional territory of the Attawandaron (Neutral), Anishnaabeg, and Haudenosaunee peoples. The University of Waterloo is situated on the Haldimand Tract, land promised and given to Six Nations, which includes six miles on each side of the Grand River.

## Bryan Cooper

---

**From:** Syd Ahmad [REDACTED] m>  
**Sent:** Wednesday, May 5, 2021 4:51 PM  
**To:** Bryan Cooper  
**Subject:** [External] Re: River Road Secondary Plan

Thanks so much for your prompt response

---

**From:** Bryan Cooper <CooperB@cambridge.ca>  
**Sent:** May 5, 2021 4:48 PM  
**To:** [REDACTED]  
**Cc:** Nicholas Ermeta <ErmetaN@cambridge.ca>  
**Subject:** RE: River Road Secondary Plan

Hi Syd,

The draft material for the River Rd Secondary Plan study can be found at the following link (click on the River Road neighbourhood secondary plan, zoning and servicing study drop down)

<https://www.cambridge.ca/en/build-invest-grow/current-development-applications.aspx>

A public meeting was held to consider the proposed amendments on April 27, 2021. We are targeting the June 22, 2021 Council meeting for a recommendation report.

Thanks  
Bryan

### **Bryan Cooper MCIP, RPP**

Senior Planner  
City of Cambridge, Community Development Department  
Development Planning Division  
50 Dickson Street, 3rd floor  
PO Box 669, Cambridge ON N1R 5W8  
[cooperb@cambridge.ca](mailto:cooperb@cambridge.ca)  
Tel: (519) 623-1340 ext 4598

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**From:** Nicholas Ermeta  
**Sent:** Tuesday, May 4, 2021 8:07 PM  
**To:** Elaine Brunn Shaw <[BrunnShawE@cambridge.ca](mailto:BrunnShawE@cambridge.ca)>  
**Cc:** [REDACTED]  
**Subject:** River Road Secondary Plan

## Bryan Cooper

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**From:** Linda Strickland <[REDACTED]@pm>  
**Sent:** Friday, April 30, 2021 10:58 AM  
**To:** Bryan Cooper  
**Cc:** Mariah Strickland  
**Subject:** [External] River Rd secondary plan and zoning

Dear Bryan

During the virtual meeting on Tuesday, April 27, regarding River Road, Mike Mann directed participants to email you if we wanted to be informed about any further information, committee meetings etc regarding this area.

Can you please clarify if you will be responsible in ensuring further communication of information or do I need to mail a written request to the development department as directed on the Notice of Public meeting that we received in the mail

[REDACTED]  
Thank you.

I look forward to clarification regarding this matter.

Linda Strickland Harth.

Sent from my iPad



## Bryan Cooper

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**From:** dgmalley <[REDACTED]>  
**Sent:** Tuesday, April 27, 2021 10:16 AM  
**To:** Bryan Cooper  
**Subject:** [External] River Rd.

Hi Bryan:

Could you please ensure that I am on the contact list for the River Rd. Secondary Plan?

Thanks very much,

Dan Malloy



Sent from my Galaxy

## Bryan Cooper

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**From:** Bryan Cooper  
**Sent:** Monday, April 26, 2021 5:35 PM  
**To:** [REDACTED]  
**Cc:** Mike Devine; Elaine Brunn Shaw; [REDACTED]; Deanne Friess  
**Subject:** April 27th Meeting: Notes on River Road Secondary Plan

Good evening Tom and Tamara,  
I was provided your questions below. Please see preliminary comments in red below from staff. We will include this as correspondence and will be included and responded to as part of the future staff recommendation report on this file.

Thanks  
Bryan

On Apr 25, 2021, at 10:08 PM, Tom Hetherington [REDACTED] wrote:

Hello Mike,

We have read the 300+ pages that are included in your package for Tuesday's meeting that relate to the River Road Secondary Plan.

First of all, we want to impress upon you our appreciation for this opportunity to provide input and help shape the plan for the area. Looking through the chronology of information, it is apparent that there has not only been public consultation, but also that the citizens' input has been considered.

Secondly we want to thank you for making extra efforts to engage with the affected and interested citizens so that we are both informed at every step of the process and you are aware of what our concerns have been. We appreciate your efforts for a strong mutual understanding of the process and implications for our neighborhood.

After reading through this extensive background information and recommendations, we have a few remaining questions:

1. Please clarify the timing of the interim and ultimate cross sections on River Road. Is the plan to have the city do the interim cross-section and then have developers incorporate the ultimate cross section for the sections they develop? For the eastern end of River Rd/Townline, will the city incorporate the ultimate cross section, and when? (As you know, there are a variety of forms of pedestrian traffic and that a bicycle friendly passage aligns with the provincial mandate for more cycling friendly paths which also justifies the ultimate over the interim.) Please also comment on the current development at 340, 350, 360 and the use of sidewalk (interim) vs multi-use trail (ultimate) as it should be MUT as per this explanation.

(page 63 of Agenda, 18.6.4.1 River Road)

Based on the secondary plan it isn't anticipated that the development of the properties fronting on River Road will proceed in a single comprehensive development. As such the interim cross-section improvements on River Road will most likely be a City driven initiative. Note that the timing and funding for the River Road interim improvements is still to be determined and would need to be considered through the City's capital improvement budget program and the next Development Charge Study By-law (if it is to be funded from Development Charges).

As discussed at the Liaison group meeting the City would not install the piecemeal multi-use trail across the frontage of one property only to transition back to standard sidewalk. Phase 1 interim cross section would be utilized until a sufficient amount of road widening have been acquired to facilitate the design and implementation the MUT. This is why 340-360 River Road has proceeded with an interim asphalt sidewalk.

2. Please confirm that the proposed municipal road from River Rd to McMeekan Road will have enough allowance for the ladder fire truck. We have heard that other streets such as Melran is not ladder truck-friendly with its width, curves and on street parking. Larger road allowances will accommodate fire trucks and other future demands that may arise.

(Schedule 3 of the Active Transportation Structure)

The future municipal road connection from McMeekan Road to River Road is proposed as a standard 18.5m cross section which accommodates a fire truck access. Melran Dr. is a 20m right-of-way and was also designed to allow fire truck access.

3. As noted in the documentation, there are also concurrent projects underway, at various stages. Will any conversion from well/septic to municipal water/sewage be coordinated among these projects? (340, 350, 360 River Rd Townhomes, 442-448 River Rd homes, Black Bridge & Townline Rd EA & project). 340-360 River Road was designed to allow for a future connection when the sanitary sewer is extended on River Road and the development at 442-448 River Road would be required to provide for the same. No servicing was considered on Townline Road as part of the EA and road improvement project.

4. Traffic calming to encourage lower speeds is necessary and it is appreciated this is noted in the reports of this Secondary Plan. Lamp post lighting, similar to those in areas of Galt with a design sympathetic to the heritage of the area rather than concrete light standards could also contribute to encouraging lower speeds.

The City has implemented Seasonal Traffic Calming signs as a first step to address speeding on River Road. The next step would be to assess the impact those measures have had. If speeding persists based on the City's Traffic Calming Policy further measures including physical traffic calming would be considered. If physical traffic calming is warranted, then pending budget, implementation could proceed prior to reconstruction of River Road or depending on timing of any reconstruction incorporated into said reconstruction. Street lighting is not a tool the City uses for traffic calming. Lighting details will be considered as part of detailed design.

5. In considering fencing/barriers, it would be immensely appreciated that there be NO chain link fence. Instead, the creative use of landscape buffers and decorative fencing would be more sympathetic to the area views.

We will consider additional direction on fencing within the policies of the secondary plan.

6. The Cultural Heritage Resource Assessment noted that River Road merits recognition by the City of Cambridge as a City of Cambridge scenic route as well as potential CHL or extension of the Black Bridge CHL. What are the next steps in this process from identification to establishment of one or both designations? Could the mature neighbourhood designations be considered for the next City's OP amendment? (page 38 of the Agenda, 18.3.2.1 River Road - Potential Scenic Heritage Road & CHL). Could there be a sign noting the area's heritage designation?

Should Council decide to pursue designation of River Road as a Cultural Heritage Landscape, the next step would be to develop a Technical Study as per the Region's Implementation Guidelines for Cultural Heritage Landscape Conservation. The Technical Study, along with a staff recommendation report will be provided to the Municipal Heritage Advisory Committee to be received and whether to recommend

Council designate the area as a CHL through an Official Plan Amendment. Should Council agree to designate, the CHL would be designated under the City's Official Plan. A sign indicating the area's CHL designation may be a recommendation in the Technical Study and would also be subject to Council approval.

Please reach out to discuss further, as necessary.

Thank you.

Sincerely,

Tom & Tamara Hetherington

cc: Denis & Irene Lanno

**Bryan Cooper**

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**From:** S Brooks [REDACTED] >  
**Sent:** Tuesday, April 20, 2021 7:46 PM  
**To:** Bryan Cooper  
**Subject:** [External] River road secondary plan and Zoning bylaw amendments

Bryan Cooper  
Senior Planner

I do not understand why you are so set on changing River Road. We have been here for a long time and never hurt anyone that I have heard of. We are on septic and well water, does that annoy you. We pay our taxes. We are on the edge of the city of Cambridge. Why can't we keep our larger than average lot. Does that bother you. Or is there a developer that wants all this land so he can make millions off of it. Does the city of Cambridge have a stake in this developers success. Why else would you fight so long and hard to destroy our homes. What have we done that is so bad that you want to take away what we have worked for, for years. Please give me a good reason why you should take our homes away from us. I moved here 3 years ago because of the size of the lot and its location. We don't want to live in a 30' by 30' lot or a 50' by 50' lot. We want to enjoy the outdoors without neighbours so close you can hear them talking.

Tell me what you want all this land for? Why the bylaw Amendments. Why the rezoning?

Why do you want to uproot families that have been here for a very long time?

My question is WHY?

Sincerely,  
Sylvia Brooks  
[REDACTED]

## Bryan Cooper

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**From:** Majed Darr [REDACTED]  
**Sent:** Thursday, March 11, 2021 4:01 PM  
**To:** Bryan Cooper  
**Subject:** [External] Re: [External] Lot 9 River Road

Good Afternoon

Thank you very much for your quick response! I appreciate you putting me on the mailing list.

Best Regards

On Thu, Mar 11, 2021 at 1:47 PM Bryan Cooper <[CooperB@cambridge.ca](mailto:CooperB@cambridge.ca)> wrote:

Good afternoon Majed,

You are correct there is currently an interim control by-law in effect for this property. At this time I can't confirm whether the existing R2 zoning will remain as is, or if some modifications to it will be put in place. A statutory public meeting will occur in the spring and the details of the proposed zoning will be considered at that meeting. I will add your name to the mailing list for future notices on this project.

If you have any other questions please let me know.

Thanks

Bryan

**Bryan Cooper MCIP, RPP**

Senior Planner

City of Cambridge, Community Development Department

Development Planning Division

50 Dickson Street, 3rd floor

PO Box 669, Cambridge ON N1R 5W8

[cooperb@cambridge.ca](mailto:cooperb@cambridge.ca)

Tel: (519) 623-1340 ext 4598

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**From:** Majed Darr <[REDACTED]>  
**Sent:** Thursday, March 11, 2021 8:51 AM  
**To:** Bryan Cooper <[CooperB@cambridge.ca](mailto:CooperB@cambridge.ca)>  
**Subject:** [External] Lot 9 River Road

Hello Bryan

I have recently purchased the above mentioned Lot from Sean O'Neil and was directed towards you regarding some clarification I need as he had mentioned you are very familiar with this project of theirs.

There is currently an interim control bylaw on this property. I know the zoning is R2 and was wondering if it would remain the same after the lifting of the interim control? Also, the designer has been asking me if the rule of 16M front yard setback from the centreline of the road still applies to this or is it just 10M from the road.

We are currently waiting on receiving an answer to this question so he can then ascertain the building envelope and move forward with the design of the home.

I really appreciate any help you could provide in this matter!

Best Regards

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## Bryan Cooper

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**From:** Denis F. Lanno [mailto:denis.f.lanno@cityofcalgary.ca] >  
**Sent:** Friday, November 20, 2020 11:48 AM  
**To:** Bryan Cooper  
**Cc:** Mike Devine  
**Subject:** [External] Lanno comments River Road  
**Attachments:** River Road Public Consultation Opportunity.docx

**Categories:** [SharePoint] This message was saved in 'Planning Services > ZA and OP Amendments > 2019 > River Rd ICBL > Public Submissions > comments post oct 13 2020 meeting'

Hi Brian, I hope you are well. Looks like we won't be meeting in person for some time. We're all tired of Covid, but we must be diligent.

Attached, are some of the concerns my neighbours and I share.

All the best.

Cheers,



Sent from [Mail](#) for Windows 10



Denis Lanno

Nov 20, 2020

## **Comment sheet**

### **River Rd Secondary Plan and Servicing Study**

- We were all caught off guard when the Reid's development was proposed for 340 River Road. We at great cost, and effort learned about the process, and recognized the city had very few options available to them to stop this development. It was very encouraging to us when the interim control bylaw was announced, because we believed, as our Lawyer indicated, it was an opportunity to save the serenity and the natural beauty of River Road.
- We are concerned with the three roads, which are proposed to cut onto River Road, and our belief was there would be no additional cut ins onto River Road. When lane 211 development was presented to us it was shown as a dead end, and the developer agreed to a central pumping station rather than grinders. This was very encouraging to use, because we believed this would set the precedent for the rest of River Road.
- We are also concerned with the mini subdivision proposed at Townline and River Road. The oldest home in this area was built in 1982. The rest of the homes are 30 years old or less with one of the most beautiful homes being just 18 years old, and just across the street we have a beautiful heritage home. We wonder how it is acceptable to tear down these beautiful homes. This area was rezoned once already, and this is why these magnificent homes were built. My neighbours, and I bought into a lifestyle. We decided to make substantial investments to live here believing it would remain this way.
- We are concerned with traffic control, and we would like to see a 40Klm an hour speed limit with speed bumps similar to Riverbank Drive. Traffic is already much higher and getting worse with every development, and speed is an issue.
- All along we have said we do not oppose development, provided it is complementary to the current environment. We all recognize River Road needs a facelift, and we believe the current proposal is doable if our concerns can be reasonably addressed.
- We would like to see these new neighbourhoods with pedestrian and bicycle access to River Road, but not cars. We can't imagine what Queen Street will be like with all the additional traffic coming not just from just River Road, but with all the additional development proposed for the area.
- We imagine River Road with bicycle trails, a walking path all tying into the Cultural Heritage Landscape we worked so hard to achieve. Our efforts saved the Blackbridge Road Bridge and the surrounding area from development, and it has become a lovely area for hikers, canoers, and fishing.
- River Road is unique within Hespeler, I remember going out of my way just to drive down River Road, so I could enjoy the serenity and natural beauty long before we built here. We like our neighbours moved here, because of what it is. I ask all of you to consider the things that were important to you when you decided where to live. We all have our reasons, for my neighbours, and I it was the community we were looking for.
- We believe these compromises are reasonable, and we hope we can find a way to preserve the serenity, so we and the surrounding community can all benefit from it for years to come.  
Thank you for your consideration.

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**From:** Bryan Cooper <CooperB@cambridge.ca>  
**Sent:** November 18, 2020 9:36 AM  
**To:** 'Russell Racey' <[REDACTED]>  
**Cc:** Judy Bielefeld <[REDACTED]> m>; Patric Malinski <[REDACTED]>  
**Subject:** RE: [External] River Rd Secondary Plan and Servicing Study -- a virtual discussion?

Hello Russell,

Yes the purpose of the meeting is to have a discussion with a small working group. This would give an opportunity for questions to be asked and discussed.

**Bryan Cooper MCIP, RPP**

Senior Planner  
City of Cambridge, Community Development Department  
Development Planning Division  
50 Dickson Street, 3rd floor  
PO Box 669, Cambridge ON N1R 5W8  
[cooperb@cambridge.ca](mailto:cooperb@cambridge.ca)  
Tel: (519) 623-1340 ext 4598

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**From:** Russell Racey <[REDACTED]>  
**Sent:** Wednesday, November 18, 2020 9:28 AM  
**To:** Bryan Cooper  
**Cc:** Judy Bielefeld; Patric Malinski  
**Subject:** [External] River Rd Secondary Plan and Servicing Study -- a virtual discussion?

Mr Cooper,

I've already responded earlier requesting to participate in the next virtual session but I must ask you: ***Will Residents this time be able to table questions online?*** Or will this be another muted PowerPoint presentation for a City planner to drone on about the finer esthetics of high-density mortgage prison complexes?

As you recall, even Councillor Devine voiced concern about how the October 13th virtual session came across, as we watched you cherry pick which emailed questions you chose to answer. I can only imagine, not very many.

So if you would please be so kind as to clarify to all parties as to which type of Zoom chat is this to be? A dog & pony show to a captive audience like the last one? Or an honest discussion where we, the taxpayers, actually have a say?

Thank you,

Russell Racey

A solid black rectangular box used to redact the signature of Russell Racey.

## Bryan Cooper

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**From:** Joy Hancock's [REDACTED]  
**Sent:** Tuesday, October 13, 2020 9:54 PM  
**To:** Bryan Cooper  
**Subject:** [External] Disappointing zoom meeting  
  
**Categories:** [SharePoint] This message was saved in 'Planning Services > ZA and OP Amendments > 2019 > River Rd ICBL > Public Submissions > October 13 2020'

Hi Bryan

The meeting re the river road development was most disappointing from a public input perspective. For those of us who are not comfortable with technology, we no longer have a voice. Finding a survey to complete is daunting and time consuming for many.

Plus we want to show passion and commitment which is not possible by filling a pre determined set of questions that would be available in a survey!

Ridiculous really.

Joy Hancock

## Bryan Cooper

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**From:** Jody Palmer [REDACTED]  
**Sent:** Sunday, February 23, 2020 1:44 PM  
**To:** Adam Ripper; Bryan Cooper; karen grant; maya-giambattista@fotenn.com; Donna Reid; Mike Devine  
**Subject:** Conversations during River Road Secondary Plan study neighbourhood meeting

**Follow Up Flag:** Follow up [REDACTED]  
**Flag Status:** Flagged

Hi All,

I greatly enjoyed meeting with you all at the neighbourhood workshop to discuss the Cambridge River Road Secondary Plan Study. I wanted to follow up on an point that arose in a number of conversations my wife and I had with you during that meeting.

**Summary:** I believe we must ensure that the moratorium is extended until the Zoning bylaw is updated to match the secondary plan lest the existing bylaw be used to allow development which undermines intent of the secondary plan.

As I understand it, the zoning bylaw governs development while the official plan governs updates to the zoning bylaw and applications for site-specific amendments.

Let me illustrate with a hypothetical example. If as a result of this consultation process and subsequent council motions, a secondary plan is adopted which increases setbacks on the east side of river road, but the zoning bylaw remains unchanged, development will be able to continue with the setbacks as defined in the zoning bylaw. It is only when developers seek exemptions or site-specific plans that the secondary plan has legal effect. This could easily result in development which impedes the intent of the secondary plan.

For this reason I believe it is crucial that staff recommends, and the city agrees, to extend the moratorium until the zoning bylaw is updated to match the secondary plan which, hopefully, arises from this study.

Thank you for your time.

- Jody Palmer

[REDACTED]  
[REDACTED]  
P.S. my permanent email address is [REDACTED] it currently forwards to my gmail address, but the personalized domain is the most reliable into the future.

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Disclaimer: Any meaning derived from this collection of bits is entirely the construction of the reader and may or may not represent the intent of the author.

## Bryan Cooper

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**From:** Adam Ripper  
**Sent:** Tuesday, February 18, 2020 3:59 PM  
**To:** [REDACTED]  
**Cc:** Bryan Cooper; Mike Devine; Elaine Brunn Shaw; Shannon Smith  
**Subject:** FW: River Road Secondary Plan - Public Open House

Hi Patric,

In advance of the Public Open House tomorrow, I summarized the City's response to your questions regarding the environmental considerations, regulated areas and changes to development policies as a result of the City's declaration of climate crisis below:

- Any proposed improvements to the River Rd road allowance would require review by GRCA since it is within their regulated area.
- The City has prepared a Climate Adaptation Plan  
[https://www.cambridge.ca/en/resources/rpt\\_cambridge\\_climate\\_adapt\\_plan\\_20190823\\_access.pdf](https://www.cambridge.ca/en/resources/rpt_cambridge_climate_adapt_plan_20190823_access.pdf)
- Environmental considerations will be included in the preparation of the secondary plan – City, Regional and GRCA constraints will be considered – there is an opportunity to include current GRCA mapping (e.g wetlands and floodplain) which are not reflected in the City's Official Plan for this area.
  - Individual environmental impact studies will still be required at the detailed plan submission – these are completed by professional consultants and reviewed by professional City, Regional or Conservation Authority staff.
- Planning decisions are required to conform to the 2019 Provincial Growth Plan – A Place To Grow. Climate change and mitigation is noted throughout the Growth Plan. Part of the focus on developing within existing built-up areas - which the River Rd neighbourhood and study area is – is to support transit, active transportation and minimizing land consumption through compact built form. This is in contrast to continual outward suburban expansion on the outer edges of the cities which has historically occurred in Ontario. Infilling is more cost effective from a servicing perspective, minimizes land consumption, preserves environmental and agricultural land and reduces car dependency, which are all good things from an environmental perspective.

Both City Staff and the Planning Consulting team will be on hand at the meeting tomorrow evening if you would like to discuss any of the above in further detail.

Regards

Adam Ripper, P.Eng.

Project Engineer  
The Corporation of the City of Cambridge  
50 Dickson Street, P.O. Box 669  
Cambridge, ON N1R 5W8  
Phone: 519.623.1340 ext. 4778



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**From:** Patric Malinski [REDACTED]  
**Sent:** Friday, February 07, 2020 11:08 AM  
**To:** Adam Ripper

**Cc:** Bryan Cooper; Mike Devine; Elaine Brunn Shaw; Shannon Smith

**Subject:** RE: River Road Secondary Plan - Public Open House

Adam,

Thank you for the information.

In terms of the study area, potential servicing and road widening, there would surely be an impact to the GRCA regulated erosion hazards and significant valley. For example, the planned road widening along the Reid's properties, the GRCA documents noted that enhancements would be likely needed on the west side of the road where the slope is located. During our area tour it was noted that there is a significant slope on the west side for most of River Road. I'm assuming that all the options will be prepared for the public meetings.

As a side, I am looking for additional information around the city's declaration of climate crisis and what that means for planning and development. Have there been any significant changes to the city's approach to planning and development- recognizing that the most significant changes to the landscape/environment falls under this category. Could you point me in the right direction of any new policies/documents on this topic since the declaration was announced?

The land in this area is vulnerable to future change, wouldn't environmental considerations/protections be an important pillar of this secondary plan? So that developers understand what parts of the lands are to be protected, such as LSNA, wetlands etc. Prior to submitting an application.

The issue that occurs when developers complete their studies is the bias that comes with them paying for a third party to complete the studies. This was recognized by councillor Liggett at the Dec 10th public meeting, asking that the laneway 211 study be reviewed to ensure it was completed properly. Unfortunately, when environment and profit compete, often the environment gets sacrificed. This can be seen in the current laneway 211 development application, even where there are multiple endangered species the development takes a scorched earth approach with no clear measures of how the species will be protected.

Creating a comprehensive secondary plan can guide sustainable development telling the developers what is and isn't allowed. Rather than developers driving how the City is going to evolve.

Thank you  
Patric Malinski

Sent from my Samsung Galaxy smartphone.

----- Original message -----

From: Adam Ripper

Date: 2020-02-07 09:36 (GMT-05:00)

To: Patric Malinski

Cc: Bryan Cooper , Mike Devine , Elaine Brunn Shaw , Shannon Smith

Subject: RE: River Road Secondary Plan - Public Open House

Hi Patric,

For the purpose of the River Road Neighbourhood Secondary Plan, Zoning and Servicing Study, delineation of environmental features will be based on the available existing environmental features mapping. The existing environmental features mapping can be accessed via the Region of Waterloo (e.g., Regional Core Environmental Features), the GRCA (e.g., floodplain mapping) and Land Information Ontario (e.g. MNR wooded area).

Subsequent to the Secondary Plan, Zoning and Servicing Study being completed, any future development applications will be required to conduct an Environmental Impact Study on a property by property basis which will involve conducting field investigations, similar to what has been required for the recent R04/19 442-448 River Road Application which is immediately adjacent to the Locally Significant Natural Area.

As such, field investigations will not be required as part of this Study.

[Adam Ripper, P.Eng.](#)

Project Engineer

The Corporation of the City of Cambridge

50 Dickson Street, P.O. Box 669

Cambridge, ON N1R 5W8

Phone: 519.623.1340 ext. 4778



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**From:** Patric Malinski [REDACTED]  
**Sent:** Thursday, February 06, 2020 3:38 PM  
**To:** Adam Ripper  
**Cc:** Bryan Cooper; Mike Devine; Elaine Brunn Shaw; Shannon Smith  
**Subject:** Re: River Road Secondary Plan - Public Open House

Adam,



Thank you for the updated information. I have reviewed the documents and it seems that the environmental impact is a gap in the current document. On multiple occasions the public has stated their concern for the environmental impact to this area, as it contains Locally Significant Natural Areas. During the public meetings, city staff made mention to an environmental study also being completed for the area.

What is concerning is the timeline that is proposed, when it comes to completing a proper environmental assessment, our resources inform us that this timeline does not cover the required amount of seasons to observe the wildlife. Typically at least four seasons are observed in an environmental assessment. We are already aware of the endangered species that nest in the area covered by this control bylaw..

The contract was awarded in November, even if an environmental assessment began immediately the completion for May/June misses multiple critical seasons for migration, nesting, etc.. Please advise how this will be addressed.

Thank you,

Patric Malinski

----- Original message -----

From: Adam Ripper

Date: 2020-02-06 10:54 (GMT-05:00)

To: Patric Malinski

Cc: Bryan Cooper , Mike Devine

Subject: River Road Secondary Plan - Public Open House

Hi Patric,

I wanted to let you know that the Initial Open House Public Meeting for the River Road Secondary Plan has been scheduled for the evening of Wednesday February 19<sup>th</sup>. I've attached a copy of the formal notice which be sent to the surrounding Property Owners and anyone who was on the mailing list for the (R10/18) 340-360 River Road and (R04/19) 442-448 River Road Applications.

Also, a project page for the River Road Study has been added to the City's website, which can be found here:

<https://www.cambridge.ca/en/build-invest-grow/current-development-applications.aspx>

Finally, the Project Consultant has been directed by City Staff to designate the River Road Neighbourhood Group as a Key Stakeholder in the project.

Regards

Adam Ripper, P.Eng.

Project Engineer

The Corporation of the City of Cambridge

50 Dickson Street, P.O. Box 669

Cambridge, ON N1R 5W8

Phone: 519.623.1340 ext. 4778



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**From:** Patric Malinski [REDACTED]  
**Sent:** Monday, November 25, 2019 9:54 PM  
**To:** Adam Ripper  
**Cc:** Shannon Smith; Elaine Brunn Shaw; Mike Devine  
**Subject:** RE: City of Cambridge Notification - River Road Site Tour

Hello,

Could you please provide an update on the control by-law studies along River Road. During the site tour, we learned that the consulting firm was awarded the contract. Has a project timeline been established? When can we expect the public consultation phase? I would ask that we remain informed and involved in the process.

# River Road Secondary Plan

## **Protect the CHARACTER of the area**

- Established Neighborhood Status for River Road
- River Road City of Cambridge Scenic Route
- Expansion of the Black Bridge Culture Heritage Landscape
- Single family homes on River Road to keep in character with the neighbourhood

## **The “Ultimate” Plan is needed (Attachment No.6)**

- As one project, is the most cost and time efficient approach.
- Definitively, when will the services (road, stormwater, water, gas, septic, road shoulders, lighting, multi-use trail etc.) be put up River Road to start the project?
- When is it Budgeted ?
- Why not as one project rather than piecemeal, especially given that some areas of River Road will change while others remain the same?

# Patterson Planning Consultants Inc.

Via Email

Our File: P-1051

October 19, 2020

**Mr. Bryan Cooper, MCIP, RPP  
Senior Planner  
Community Development Department  
Development Planning Division  
City of Cambridge, 3rd Floor  
50 Dickson Street  
Cambridge, ON, N1R 5W8**

Dear Mr. Cooper:

**Re: Virtual Public Open House #2  
River Road Secondary Plan, Zoning, and Servicing Study  
October 13, 2020**

---

On Tuesday, October 13, 2020, the City of Cambridge and the consultant team associated with the River Road Secondary Plan, Zoning and Servicing Study conducted a virtual open house to present update materials for comment. A current status of the project, suggested timelines and four options for development were presented. The intent being to solicit feedback from the community and to continue to advance forward towards the preparation of a “preferred option” which will be presented to Council.

As you are aware, Patterson Planning Consultants Inc. represents the proponent for the development applications (Zoning By-law amendment and Vacant Land Plan of Condominium) which have been submitted for the lands at 448 River Road (including lands from 442 River Road and portions of Municipal Lane 211). The municipal file number associated with the application is R04/19.

As you are aware, an Informal Public meeting was held regarding R04/19 on December 10, 2019. You may recall at that meeting that the Ward Councillor (Councillor Devine) noted that the number of road connections to River Road should be minimized to the greatest extent possible. We believe this has been further voiced at Open House which was held on February 19, 2020. As such we were quite surprised upon review of the presentation materials that the concepts continue to show multiple vehicular connection points to River Road. This would appear contrary to the public opinion and suggested direction that we have heard to date.

Given the options presented, we are naturally in favour of and support Option 4 which reflects (although modified) our current application which includes development on a private roadway and Olivewood Way being terminated in a cul-de-sac. Our proposal reflects comments that we

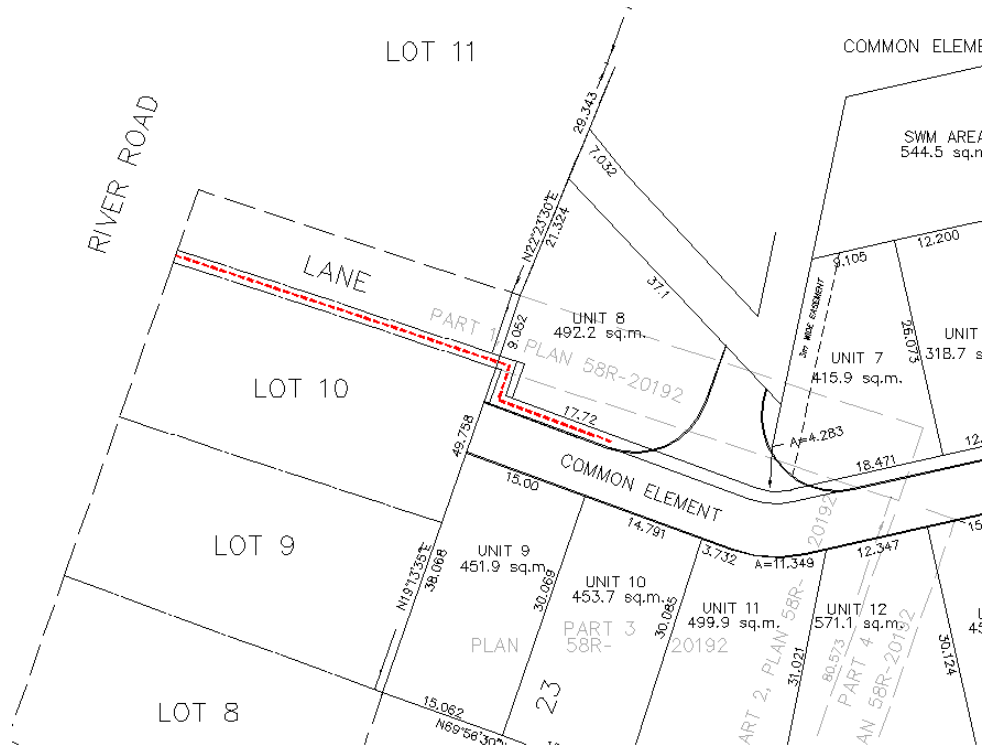
have heard to date and provides a contextually appropriate form of development on the subject lands.

Specifically speaking to the materials that were presented:

1. Water and Servicing - Page 12 of the presentation materials provides some details of the water and servicing considerations for the area. The development application for 448 River Road (with a portion of 442 River Road) is noted on this image; however the limits of the development are incorrect. There are currently two existing Lots of Record associated with 442 River Road which are not included in the development applications. Similarly the image presented would appear to include the lands at 452 River Road. Those lands are not included in the development proposal and should be removed from any presentation materials going forward which falsely give the interpretation that the development project fronts onto River Road. Our project has no direct frontage onto River Road and the intent is for the two Lots of Record that formed part of 442 River Road to be developed with single detached dwellings. We cannot speak to the development interests of 452 River Road however they are not part of our project.
2. Opportunities - Page 13 of the presentation materials would suggest an extension of Olivewood Way to meet River Road is an opportunity for this community. As noted, we believe our current development proposal reflects the public position that vehicular connections to River Road should not be supported. This same image would indicate that the existing structure at 452 River Road has heritage attributes and should be incorporated into the street and block pattern. We would respectfully opine that providing a street through this area would not benefit this property and that the current access configuration for those lands should be maintained (via the laneway). The image also suggests that the extension of Olivewood Way to River Road will terminate and create an "opportunity for improved views and connections to the river and natural features". If Olivewood Way were to be extended to River Road, it would not terminate at a point would allow any views or connections to the River. The image below provides context of the current street view from the terminus of Laneway 211 looking west towards the River. There is no view opportunity in this location without significant tree removals to allow any view of the River. This would greatly impact the current character of River Road and we believe this would run contrary to the desires of the community.



3. Constraints - Page 14 of the presentation materials would provide direction that there is limited development opportunity adjacent to the wetland areas and butternut trees found on the 448 River Road lands. We disagree with this assessment and believe that our development proposal provides justification and support to the provision of suitable buffers to the wetland and butternut trees while still being able to provide needed housing in this area that is contextually appropriate for the existing neighborhood.
4. Lots of Record - Existing Lots 9 and 10 of Registered Plan 590 are not recognized in any of the Options which are included in the presentation material. It is our understanding that these lots can be conveyed individually and they can be developed so long as zoning setbacks and regulations are adhered to. On that basis any options presented should display the existing lot fabric and provide clarification that existing lots can be developed.
5. Option 1 – Option 1 does not accommodate our development proposal and illustrates the extension of Olivewood Way to meet River Road. We are not in support of this option as it does not support the development of 448 River Road or the existing Lots of Record (from 442 River Road) that front onto River Road.
6. Option 2 – Option 2 does not accommodate our development proposal and illustrates the extension of Olivewood Way to meet River Road. We are not in support of this option as it does not support the development of 448 River Road or the existing Lots of Record (from 442 River Road) that front onto River Road. The introduction of “Neighborhood Residential 2” is not appropriate for the River Road area and would not be contextually appropriate to maintain the River Road area’s character.
7. Option 3 – Option 3 does not accommodate our development proposal and illustrates the extension of Olivewood Way to meet River Road. We are not in support of this option as it does not support the development of 448 River Road or the existing Lots of Record (from 442 River Road) that front onto River Road. The introduction of “Neighborhood Residential 2” is not appropriate for the River Road area and would not be contextually appropriate to maintain the River Road area’s character.
8. Option 4 - Option 4 incorrectly notes that a road connection from River Road to Olivewood Way is being proposed. This Option generally recognizes the development proposal put forth for 448 River Road as well as the two existing lots (Lots 9 and 10 of Registered Plan 590). We are not in support of having access to the existing lots facing River Road being provided via rear lane access. The lot fabric fronting onto River Road recognizes lots of substantial lot frontage. Providing individual driveway access for these lots will not impact the character of River Road and should be supported.
9. Pedestrian connections – During the presentation it was noted that a priority of the consultant team was to ensure vehicular and pedestrian connections could be provided to River Road. While we disagree with multiple vehicular access points, we do encourage the provision of pedestrian access points. We have reviewed the options for pedestrian connections from the 448 River Road project and would offer the brief illustration below to suggest that a pedestrian connection from the development through to River Road (via Laneway 211) may be possible. We would be pleased to discuss the opportunities to create this pedestrian connection with City staff and the consultant team.



We thank you for the opportunity to provide comment in regard to the Secondary Plan process that is ongoing. As we have a development application in process, we would welcome the opportunity to work with the consultant team to ensure our proposal will benefit the overall community.

On the basis of the Options provided to date, and given our pending applications, our support rests solely with Option 4 as it somewhat reflects our development proposal for the lands and also recognizes the existing lots with frontage onto River Road.

If you have any questions or require further information, please do not hesitate to contact the undersigned.

Yours truly,

**Patterson Planning Consultants Inc.**

**Scott J. Patterson, BA, CPT, MCIP, RPP  
Principal**

SJP/s

*Copy: Kiah Group (Butternut Grove) LP.*

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External]  
**Date:** Tuesday, October 13, 2020 8:05:48 PM

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Given the elevation differences between the sides of the roads, Are there any considerations to limiting or staging built heights?

Jody

--

Disclaimer: Any meaning derived from this collection of bits is entirely the construction of the reader and may or may not represent the intent of the author.



**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Last River Road Question  
**Date:** Tuesday, October 13, 2020 8:38:55 PM

---

Since we are essentially a bedroom community & a good number of residents look for easy access to the 401, have you examined creating access towards the 401 rather than towards River Road from the new development areas?

Kindest Regards,  
Anne

Anne Horsley



**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Meeting questions River Road  
**Date:** Tuesday, October 13, 2020 8:38:38 PM

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The design plans options provided do not go into depth with the wetlands on the east side of RRd. With the boundary restrictions is there opportunity to turn that area into an additional open green space? And convert that connection road into a pedestrian only path instead?

Sent from [Mail](#) for Windows 10

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Meeting River Road  
**Date:** Tuesday, October 13, 2020 8:54:22 PM

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Despite the inability to have voice questions. Good job to the team relaying the current status of this project. Were headed in the right direction! Thank you to you all.

Patric

Sent from [Mail](#) for Windows 10

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Question - missing construction safety barriers.  
**Date:** Tuesday, October 13, 2020 8:12:32 PM

---

Regarding the Matchbox Prison Complex Reid is erecting between River & Endeavour, where are the Construction Safety Barriers protecting residents (i.e. children & pets) from heavy machinery and other construction site hazards? Existing backyard fencing is either inadequate and missing. Shouldn't we notify Ontario Health & Safety?

For example, when they were clearing, little Johnny could have fallen into the wood chipper.

- Russ Racey  
[REDACTED]

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] question on well and septic  
**Date:** Tuesday, October 13, 2020 8:22:48 PM

---

what will be the impact on current properties on the north/west side of river road who are currently on well and septic? will they have to change?

--

Disclaimer: Any meaning derived from this collection of bits is entirely the construction of the reader and may or may not represent the intent of the author.

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Question  
**Date:** Tuesday, October 13, 2020 8:08:38 PM

---

Can we have a meeting where residents are able to interact with a question and answer period, not just submit a question by email to be answered?

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Re: Question - missing construction safety barriers.  
**Date:** Tuesday, October 13, 2020 8:52:30 PM

---

Unlike building a subdivision on vacant farmland, construction along River Rd. borders residential properties exposing residents to obvious hazards. Securing the perimeter of a construction site often requires **barricades of solid 'anti-climb' fencing** ( $\approx 15\text{ft}$ ) to keep heavy machinery, excavation and other hazards safely out of reach of children, pets, passersby, etc. It also acts as a sound barrier and reduces the spread of dust and pollutants. Tall barricades are commonplace wherever adjoining properties, businesses and people are at risk. Existing private fencing does not meet *Safety* standards for cordoning off a construction zone.

I've seen no information in City planning documents or Reid's proposal and have reason to suspect that this detail was omitted altogether. In other words, Ontario Health & Safety will have to be notified ASAP.

BTW, thanks for tabling my question.

Russ Racey  
[REDACTED]

On Tue, 13 Oct 2020 at 20:41, Russell Racey [REDACTED] wrote:  
My question was evaded. I didn't ask about being safe from mud. I asked about CONSTRUCTION SAFETY BARRIERS.

- Russ  
[REDACTED]

On Tue, 13 Oct 2020 at 20:12, Russell Racey <[REDACTED]> wrote:  
Regarding the Matchbox Prison Complex Reid is erecting between River & Endeavour, where are the Construction Safety Barriers protecting residents (i.e. children & pets) from heavy machinery and other construction site hazards? Existing backyard fencing is either inadequate and missing. Shouldn't we notify Ontario Health & Safety?

For example, when they were clearing, little Johnny could have fallen into the wood chipper.

- Russ Racey  
[REDACTED]

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] RE: Questions  
**Date:** Tuesday, October 13, 2020 8:45:23 PM

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Given the Developments proximity to Speed river, Have the 6 Nations been involved as a stakeholder? There have been lithic artifacts found, with potential for more in the undisturbed land.

Sent from [Mail](#) for Windows 10

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**From:** [Bryan Cooper](#)  
**Sent:** October 13, 2020 8:08 PM  
**To:** [REDACTED]  
**Subject:** RE: Questions

Hi Patric  
Yes emails. I apologize if I wasn't clear.

This message, including any attachments, may contain information which is confidential and/or exempt from disclosure under applicable law, and is intended only for the use of the designated recipient(s) listed above. Any unauthorized use or disclosure is strictly prohibited. If you are not the intended recipient, or have otherwise received this message by mistake, please notify the sender by replying via email, and destroy all copies of this message, including any attachments, without making a copy. Thank you for your cooperation.

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**From:** [REDACTED]  
**Sent:** Tuesday, October 13, 2020 8:08 PM  
**To:** Bryan Cooper  
**Subject:** [External] Questions

Sorry Brian I think it wasn't clear how questions will asked. Are all participants supposed to email you for the next hour

Sent from [Mail](#) for Windows 10



**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] RE: Questions  
**Date:** Tuesday, October 13, 2020 8:15:22 PM

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-In all the options presented, does River Road need the infrastructure placed (SWM, Utilities, ETC.)?

Sent from [Mail](#) for Windows 10

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**From:** [Bryan Cooper](#)  
**Sent:** October 13, 2020 8:08 PM  
**To:** [REDACTED]  
**Subject:** RE: Questions

Hi Patric

Yes emails. I apologize if I wasn't clear.

This message, including any attachments, may contain information which is confidential and/or exempt from disclosure under applicable law, and is intended only for the use of the designated recipient(s) listed above. Any unauthorized use or disclosure is strictly prohibited. If you are not the intended recipient, or have otherwise received this message by mistake, please notify the sender by replying via email, and destroy all copies of this message, including any attachments, without making a copy. Thank you for your cooperation.

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**From:** [REDACTED]  
**Sent:** Tuesday, October 13, 2020 8:08 PM  
**To:** Bryan Cooper  
**Subject:** [External] Questions

Sorry Brian I think it wasn't clear how questions will asked. Are all participants supposed to email you for the next hour

Sent from [Mail](#) for Windows 10

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Re: River Road  
**Date:** Tuesday, October 13, 2020 8:15:30 PM

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Why can we not create a trail without taking down the trees on Olivewood, we have many animals including coyotes that use this space, we need to think of the environment not a subdivision

---

**From:** [REDACTED]  
**Sent:** October 13, 2020 8:07 PM  
**To:** Bryan Cooper <CooperB@cambridge.ca>  
**Subject:** River Road

I will be honest I do not want the trees taken down at all and no open spaces. The elevation is to high on Olivewood way and no one on my street want the street open to houses that look totally different than ours

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] Re: trust  
**Date:** Tuesday, October 13, 2020 8:41:01 PM

---

please read the question as written



Some of these questions are based in trust. We have seen council object to a project and then be forced to/ switch to agree. We have seen buildings torn down and trees torn down while a development freeze is on.

What guarantees do we have that any of this will make a difference? That the next developer with big pockets or small morals won't just get their way?

On Tue, Oct 13, 2020 at 8:39 PM Jody Palmer [REDACTED]

[REDACTED] wrote:

Some of these questions are based in trust. We have seen council object to a project and then be forced to/ switch to agree. We have seen buildings torn down and trees torn down while a development freeze is on.

What guarantees do we have that any of this will make a difference? That the next developer with big pockets or small morals won't just get their way?

- Jody

--

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Disclaimer: Any meaning derived from this collection of bits is entirely the construction of the reader and may or may not represent the intent of the author.

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River access and views  
**Date:** Tuesday, October 13, 2020 8:13:05 PM

---

Where would you locate additional access to the river? There are 4 or 5 sites noted on the diagram but are mostly on private lands. What does this propose?

Kindest Regards,  
Anne

Anne Horsley



A Referral is sending someone you care about  
To someone you trust. Your referrals are always  
Appreciated

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River Road - buildings of interest  
**Date:** Tuesday, October 13, 2020 8:17:47 PM

---

Buildings of interest - it sounds as if these designated buildings are going to be demolished/repurposed, I am hearing they are only going to be kept if structurally sound - heritage buildings often are not, given their age. Some of the options show these buildings being split into multiple lots (eg Option 4)

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River Road  
**Date:** Tuesday, October 13, 2020 8:18:01 PM

---

Is there a plan for municipal water & sewer to the existing homes? Currently these home are on septic & well. Will streetlights be added too?

This could take away from the current 'rural' feel of the road and most residents enjoy the current state.

Kindest Regards,  
Anne

[REDACTED]

[REDACTED] you care about  
To someone you trust. Your referrals are always  
Appreciated

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River Road  
**Date:** Tuesday, October 13, 2020 8:10:29 PM

---

Where will the extra 1 metre of road width come from to improve the walkability on Road?  
There are a lot of homes that are currently very close to the existing road's edge. Are you only addressing the new development sites or also the existing homes?

Kindest Regards,  
Anne

Anne Horsley



A Referral is sending someone you care about  
To someone you trust. Your referrals are always  
Appreciated

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River Road  
**Date:** Tuesday, October 13, 2020 8:07:56 PM

---

Hi Brian

Why do you feel the laneways should empty onto River Road? This will increase traffic on an already overburdened road for pedestrians. It was discussed earlier that these laneways would be well suited to pedestrian traffic only.

Kindest Regards,  
Anne

Anne Horsley

[REDACTED]

[REDACTED]

A Referral is sending someone you care about  
To someone you trust. Your referrals are always  
Appreciated



**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River road  
**Date:** Tuesday, October 13, 2020 8:33:30 PM

---

Has the city looked into the development of the infrastructure on those roads can River Road be dug up that deep

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] River Road meeting  
**Date:** Tuesday, October 13, 2020 8:51:01 PM

---

A comment: to follow up from the last comment read verbatim: this developer is proposing other such unsympathetic developments elsewhere in Cambridge

**From:** [REDACTED]  
**To:** [Bryan Cooper](#)  
**Subject:** [External] river road  
**Date:** Tuesday, October 13, 2020 8:12:54 PM

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Hey - it would be great to have the chat going to ask questions. Then you would not have a wave all at once.

Thank you for this presentation and the opportunity to participate and ask questions. Very interesting, a lot considered. Much appreciated.

A few questions:

Will this be something that would be a part or subset of the City Official Plan or a stand alone reference?

Would you consider pedestrian pathway on the south side of River Road only and allow for multi-use trail on the other side so it is a wider path?

To decrease asphalt on river road, would you consider laneways to the back of the River Road facing homes? It would also keep pedestrian traffic safer.

I apologize if you covered some of this, I was disconnected 3 times during the meeting and may have missed some points.

Thank you  
Tamara Hetherington

**From:**  
**To:** [Bryan Cooper](#)  
**Subject:** [External] sustainability  
**Date:** Tuesday, October 13, 2020 8:07:20 PM

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Is there going to be any effort at sustainability? Will there be LEED Neighbourhood or Zero Carbon requirements?

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**Public Submission Response Table**

Comment	Response
How will the plan impact properties on the south side of River Rd, including retaining walls and mature trees	At this time, it is difficult to determine specific property impacts for reconstruction of River Rd. Interim improvement to River Rd, which can be accommodated within the existing road width are recommended; and an ultimate design which could only be constructed when the City has widened River Rd, as well as budgeted for the reconstruction. For the ultimate design to happen, the City would need to acquire road widenings. This is typically acquired through the development process, so it could be a very long time until the City is able to reconstruct River Rd as per the ultimate street design. There are also challenges with grades, particularly along the properties that back on to the Speed River, so it may not be that the City acquires land from both sides of the street. Prior to any decision to reconstruct River Rd there would be additional public consultation opportunities to discuss specific property impacts.
What will water and sewer service mean for properties that front on River Rd.	If services were installed along River Rd. then properties would be required to connect. However, some properties on the Speed River side would likely not be able to connect due to the steep drop in grades. That would be something that would be looked at on a property specific basis.
Recommended width for private roads cannot be met	The secondary plan policy has been revised to note that a 9 m cross section is not necessarily required if adequate visitor parking can be provided to the satisfaction of the City.

Concerns over tree preservation and removal of trees; protecting the natural environment and wildlife	Trees are an important component of the River Road Secondary Plan area. The policies of the secondary plan seek to maintain the Urban Forest Plan canopy target of 30%. Tree management plans are required for new developments. Retention of existing trees, where possible, is encouraged and new developments are required to provide street trees and compensation plantings for removals.
Homes will look different than existing homes	A variety of home styles and design is encouraged within the secondary plan area. The policies of the secondary plan and implementing zoning by-law include urban design requirements. Site plan approval is recommended to be required for new development fronting onto River Rd.
Timing of interim vs ultimate cross section	<p>Based on the secondary plan it is not anticipated that the development of the properties fronting on River Road will proceed in a single comprehensive development, and as such the interim cross-section improvements on River Road will most likely be a City driven initiative. Note that the timing and funding for the River Road interim improvements is still to be determined and would need to be considered through the City's capital improvement budget program and the next Development Charge Study By-law (if it is to be funded from Development Charges).</p> <p>It is recommended that the Phase 1 interim cross section for River Rd would be utilized until a sufficient amount of road widenings have been acquired to facilitate the design and implementation a multi-use trail.</p>

Will the proposed municipal road from River Rd to McMeeken Dr. have enough allowance for a ladder fire truck	The future municipal road connection from McMeeken Rd to River Rd is proposed as a standard 18.5m cross section which accommodates a fire truck access. Melran Dr. is a 20m right-of-way and was also designed to allow fire truck access.
Will current development projects underway be required to connect to municipal services	340-360 River Road was designed to allow for a future connection when the sanitary sewer is extended on River Road and the development at 442-448 River Road would be required to provide for the same.
Traffic calming is necessary. Lamp post lighting similar to those in areas of Galt with a design sympathetic to the heritage of the area rather than concrete light standards could also contribute to encouraging lower speeds.	The City has implemented Seasonal Traffic Calming signs as a first step to address speeding on River Road. The next step would be to assess the impact those measures have had. If speeding persists based on the City's Traffic Calming Policy further measures including physical traffic calming would be considered. If physical traffic calming is warranted, then pending budget, implementation could proceed prior to reconstruction of River Road or depending on timing could be incorporated into reconstruction. Street lighting is not a tool the City uses for traffic calming. Lighting details will be considered as part of detailed design.
In considering fencing/barriers, it would be immensely appreciated that there be NO chain link fence. Instead, the creative use of landscape buffers and decorative fencing would be more sympathetic to the area views.	The secondary plan policy includes references to the use of low-level decorative fencing around front and exterior side yards and that front yard areas should be large and naturalized to maintain the rural character of the area. Landscaping plans can be required as part of the site plan approval process for new development along River Rd.
The Cultural Heritage Resource Assessment noted that River Road merits recognition by the	Should Council decide to pursue designation of River Road as a

<p>City of Cambridge as a City of Cambridge scenic route as well as potential CHL or extension of the Black Bridge CHL. What are the next steps in this process from identification to establishment of one or both designations?</p>	<p>Cultural Heritage Landscape, the next step would be to develop a Technical Study as per the Region's Implementation Guidelines for Cultural Heritage Landscape Conservation. The Technical Study, along with a staff recommendation report will be provided to the Municipal Heritage Advisory Committee to be received and whether to recommend Council designate the area as a CHL through an Official Plan Amendment. Should Council agree to designate, the CHL would be designated under the City's Official Plan.</p>
<p>Is there a developer that wants to redevelop the larger lots in the community and does the City have a stake in any such development?</p>	<p>Staff are not aware of any other development proposal beyond 340-360 River Rd (under construction) and the current zoning by-law amendment/vacant condominium proposed at 442/448 River Rd. The City does not have a stake in any development project. The portion of laneway #211 is proposed to be acquired and integrated into the proposal at 442/448 River Rd, if approved. The remaining portion will continue to be used as a laneway and access for 452 River Rd.</p>
<p>Is the City taking away people's homes?</p>	<p>The City is not expropriating any homes. The current road width of River Rd. is approximately 14.0 m. The ultimate plan for River Rd. is for a 20 m road allowance to allow for servicing and pedestrian/cycling facilities. Over time land will therefore be required in the form of road widenings to facilitate the planned street improvements. Land dedication can be acquired through the redevelopment process or through the City purchasing/expropriation.</p>
<p>Why does the City want smaller lots in this area?</p>	<p>For properties fronting on River Rd, the proposed secondary plan and</p>



	<p>zoning by-law amendment seeks to maintain larger residential lots (30 m/100 ft frontage). The proposed zoning allows single detached, semi-detached and triplex homes on 30 m wide lots. The River Rd area was added to the urban area in the City through the last update to the City's Official Plan. When this was done, the area was designated for Low and Medium Density residential land uses, with a density allowance of 40 units per hectare of land. New development is required to be on municipal sanitary and water services. These changes were made in 2012.</p> <p>The proposed River Rd Secondary Plan reduces the overall density for the study area from 40 units per hectare to a range between 12-16 units per hectare, and 9-13 units per hectare along River Rd. itself. The central area of the study by the extension of McMeeken Dr. is planned for development up to 40 units per hectare, which is consistent with the current Official Plan permissions. If the secondary plan and zoning by-law is approved this will result in less intensive development along River Rd. than what is already permitted in the current Official Plan.</p>
<p>Neighbourhood is concerned with three connections to River Rd</p>	<p>The preparation of the Secondary Plan has been an iterative process with different concepts for the area developed over time and based on public and staff feedback. Initial proposals identified three municipal road connections – Alderson, McMeeken Dr. and Olive Wood Way. The final recommended plan has been revised to include only one municipal connection at McMeeken Dr. A pedestrian connection is proposed over Olivewood Way.</p>

Concerned with redevelopment at Townline and River Rd; most homes are relatively new homes. Is it acceptable to teardown these homes?	A previous demonstration plan showed how this property could potentially be redeveloped. The current demonstration plan (conceptual only) no longer shows this redevelopment. The current policies and zoning would permit development on River Rd up to triplexes on large (30 m <sup>2</sup> /900 m <sup>3</sup> area lots). The City does not have a demolition control by-law to prevent the demolition of homes if a property owner wishes to rebuild in accordance with the zoning by-law.
Concerns with traffic control, would like 40 km speed limit with speed bumps, similar to Riverbank Dr. Traffic is getting higher and worse and speed is an issue.	The City has implemented Seasonal Traffic Calming signs as a first step to address speeding on River Road. The next step would be to assess the impact those measures have had. If speeding persists based on the City's Traffic Calming Policy further measures including physical traffic calming would be considered. If physical traffic calming is warranted, then pending budget, implementation could proceed prior to reconstruction of River Road or depending on timing of any reconstruction incorporated into said reconstruction.
New neighbourhoods should be developed with pedestrian and bicycle access to River Rd. but not cars.	The current recommended plan proposes a single municipal road connection to River Rd. Olivewood Way is proposed to be connected with a pedestrian walkway only.
We imagine River Rd with bicycle trails, a walking path tied into the Blackbridge Cultural Heritage Landscape.	The ultimate cross section includes a multi-use trail that would allow separated pedestrian and cycling.
Concerns with the format of the October 13 2020 virtual meeting and inability to ask questions live; concerns with the need to access technology	Acknowledged. Following the October 13, 2020 meeting staff and a small working group met virtually to further discuss the proposed plans.
The interim control by-law should be extended until the zoning by-law is updated to match the secondary plan.	An implementing zoning by-law is recommended to be approved at the same time as the secondary plan. The

	interim control by-law will be repealed once this is in effect.
Concern that no environmental assessment was being completed as part of the study.	<p>For the purpose of the River Road Secondary Plan delineation of environmental features was based on the available existing environmental features mapping (Regional Core Environmental Features), the GRCA (e.g., floodplain mapping) and Land Information Ontario (e.g. MNR wooded area).</p> <p>Subsequent to the Secondary Plan, Zoning and Servicing Study being completed, any future development applications will be required to conduct an Environmental Impact Study on a property by property basis which will involve conducting field investigations, similar to what has been required for the recent R04/19 442-448 River Road Application which is immediately adjacent to the Locally Significant Natural Area.</p>
<p>I am looking for additional information around the city's declaration of climate crisis and what that means for planning and development. Have there been any significant changes to the city's approach to planning and development-recognizing that the most significant changes to the landscape/environment falls under this category. Could you point me in the right direction of any new policies/documents on this topic since the declaration was announced?</p> <p>The land in this area is vulnerable to future change, wouldn't environmental considerations/protections be an important pillar of this secondary plan? So that developers understand what parts of the lands are to be protected, such as LSNA, wetlands etc. Prior to submitting an application.</p>	<p>Any proposed improvements to the River Rd road allowance would require review by GRCA since it is within their regulated area.</p> <p>The City has prepared a Climate Adaptation Plan:  <a href="https://www.cambridge.ca/en/resource/s/rpt_cambridge_climate_adapt_plan_20190823_access.pdf">https://www.cambridge.ca/en/resource/s/rpt_cambridge_climate_adapt_plan_20190823_access.pdf</a></p> <p>Environmental considerations have been included in the preparation of the secondary plan – City, Regional and GRCA constraints have been included.</p> <p>Individual environmental impact studies will still be required at the detailed plan submission – these are completed by professional consultants and reviewed by professional City,</p>

	<p>Regional or Conservation Authority staff.</p> <p>Planning decisions are required to conform to the 2019 Provincial Growth Plan – A Place To Grow. Climate change and mitigation is noted throughout the Growth Plan. Part of the focus on developing within existing built-up areas - which the River Rd neighbourhood and study area is – is to support transit, active transportation and minimizing land consumption through compact built form. This is in contrast to continual outward suburban expansion on the outer edges of the cities which has historically occurred in Ontario. Infilling is more cost effective from a servicing perspective, minimizes land consumption, preserves environmental and agricultural land and reduces car dependency, which are all good things from an environmental perspective.</p>
River Rd. area should be included as an established neighbourhood	<p>Through the City's comprehensive zoning by-law review, established neighbourhood criteria have been created. An established neighbourhood must meet the following criteria: have a high percentage (60% or higher) of the residences built prior to 1948; a low percentage of potentially severable lots (less than 5%); a high percentage of detached garages (over 25%) usually found in the rear yard serviced by lanes; the majority of residences being of a consistent height (typically one or two storey) and the majority of residences exhibiting a setback consistent with the adjoining properties.</p> <p>Although not meeting the strict definition of "an established</p>

	<p>neighbourhood” it is acknowledged that River Rd. and the surrounding area is a unique neighbourhood in the City of Cambridge. Part of the recommendations of the secondary plan studies is for the City to consider a cultural heritage landscape study for the area and that River Rd. should qualify as a Scenic Road and that the City develop policies around scenic roads. In addition, site specific development standards have been prepared in the Secondary Plan policy and implementing zoning by-law to maintain important characteristics of the River Rd. area. These include standards such as averaging front yard setbacks for new buildings, building height regulations and new standards for parking and garages; many of which are similar standards that are proposed to be applied to other “established neighbourhoods” in the City.</p> <p>Finally, a modified rural cross section has been developed to guide the future reconstruction of River Rd.</p>
River Rd should be a Cambridge Scenic Road	The Cultural Heritage Report recommends that River Rd. should qualify as a Scenic Road and that the City develop policies around scenic roads. Creating local scenic road policies have been identified as an item in the City’s 2022 Official Plan update.
The Black Bridge Cultural Heritage Landscape should be expanded to include River Rd.	Staff is recommending that the City conduct a study to determine whether the area should qualify as a Cultural Heritage Landscape area.
River Rd. should be limited to single family homes to keep in character with the neighbourhood	The City cannot zone based on a family unit/relationship. The proposed zoning for River Rd allows single detached, semi-detached and triplex

	<p>dwelling on large residential lots. The implementing policies and by-law have been developed to maintain important characteristics of River Rd.</p>
<p>The ultimate cross section for River Rd. should be implemented</p>	<p>At this time River Rd. is not wide enough to accommodate the ultimate cross section. Staff is recommending that the interim cross section be included in the next budget cycle so that a pedestrian walkway can be constructed along River Rd sooner.</p>
<p>When will services (road, stormwater, gas, septic, road shoulders, lighting, multi-use trail) be constructed in River Rd.</p>	<p>The ultimate reconstruction and servicing of River Rd. could be many years away since the City does not have the required road width to accommodate infrastructure. Road widenings are normally acquired through the development review and approval process.</p>
<p>When is the road budgeted?</p>	<p>Staff is recommending that the interim road improvements be included in the next budget cycle (2022)</p>
<p>Why are the improvements not completed as one project instead of piecemeal?</p>	<p>As noted, the City is unable to implement the ultimate road design due to land constraints. Road widenings will be acquired over time; but the physical reconstruction of River Rd for the ultimate design would be completed as one project and not piecemeal.</p>
<p>Preference for the development scenario where Olivewood Way is developed on a private roadway and terminated as a cul-de-sac.</p>	<p>The proposed secondary plan identifies Olivewood Way as a private road with a pedestrian connection to River Rd. The exact details of this development proposal will be considered through the site-specific application for 442/448 River Rd.</p>
<p>There are currently two existing Lots of Record associated with 442 River Road which are not included in the development applications. Similarly, the image presented would appear to include the lands at 452 River Road.</p>	<p>Noted.</p>

<p>Those lands are not included in the development proposal and should be removed from any presentation materials going forward which falsely give the interpretation that the development project fronts onto River Road.</p>	
<p>Opportunities - Page 13 of the presentation materials would suggest an extension of Olivewood Way to meet River Road is an opportunity for this community. As noted, we believe our current development proposal reflects the public position that vehicular connections to River Road should not be supported. This same image would indicate that the existing structure at 452 River Road has heritage attributes and should be incorporated into the street and block pattern. We would respectfully opine that providing a street through this area would not benefit this property and that the current access configuration for those lands should be maintained (via the laneway). The image also suggests that the extension of Olivewood Way to River Road will terminate and create an “opportunity for improved views and connections to the river and natural features”. If Olivewood Way were to be extended to River Road, it would not terminate at a point would allow any views or connections to the River.</p>	<p>Noted. The final plan shows only one municipal road connecting to River Rd (McMeeken Dr. extension). Olivewood Way is identified as a private condominium road with a pedestrian connection.</p>
<p>Constraints - Page 14 of the presentation materials would provide direction that there is limited development opportunity adjacent to the wetland areas and butternut trees found on the 448 River Road lands. We disagree with this assessment and believe that our development proposal provides justification and support to the provision of suitable buffers to the wetland and butternut trees while still being able to provide needed housing in this area that is contextually appropriate for the existing neighborhood.</p>	<p>Noted. The site-specific environmental impact study for this development application will determine the limits of development and will be required to be approved by the Region of Waterloo and Grand River</p>
<p>Lots of Record - Existing Lots 9 and 10 of Registered Plan 590 are not recognized in any of the Options which are included in the presentation material. It is our understanding</p>	<p>Noted.</p>

that these lots can be conveyed individually and they can be developed so long as zoning setbacks and regulations are adhered to. On that basis any options presented should display the existing lot fabric and provide clarification that existing lots can be developed.	
Pedestrian connections – During the presentation it was noted that a priority of the consultant team was to ensure vehicular and pedestrian connections could be provided to River Road. While we disagree with multiple vehicular access points, we do encourage the provision of pedestrian access points. We have reviewed the options for pedestrian connections from the 448 River Road project and would offer the brief illustration below to suggest that a pedestrian connection from the development through to River Road (via Laneway 211) may be possible. We would be pleased to discuss the opportunities to create this pedestrian connection with City staff and the consultant team.	Noted.
Private servicing systems improvements and maintenance in interim until road improvements could then be redundant if residents along River Road required to connect to services in future; Whether people on private services will need to connect; in particular on Speed River side;.	It should be noted that there are a number of existing low-lying properties on the River side of River Road which may not be able to connect to the future sanitary sewer on River Road via a gravity connection. This is due to the existing homes being situated much lower than the road. When municipal sewers are extended on River Road in the future, the City will not force these properties to connect to municipal services if a gravity connection is not feasible.
How do people participate in the “roll out of the plan”	There will be additional public consultation opportunities will River Rd is eventually reconstructed.
If some of those homeowners do sell, the plan should prevent out of character development.	The policies and zoning standards are intended to ensure that new development is compatible with the exiting character of the



	neighbourhood.
Reference to detached homes along Highway 2 in Cobourg	Staff have reviewed the online zoning for the City of Cobourg. Although there are a number of single detached homes along much the Highway 2/King St corridor; many appear to be zoned to permit singles, semi-detached and duplex dwellings on comparatively smaller lots (e.g. 370 m <sup>2</sup> ) than what is proposed in the regulations for River Rd (minimum 900 m <sup>2</sup> ).

## 3.0 Engagement Approach

The findings of the Background Review, Opportunities and Constraints Analysis, and Public Information Centre No.1 will help establish a vision and potential development options for the Study Area. We will use these findings to develop a series of frameworks that help encapsulate the high-level build-out of the area, and provide an overarching reference to guide future development, infrastructure projects, public investments, etc.

**60+**  
participants

**2**  
local councillors

**6**  
city staff

**5**  
consultants

### 3.1 Public Information Centre No. 1 Open House and Working Session

The City and the Consultant Team hosted an Open House/Working Session on Project Introduction, Background Policy Review and Transportation and Servicing Assessment on February 19, 2020 at the Hespeler Arena. The objective of the open house/working session was to introduce the project, present the background review and analysis, and work with participants to refine and augment the opportunities and constraints analysis to reflect their issues/concerns. A series of questions were also presented for consideration to better understand how participants experience River Road and the Speed River, as well as, any specific views and vistas they thought should be preserved, additional connections/links, and strategies to ensure fit in the neighbourhood.

The Consultant Team hosted an Open House which allowed participants to drop-in, review background review and analysis, preliminary opportunities and constraints, and provide their comments to a member of the Consultant Team. Following this a hands-on working session was held to solicit in-depth feedback from participants. The workshop began with a presentation by the Consultant Team, including:

- / Introduction (Study Team, Study Overview, Study Timeline, Study Area)
- / Background Review (Policy Analysis, Walking Tours, Transportation Assessment, Servicing Assessment, and Cultural Heritage Assessment)
- / Opportunities and Constraints
- / Working Session Questions
- / Next Steps

The following provides an overview of the agenda that was followed on the day of the Public Information Centre:

The presentation can be found on the project website at [www.cambridge.ca](http://www.cambridge.ca) or the appendices.

## Public Information Centre No. 1 Agenda

### Open House 6:00-6:45PM

Key information was presented on a series of display boards. This allowed participants who are unable to stay for long will have the opportunity to engage with members of the Consultant Team and provide quick feedback on a comment sheet.

### Presentation 6:45PM-8:00PM

The consultant team presented their initial findings from the background review, and the opportunities and constraints analysis. The background review included an overview of the background planning and policy context, environmental context, cultural heritage context, existing transportation assessment, and an existing servicing assessment. Following the presentation, participants worked in small groups to answer a series of questions related to elements of the study and study area.

### Working Session 8:00-9:30PM

Groups were given the chance to provide any additional considerations they think should be explored in the River Road Secondary Plan. The following questions were asked to better understand how residents and the public use and experience River Road:

- / **Question 1** - How do you experience River Road? Do you drive, walk, cycle?
- / **Question 2** - How do you experience Speed River? Are there any opportunities to experience the River?
- / **Question 3** - Are there any specific views and vistas that ought to be preserved?
- / **Question 4** - Are there additional vehicular or pedestrian only links that should be implemented in the area?
- / **Question 5** - Based on current permitted land use policy, what design strategies would you like to see implemented to ensure fit in the neighbourhood?

Providing two opportunities for feedback (open house/working session) ensured interested participants were able to provide feedback regardless of the time they had available.

## What we Heard



### Summary

#### **Question 1 - How do you experience River Road? Do you drive, walk, cycle?**

Based on the responses received, a majority of respondents live, drive, walk and cycle along River Road.

#### **Question 2 - How do you experience Speed River? Are there any opportunities to experience the River?**

Many respondents enjoy the natural features associated with the river, and have identified opportunities to improve experiences along the river.

#### **Question 3 - Are there any specific views and vistas that ought to be preserved?**

Respondents have expressed that they want to keep the rural nature of the River Road, preserving the current right-of-way as well as the views and vistas associated with it.

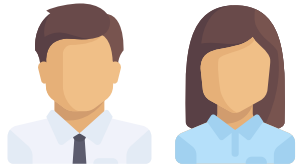
#### **Question 4 - Are there additional vehicular or pedestrian only links that should be implemented in the area?**

Respondents expressed their concerns with adding new vehicular connections and increasing traffic, and suggested introducing traffic calming measures.

#### **Question 5 - Based on current permitted land use policy, what design strategies would you like to see implemented to ensure fit in the neighbourhood?**

Respondents commented on keeping the area low-density, and single-detached with regards to built form, preserving the existing character, ensuring carbon neutral development, and providing adequate transition between existing and proposed development.

## What we Heard



## Actual Responses

### Question 1 - How do you experience River Road? Do you drive, walk, cycle?

- / Drive, walk and live on the road
- / Prefer to drive River Road for the pleasant experience
- / Drive, walk, cycle and live on it
- / Designated bike trail
- / Walking and cycling is precarious
- / Traffic slowing bollards have made walking and cycling more dangerous
- / Walk entire length of River Road and cycle along the road
- / Like the low level of lighting, keeps rural feel
- / All, and run
- / As a scenic route
- / Drive, walk dog, bike, horse riding

### Question 2 - How do you experience Speed River? Are there any opportunities to experience the River?

- / Appreciate the beautiful nature associated with the river
- / The views and experience of the river must not be compromised
- / Visual Enjoyment/Viewing, Boating, Bird Watching, Fishing
- / Limited/No access to river
- / Connect through SPS to river lookout
- / Ellacott or Blackridge Road access
- / Wildlife observation
- / Joe Farwell paddles on this section
- / Go down to the lookout
- / Amazing number of photographers there
- / Canoeing/paddling the river
- / Public access points to the River
- / Sitting on a dock
- / Annual canoe/kayak race
- / Photography

### Question 3 - Are there any specific views and vistas that ought to be preserved?

- / The entire riverside along River Road
- / No additional access (Road) to River Road
- / Prefer no lighting
- / Do not widen, no sidewalks, no trails
- / The view at the foot of Laneway 211
- / Preserve foliage and river greenery
- / Keep development off the river side
- / Cover up pumping stationing – blight on the landscape
- / Preserve all views and vistas
- / Want to reduce the light pollution
- / Save the trees – reduce greenhouse gas
- / Ability to protect the vista for feel and character for the film industry – economic advantage
- / The road should be scenic and lined with trees
- / Lilac trees along River Road

### Question 4 - Are there additional vehicular or pedestrian only links that should be implemented in the area?

- / We want reduced traffic and fewer vehicles, not more
- / Vehicles driveway too fast already
- / Large percentage blow right through existing stop signs
- / Install speed bumps east and south west of pumping station
- / No streetlights, we do not want street pollution
- / Safety improvements
- / Traffic calming
- / No access to the river for the public
- / Consider turtle crossing all along River Road
- / No more links
- / Defer more through traffic
- / Calming features (i.e. large speed bumps (similar to Avenue Rd)
- / Pedestrian only
- / We want this to be a scenic route
- / Reduced speed limit to 40 km/hr
- / Do not expand/width the road, this will act as traffic calming
- / Okay with walkways and pathways but not more roads

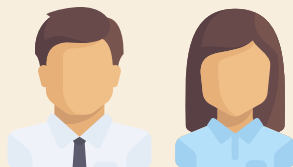
**Question 5 - Based on current permitted land use policy, what design strategies would you like to see implemented to ensure fit in the neighbourhood?**

- / Lower density dwellings – rezone to be single home only (if you can rezone up, you can rezone down)
- / No city water
- / No city septic
- / Single detached homes on larger lots (50' x 150')
- / Enforce carbon neutral development consistent with City's climate crisis
- / Uphold appropriate setbacks
- / Carbon neutral development
- / Need to clear definition of transition from Queen E to River Road
- / Move Traffic toward HWY 29, away from River Road
- / Need to screen the pumping station
- / Height kept down on structures in line with existing homes
- / Quality facades in keeping with the character of the area (vintage) – Like City of Waterloo
- / Maintain certain number of mature trees on lots
- / Smaller homes, larger lots
- / Bungalows, single family homes
- / Height of home to respect views of current residences
- / Maintain low density and not allow exceptions to change the zoning by-law
- / Designated scenic route
- / Area that accommodates natural habitat, frogs, birds, snapping turtles, fireflies and swans
- / Zero carbon building standard
- / Protected well water and spring water

**Other/Additional Questions/Comments**

- / The community group wants to be involved on a regular basis and throughout the process. The community group has also offered and suggested that the team attend a walking tour (including trails) with the community group. Residents also requested access to the raw traffic data.
- / Concerns were raised about swan migration and natural features.
- / Residents felt that it's a wonderful community and are happy with the existing conditions.
- / Copper St/Townline and Copper St/Queen are no truck zones
- / River Road is built on bedrock
- / Who defined the study area boundaries?
- / Ward 2 Councilor Mike Devine had the opportunity to walk thru community
- / Are there people interested in developing lands?
- / What peaked this assessment/study?
- / Forbes Estate developer in Draft Plan of Subdivision
- / Climate change crisis has been declared by the City, how is this study addressing climate change
- / At the conclusion of study, will the zoning be changed?
- / The environment is one of the most amazing things, being able to keep windows open at night and hearing living things
- / Control ponds were dredged and destroyed wildlife. No environmental assessment available/provided.
- / Area doesn't need to be improved
- / Improve by leaving alone
- / Trucks and heavy equipment take place, what's the accessibility of the road traffic and construction?
- / What input do residents actually have in the process?
- / Are there any enhancements that would disqualify River Road from being a scenic road?
- / RE: environmental protections, are you able to add protections to existing green space?
- / Opportunities to liaise with Residents' Group
- / Why isn't GRCA here? What is taking place in the area?
- / Why are we building here?
- / Why not one home on one lot vs a row of towns?

## Questions and Answers



### FAQ's from October 13, 2020 Open House

**1. Given the elevation differences between the sides of the roads, are there any considerations to limiting or staging built heights?**

Our initial and preferred land use concept(s), as well as accompanying land use/secondary plan policies will include considerations/suggestions to address the change in grade/elevation.

**2. Is there going to be any effort at sustainability? Will there be LEED Neighbourhood or Zero Carbon requirements?**

Sustainability will be integral with the development of the land use plan as well as the accompanying land use policies. While the community will not be assessed/rated for LEED ND certification, many of the LEED ND structuring elements such as location, pattern and design, and green infrastructure have been considered and incorporated into the development of the land use plans.

**3. I do not want the trees taken down at all and no open spaces. The elevation is too high on Olivewood way and no one on my street want the street open to houses that look totally different than ours.**

The initial and preferred land use concept(s) will take into consideration the removal of trees and potential issues around elevation/grading. Proposed street connections and access will be assessed in the later stages of this study, and will be further assessed if a development application is made.

**4. Why do you feel the laneways should empty onto River Road? This will increase traffic on an already overburdened road for pedestrians. It was discussed earlier that these laneways would be well suited to pedestrian traffic only.**

The proposed extension of McMeeken Drive and Olivewood Way have been aligned with existing laneways 210 and 211, respectively. Both laneways intersect with River Road and are currently open to vehicular access. The consultant team will explore opportunities to limit the number of through connections to River Road and provide pedestrian access.

**5. Can we have a meeting where residents are able to interact with a question and answer period, not just submit a question by email to be answered?**

Due to restrictions associated with COVID-19 the City is unable to host in person meetings at this time. Following the October 13 2020 digital meeting, the City requested interest from area residents/landowners for participation in a small working group to allow for dialogue and interaction between the public and staff/consulting team. This working group is being formed and a meeting will take place in early December to discuss a preferred plan for the area.

**6. I am wondering how the timeframe looks in regards to the current proposal for 442-448. Will that proposal be placed in a hold position until community feedback has been received and evaluated, and finally communicated to the public later this year / early next year?**

The City is continuing to review the zoning by-law amendment/draft plan of condominium application submitted for 442/448 River Rd (file R04/19) concurrent to the review of the secondary plan. Any public comments on this file can be directed to Bryan Cooper [cooperb@cambridge.ca](mailto:cooperb@cambridge.ca). A recommendation report will be brought forward for consideration upon the completion of the review of the file and at a time when the secondary plan is sufficiently advanced.

**7. Where will the extra 1 metre of road width come from to improve the walkability on Road? There are a lot of homes that are currently very close to the existing road's edge. Are you only addressing the new development sites or also the existing homes?**

Road widenings may occur with new development. New development or re-development may be required to provide lands to accommodate road widenings. The secondary plan will illustrate, propose or identify future road widths (or rights-of-way), therefore, when development or re-development occurs, a plan/vision is already in place.

**8. Regarding the recently approved development proposal at 340-360 River Rd. where are the Construction Safety Barriers protecting residents (i.e. children & pets) from heavy machinery and other construction site hazards? Existing backyard fencing is either inadequate and missing. Shouldn't we notify Ontario Health & Safety?**

Erosion control measures have been installed and inspected on the site in accordance with the City's grading by-law requirements. This includes silt fencing around the perimeter of the development site but there are no requirements for construction safety barriers for this grading work and the City does not have any mechanism requiring the Developer to install any.



**9. Will the secondary plan be something that would be a part of or subset of the City Official Plan or a standalone reference document?**

The Secondary Plan will be written as a standalone document, however, through an Official Plan Amendment, the Secondary Plan will be incorporated into and/or referenced as part of the Official Plan. Confirm with Mark

**10. Would you consider pedestrian pathway on the south side of River Road only and allow for multi-use trail on the other side so it is a wider path?**

The proposed road cross-sections are based on the existing right-of-ways and minimum road widening. The cross-sections propose a potential road widening along the south/east portion of River Road, due to environmental constraints, such as slope erosion hazards), and elevation challenges for properties along the river (west side of River Road).

**11. To decrease asphalt on River Rd, would you consider laneways to the back of the River Rd. facing homes? It would also keep pedestrian traffic safer.**

Our various land use options do proposed the use of rear-laneways along the rear of River Road facing homes. The use of rear-laneways would significantly reduce the number of driveways and access points along River Road, and maintain the rural character of the road.

**12. Where would you locate additional access to the river? There are 4 or 5 sites noted on the diagram but are mostly on private lands. What does this propose?**

Our land use options consider key view corridors and potential access points to the river, however, due to landownership (private lands) and environmental constraints/hazards for properties along the river, we are not proposing specific locations for access to the river but rather identifying future potential opportunities. Actual access and connections to the river shall be determined through future development applications or future studies.

**13. In all the options presented, does River Road need upgraded infrastructure) (stormwater management, utilities, etc.) Where would this be placed?**

In general, upgrades to River Road will be recommended to accommodate an improved road cross section that includes pedestrian facilities. At the time of that upgrade, it would be recommended to include municipal services (storm, sanitary and watermain) and utility upgrades, as appropriate.

**14. Why can we not create a trail without taking down the trees on Olivewood Dr, we have many animals including coyotes that use this space, we need to think of the environment not a subdivision.**

As part of any development application within the Study Area, an Environmental Impact Statement will be required to assess and explore the future potential impacts and potential migration strategies.

**15. Buildings of interest - it sounds as if these designated buildings are going to be demolished/repurposed, I am hearing they are only going to be kept if structurally sound - heritage buildings often are not, given their age. Some of the options show these buildings being split into multiple lots (eg Option 4)**

Our land use options take into consideration a series of existing and potential cultural heritage resources within and adjacent to the study area. Our land use options illustrate where and how these potential cultural heritage resources may be integrated or incorporated into future development. There are twelve properties identified as potential cultural heritage resources merit evaluation under Ontario Regulations 9/06 to determine whether they contain heritage value. Further evaluation both from a heritage value and structural point of view will be considered as part of a development application process.

**16. Is there a plan for municipal water & sewer to the existing homes? Currently these homes are on septic & well. Will streetlights be added too? This could take away from the current 'rural' feel of the road and most residents enjoy the current state.**

In general, it is the City's / Region's intent to eliminate private water and sewage systems where municipal services are present or can be implemented. If / when the preferred option includes municipal water and sanitary servicing along River Road, then the opportunity to convert private wells / septic systems to municipal services would be presented to property owners.

**17. what will be the impact on current properties on the north/west side of river road who are currently on well and septic? will they have to change?**

In general, it is the City's / Region's intent to eliminate private water and sewage systems where municipal services are present or can be implemented. If / when the preferred option includes municipal water and sanitary servicing along River Road, then the opportunity to convert private wells / septic systems to municipal services would be presented to property owners.

**18. Has the city looked into the development of the infrastructure on those roads can River Road be dug up that deep?**

Depth of excavation will generally be driven by the depths required to achieve gravity drainage for a proposed sanitary sewer to the existing sanitary pumping station. Existing ground conditions (eg. Bedrock, high groundwater) may make the construction more challenging, but this is a typical condition in Cambridge which can be overcome using appropriate construction methods.

**19. The design plan options provided do not go into depth with the wetlands on the east side of River Rd. With the boundary restrictions, is there opportunity to turn that area into an additional open green space? And convert that connection road into a pedestrian only path instead?**

The design plan has taken into consideration any natural features layers (i.e. wetlands, floodplains, etc) that have been provided as part of this study. Our land use options do not proposed development within wetlands, floodplains, or any hazard lands. These layers and features will be further refined through an Environmental Impact Statement associated with a development application.

**20. Since we are essentially a bedroom community & a good number of residents look for easy access to the 401, have you examined creating access towards the 401 rather than towards River Road from the new development areas?**

The collector road network within and surrounding the study area comprises of River Road, Melran Drive and Townline Road. There are already good established connections from this neighbourhood to Townline Road via River Road, Renner Drive or Melran Drive. There are multiple connections to Melran Drive from the neighbourhood. However, There are only two access to River Road. Those being at each end of the neighbourhood, Townline Road and Melran Dr. There are currently no local road connections to River Road in this area.

While many drivers do head towards the 401, the majority of trips still occur within Cambridge. Therefore providing an additional access to River Road provides residents with a more well distributed road network with convenient access into and out of the neighbourhood in all directions.

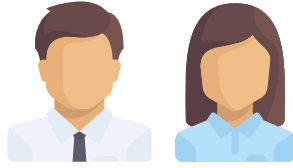
**21. Some of these questions are based in trust. We have seen council object to a project and then be forced to/ switch to agree. We have seen buildings torn down and trees torn down while a development freeze is on. What guarantees do we have that any of this will make a difference? That the next developer with big pockets or small morals won't just get their way?**

A main focus of the secondary plan study is to review the existing land use planning framework for this area of the City in order to update existing policy and zoning to guide new development. This will include a formal amendment to the City's official plan and a zoning by-law amendment which will detail specific development standards for the area. The secondary plan study was initiated in part following significant feedback from the community in response to recent development. New development will be required to conform to the policies of the secondary plan once completed. While it is true that any landowner may submit an application to consider a change to the zoning/official plan that applies to a property, a completed secondary plan/zoning by-law update for this area will provide much more direction for new development than what is provided for in the current land use planning framework.

**22. Given the development's proximity to Speed river, have the 6 Nations been involved as a stakeholder? There have been lithic artifacts found, with potential for more in the undisturbed land.**

We have not specifically been in contact with the 6 Nations. An archeological study may be required as part of a development application. Requirements will be determined as part of a pre-consultation process of a development application.

## What we Heard



## Survey Response Summaries

### **Question Set 1 - What do you like or dislike about the Village 1 precedent?**

- / We heard that respondents liked having less density (and not a lot of houses), and commented on images with more mature trees and generally that existing trees should be maintained. A comment was made regarding the small town, rural design, and landscape that the precedent maintained and that the massing and built form matched the existing character of the area. The need for trail, connections and corridors were also mentioned in the feedback.
- / We heard that some respondents expressed that the precedent images shown do not fit the street, are too dense, and that the existing rural atmosphere of River Road is lost. A comment was made about the incorporation of modern technology and design, while respecting built heritage and environmental preservation.

### **Question Set 2 - What do you like or dislike about the Village 2 precedent?**

- / We heard that respondents liked the treed boulevard between the road and the houses in one of the precedent images, and commented on the ability to maintain old town design.
- / Some respondents commented on the precedent shown being far more dense than existing development and that the images look more suburban, don't retain the existing rural feel, character or natural scenic aspects of River Road.

### **Question Set 3 - What do you like or dislike about the Neighbourhood 1 precedent?**

- / Some respondents liked the mix of architectural styles and mix of housing shown in the precedent images.
- / We heard that the precedent images illustrated too much density, and that the rural feel of River Road would be lost, resulting in a cookie cutter neighbourhood/subdivision. Comments around sufficient parking and the natural environment were also brought up.

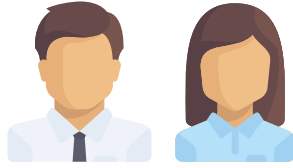
### **Question Set 4 - What do you like or dislike about the Neighbourhood 2 precedent?**

- / We heard that respondents liked the limited road connections and trees illustrated in some of the precedent images.
- / Respondents commented on the densities of the images shown, and that they would be far too much density for the neighbourhood. Concerns over the amount of tree cover that will be lost due to development. Concerns over type of massing due to where the proposed precedent would be situated (higher grade).

### **Question Set 5 - What would you change about Land Use Option 1**

- / There were concerns over adding additional intersections along River Road and a road connection to Melran. There was a suggestion to terminate road connections before River Road. Additional suggestions included increasing setbacks along River Road, creating pedestrian and bike friendly connections, and taking into consideration the topography of the area.

## What we Heard



## Survey Response Summaries

### Question Set 6 - What would you change about Land Use Option 2

- / We heard that this option being too dense and that respondents don't want road connections to River Road. Additional comments also emphasized maintaining the rural atmosphere of River Road and maintaining the existing trees.

### Question Set 7 - What do you like or dislike about Land Use Option 3

- / Respondents expressed that they do not like increasing density in the area, to leave River Road as is (including no additional connections to River Road), and to make it a place where people can live surrounded by nature, without a car.

### Question Set 8 - What do you like or dislike about Land Use Option 4

- / We heard that respondents do not like increasing density in the area, to leave River Road as is (including no additional connections to River Road), and to maintain the rural atmosphere of River Road. There were additional comments about the density and built form on the topography would be overwhelming and reducing the number of buildings and increasing the setbacks along River Road. A respondent suggested opening up Olivewood to River Road to provide better flow and reduce the burden going to Townline Road and within the internal road network.

### Question 9 - Additional Comments

- / There were concerns expressed about how the options would preserve the feel of River Road. Additional comments were made about not connecting any streets to River Road, not road connection to Melran (via Alderson) and that new development will make driving, walking/biking more of a deterrent. Some also expressed that infrastructure improvements should be made before allowing additional development.
- / A comment suggested that a multiuse trail could be extended along the south side of River Road, creating a scenic corridor for future pedestrian movement.
- / Many residents feel that the volume of traffic and the speed at which it travels through the area makes pedestrian circulation hazardous and that decreasing the connections to River Road while adding traffic calming measures would deter high speed travel in the area.
- / Consider the scenic road designation for River Road.
- / Any development in this area should be scrutinized closely to ensure environmental quality is not jeopardized.

## Attachment No. 7 – Excerpt from the April 27, 2021 Public Meeting

### 4. Public Meeting Notice

### 5. Public Meeting

#### 5.1 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

Motion: 21-121

Moved by Councillor Reid

Seconded by Councillor Devine

THAT report 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment be received.

AND THAT the City initiated applications to amend the Official Plan and Zoning By-law be referred back to staff for a subsequent report and staff recommendation.

In Favour (7): Councillor Reid, Councillor Devine, Councillor Mann, Councillor Wolf, Councillor Adshade, Councillor Hamilton, and Councillor Ermeta

Absent (2): Mayor McGarry, and Councillor Liggett

**Carried (7 to 0)**

#### 5.1.1 Staff presentation

5.1.1.1 Bryan Cooper re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

#### 5.1.2 Consultant presentation

5.1.2.1 Ute Maya-Giambattista re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

#### 5.1.3 Delegations

5.1.3.1 Mandalay McCulloch re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

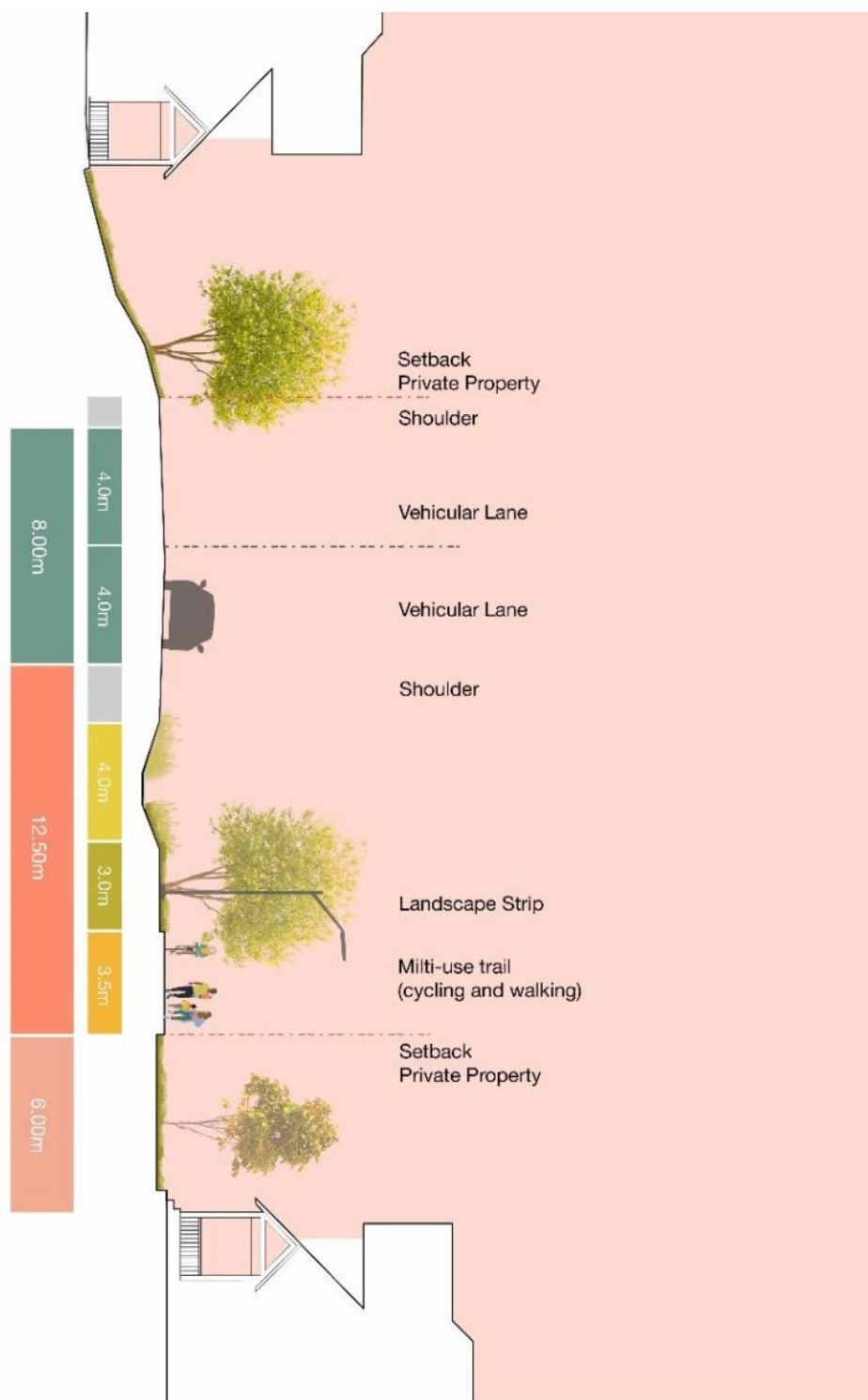
5.1.3.2 Frank Horvath re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

5.1.3.3 Marcy Merida re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

5.1.3.4 Anne Horsley re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

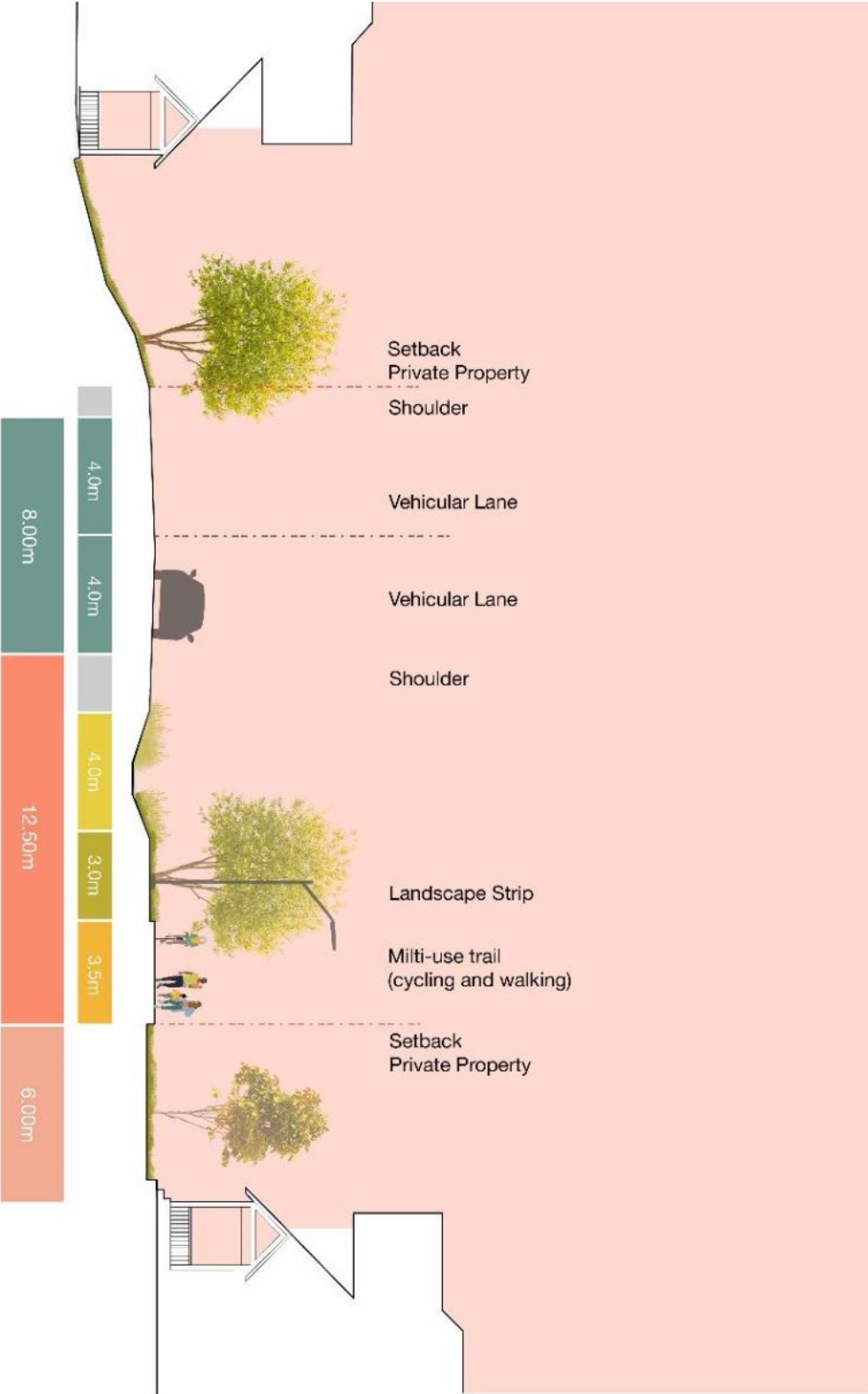
5.1.3.5 Tamara Heatherington re: 21-073(CD) City Initiated River Rd Secondary Plan and Zoning By-law Amendment

## Attachment No. 8 – River Road Cross Section – Interim





Attachment No. 9 – River Road Cross Section – Ultimate



## Attachment No. 10 – Planning Report prepared by Fotenn Planning + Design

CITY OF CAMBRIDGE

# RIVER ROAD SECONDARY PLAN

June 2021  
FINAL DRAFT

**FOTENN**

Prepared for the City of Cambridge

Prepared by Fotenn Planning + Design  
174 Spadina Avenue - Suite 304  
Toronto, ON M5T 2C2

June 2021

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CITY OF CAMBRIDGE

# **RIVER ROAD SECONDARY PLAN**

June 2021

FINAL DRAFT

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# 01

## Introduction



The River Road area is within the designated Urban Area in the City of Cambridge. The River Road Secondary Plan establishes a vision, principles and policies for the design and development of the River Road area. It also establishes the general land use patterns and conceptual location of community infrastructure such as parks and trails, roads, and services.

The lands subject to this Secondary Plan are identified on Schedules 1 to 4, which are attached hereto and form part of this Secondary Plan.

In addition to the identified Schedules and the following text, this Secondary Plan also includes the following appendices:

Appendix A - Supporting Documents:

- Phase 1 Background Report Memorandum (June 2020)
- Cultural Heritage Resource Assessment Study – Existing Conditions Report (January 2020, updated March 2020, May 2020 and March 2021)
- River Road Secondary Plan Traffic Study (May 2021)
- River Road Functional Neighbourhood Plan Servicing Assessment (March 2021, updated May 2021)

## 1.1 Background

The City of Cambridge is the second largest municipality in the Region of Waterloo. The City is forecasted to grow to 176,000 residents and 102,500 jobs by 2031. As a growing municipality, Council and staff are constantly seeking to find new and improved ways to deliver municipal services that meet residents' expectations while allowing for growth.

Through the consultation process on site specific development applications, the City has heard concerns that new infill development needs to be considerate of the surrounding rural and lower density character and that the infrastructure (sanitary services, transportation etc.) needs to be properly planned to accommodate new growth.

In response, City Council enacted an interim control by-law in accordance with the Planning Act in June of 2019 to restrict development in the area for a period of one year to allow for studies to be completed to address long term planning and servicing for the area. In the Fall of 2019, the River Road Secondary Plan, Zoning and Servicing Study was initiated to evaluate potential servicing alternatives and to develop an updated policy and regulatory framework to guide new development through a Secondary Plan and Zoning By-law Amendment for the area. The Study also included a review of area stormwater management, traffic/transportation and cultural heritage

Council extended the interim control by-law to September 23, 2021 to permit the proper completion of the study.

## 1.2 Purpose of Plan

The purpose of the River Road Secondary Plan is to provide a land use strategy to guide the detailed planning and development of the River Road Community in a manner that integrates infill development with the existing neighbourhood and adjacent rural character of River Road, while respecting and protecting surrounding natural features.

The River Road Community is planned to contribute to the achievement of a complete community and provide opportunities for a full range of housing, transportation options including active transportation, provision of a new park and protection of existing natural open space. The Secondary Plan is to provide an appropriate framework to organize the community neighbourhood structure, transportation, park, natural open space and cultural heritage elements.

Development within the River Road Secondary Plan will conform to and implement the local and regional Official Plans. In addition to the policies of this Secondary Plan, all other parts of the local and regional official plans and provincial policies shall apply. In the event of a policy conflict with the City of Cambridge Official Plan, the River Road Secondary Plan will prevail.

## 1.3 Secondary Plan Process

The City of Cambridge may implement a Secondary Plan for a particular area to ensure that development occurs at densities and forms consistent with the Official Plan. Chapter 10.2 of the City of Cambridge Official Plan details the requirements to implement a Secondary Plan, as follows:

Secondary Plans may be prepared for specific areas of the city to provide more detailed planning objectives and policies to direct and guide development.

Secondary Plans and Community Plans approved by Council shall be in conformity with the Official Plan and the Regional Official Plan and shall clarify and illustrate how the policies of the Official Plan are to be implemented.

Secondary Plans shall be incorporated into the Official Plan through the adoption of an amendment to the Official Plan. Prior to approval or amendment of a Secondary Plan; notification, hearing and appeal procedures required for any amendment to the Official Plan, as set out in the Planning Act, shall apply.

The City will consider the following criteria in the preparation of a Secondary Plan

- a. patterns of land use;
- b. population and employment projections;
- c. mix and range of housing types and densities;
- d. phasing of development in an efficient manner;
- e. provision for adequate and appropriate areas for commercial, institutional and community-oriented facilities;
- f. provision for trails, parks and open space;
- g. natural features;
- h. cultural heritage resources;
- i. incorporating intensification opportunities;
- j. development of a transportation network that facilitates the efficient movement of vehicular and pedestrian traffic including active transportation considerations;
- k. municipal services;
- l. incorporating opportunities for mixed-use and higher density development in appropriate locations;
- m. designation of land; and
- n. any other matters as deemed appropriate by the City.

## 1.4 Organization

### a. **Section 1 - Introduction**

This section provides an introduction and overview of the Secondary Plan. It outlines the purpose of the Plan, the secondary plan process, the organization of the Plan, and how the plan is implemented within the existing policy context.

### b. **Section 2 - Basis of the Plan**

This section provides an overview of the existing conditions, the background planning and policy context, transportation and servicing related policies (where applicable), and key opportunities and constraints.

### c. **Section 3 – Vision**

This section describes the vision and objectives developed for the study area. The vision and objectives are what guide the policies of the plan and ultimately shape future development within the area. The vision is also supported by a demonstration plan that illustrates one of many ways that the area could develop based on the policies and guidelines of the Plan.

### d. **Section 4 – Neighbourhood Policies**

The Neighbourhood Policies focus on the overall neighbourhood structure, character, and layout of the community. These policies address the proposed land uses, streetscapes, parks and open spaces, cultural heritage and transportation and servicing considerations.

### e. **Section 5 – Urban Design Policies**

The Urban Design Policies provide more specific guidelines and policies to realize the Neighbourhood Policies. The Urban Design Policies address block design, intersection considerations, active transportation, public realm design and private realm design.

### f. **Section 6 – Implementation**

The Secondary Plan shall be implemented through a variety of tools, including planning and development application process tools, supporting studies, and phasing and financial tools.

## 1.5 Integration with the City of Cambridge Official Plan

Policy 2.5.5 of the City’s Official Plan states that the City will prepare Secondary Plans to “ensure that development occurs at the densities and form consistent with the Official Plan”. Secondary Plans are to allocate appropriate density ranges for various areas based on the local characteristics, access to transportation and availability of amenities (e.g., schools and parks).

## 1.6 Authority

This Secondary Plan has been prepared under the Planning Act and generally aligns with the policies of the City of Cambridge Official Plan, the Region of Waterloo Official Plan, the 2020 Provincial Policy Statement and the Growth Plan for the Greater Horseshoe (2020).

# 02

## Basis

## of the Plan



## 2.1 Existing Conditions

### 2.1.1 General Location

The River Road Neighbourhood is located in the Hespeler community and is generally bounded by: Townline Road to the east; River Road and the Speed River to the west and north; and Melran Drive to the south.

The River Road Secondary Plan area includes a series of lands along River Road between the Brewster Trail (Flynn Avenue) and River Road, to Townline Road and River Road. The total area of the Secondary Plan area is approximately 19.0 hectares (47 acres) and is shown in Figure 1 - Study Area Map. Although the area is currently described as rural / suburban, it is designated for urban development according to the Low/Medium Density Residential land use designation in the City's Official Plan. This designation permits single detached, townhouses and walk-up apartment buildings to a maximum density of 40 units per hectare which is a higher density form of development than what currently exists in this part of the City

### Figure 1: Study Area Map

— Study Area





Figure 1: River Road Study Area Map



## 2.1.2 Existing Planning Context

The River Road Secondary Plan area is located in the Built-Up Area in the City's Official Plan and is designated Low/Medium Density Residential and Natural Open Space System. The Low/Medium Density Residential designation permits development of single detached dwellings, townhouses, and walk-up apartments to a maximum density of 40 units per hectare, where municipal water supply and wastewater systems are available. Compatible community facilities such as schools, parks, places of worship and convenience commercial establishments are also permitted.

The City's Official Plan directs that secondary plans may be prepared for specific areas of the City to provide more detailed planning objectives and policies to direct and guide development

### 2.1.2.1 Official Plan Amendment Description and Rationale

The City of Cambridge Official Plan is hereby amended by adding the attached "River Road Secondary Plan".

#### Secondary Plan

The purpose of the River Road Secondary Plan is to guide the future planning and development of the River Road Secondary Plan lands. The Secondary Plan supports and builds on the policies of the City of Cambridge Official Plan, and provincial and regional policies and plans, with respect to orderly development of the area. The Secondary Plan establishes a vision and the principles for the design and development of the area. It also establishes the general land use patterns and conceptual locations of parks and trails, roads and infrastructure. The Secondary Plan includes goals, general policies and land use policies that include four schedules.

### 2.1.2.2 Compliance with Provincial Legislation and Policy

#### Provincial Policy Statement (2020)

Section 3 of the Planning Act requires that decisions affecting planning matters shall be consistent with policy statements issued under the Act. The Provincial Policy Statement (PPS) was issued under the authority of Section 3 of the Act. The PPS provides policy direction on matters of provincial interest related to land use planning and development, including the protection of resources of provincial interest, public health and safety, and the quality of the natural and built environment.

Section 1.1.1 of the PPS states, in part, that "healthy, liveable and safe communities are sustained by:

- a. promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b. accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- e. promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs".

Section 1.1.3.2 states, in part, that “land use patterns within settlement areas shall be based on:

- densities and a mix of land uses which: a) efficiently use land and resources; b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; and,
- land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated”.

Section 1.1.3.3 states that “planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs”.

Section 1.1.3.4 states that “appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety”.

Section 1.4.3 states, in part, that “planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

- b. permitting and facilitating: 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and 2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;
- c. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;
- d. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and
- f. establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety”.

Section 1.5.1 of the PPS states that healthy, active communities should be promoted by: planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity; and planning and providing for a full range and equitable distribution of publicly-accessible built and natural settings for recreation, including facilities, parkland, public spaces, open space areas, trails and linkages, and, where practical, water-based resources.

The River Road Secondary Plan has been developed to ensure the efficient use of land and infrastructure, to support active transportation and the provision of parkland, and the provision of an appropriate mix of housing forms, including affordable housing options, in the community.

Infrastructure, which includes transportation corridors and sewage and water systems, shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected needs (Section 1.6.1). In addition, planning for infrastructure shall be coordinated and integrated so that they are available to meet current and projected needs. Policy 1.6.6.2 states that the extension of existing municipal water and wastewater systems are the preferred form of servicing in settlement areas. Section 1.6.4 states that “infrastructure and public service facilities should be strategically located to support the effective and efficient delivery of emergency management services”.

Section 1.6.7.1 states that “transportation systems should be provided which are safe, energy efficient, facilitate the

movement of people and goods, and are appropriate to address projected needs” and Section 1.6.7.2 states that efficient use should be made of existing and planned infrastructure, where feasible.

The River Road Secondary Plan has been developed based on a vision that includes streets designed to enable safe access for all users. The objectives of the transportation network include promoting the efficient movement of traffic and incorporating improved active transportation facilities within the neighbourhood. Planning for the River Road Secondary Plan area is based on using existing infrastructure efficiently.

Section 2.1, Natural Heritage, of the PPS states that natural features and areas shall be protected for the long term and development and site alteration shall be not be permitted in significant wetlands (2.1.4), and significant woodlands “unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions” (2.1.5). Also, development and site alteration is not permitted on lands adjacent to significant wetlands and significant woodlands “unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions” (2.1.8). In regard to ‘other natural heritage features’, negative impacts is defined in the PPS as “degradation that threatens the health and integrity of the natural features or ecological functions for which an area is identified due to single, multiple or successive development or site alteration activities”.

Policy 2.6.1 of the PPS states that significant built heritage resources and significant cultural heritage landscapes shall be conserved and Section 2.6.3 states that “planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved”.

The Secondary Plan is appropriate and consistent with the PPS.

### **Places to Grow – Growth Plan for the Greater Golden Horseshoe (2020)**

In 2019, the Province of Ontario released an update to the provincial growth plan called “A Place to Grow – Growth Plan for the Greater Golden Horseshoe” (Growth Plan). An Office Consolidation of the Growth Plan, which includes Amendment 1 (2020) was released in August of 2020. Planning applications are required to conform to Provincial plans

The Growth Plan focuses on directing new development to existing settlement areas in order to support the development of complete communities that contain a diverse mix of uses and housing options in order to optimize existing infrastructure and public transit. The Growth Plan contains development targets (45% infill) for urban areas (referred to as Built-Up areas). The River Road Secondary Plan area is located within a settlement area and is considered a Built-Up area in the Growth Plan.

The Secondary Plan conforms to the Growth Plan for the Greater Golden Horseshoe.

### **Region of Waterloo Official Plan**

The Secondary Plan area is located in the Built-up Area within the Urban Area Boundary as identified in the Region of Waterloo Official Plan. It is intended that land within the Urban Area is “to accommodate the majority of the Region’s growth within the planning horizon” of the Plan (Section 2.B.2).

The Built-Up Area identifies all lands within the built boundary of the Urban Area. Area municipalities are required to establish policies in their official plans and other supporting documents to ensure that “a minimum of 45 per cent of all new residential development occurring annually within the region as a whole will be constructed within the Built-Up Area” (Section 2.C.2).

Section 2.D.1 requires planning for new development within the Urban Area that:

- b. is serviced by a municipal drinking water supply system and a municipal wastewater system;
- c. contributes to the creation of complete communities with development patterns, densities and an appropriate mix of land uses that supports walking, cycling and the use of transit;
- d. protects the natural environment, and surface water and groundwater resources;
- e. conserves cultural heritage resources and supports the adaptive reuse of historic buildings;
- f. respects the scale, physical character and context of established neighbourhoods in areas where re-urbanization is planned to occur; and,



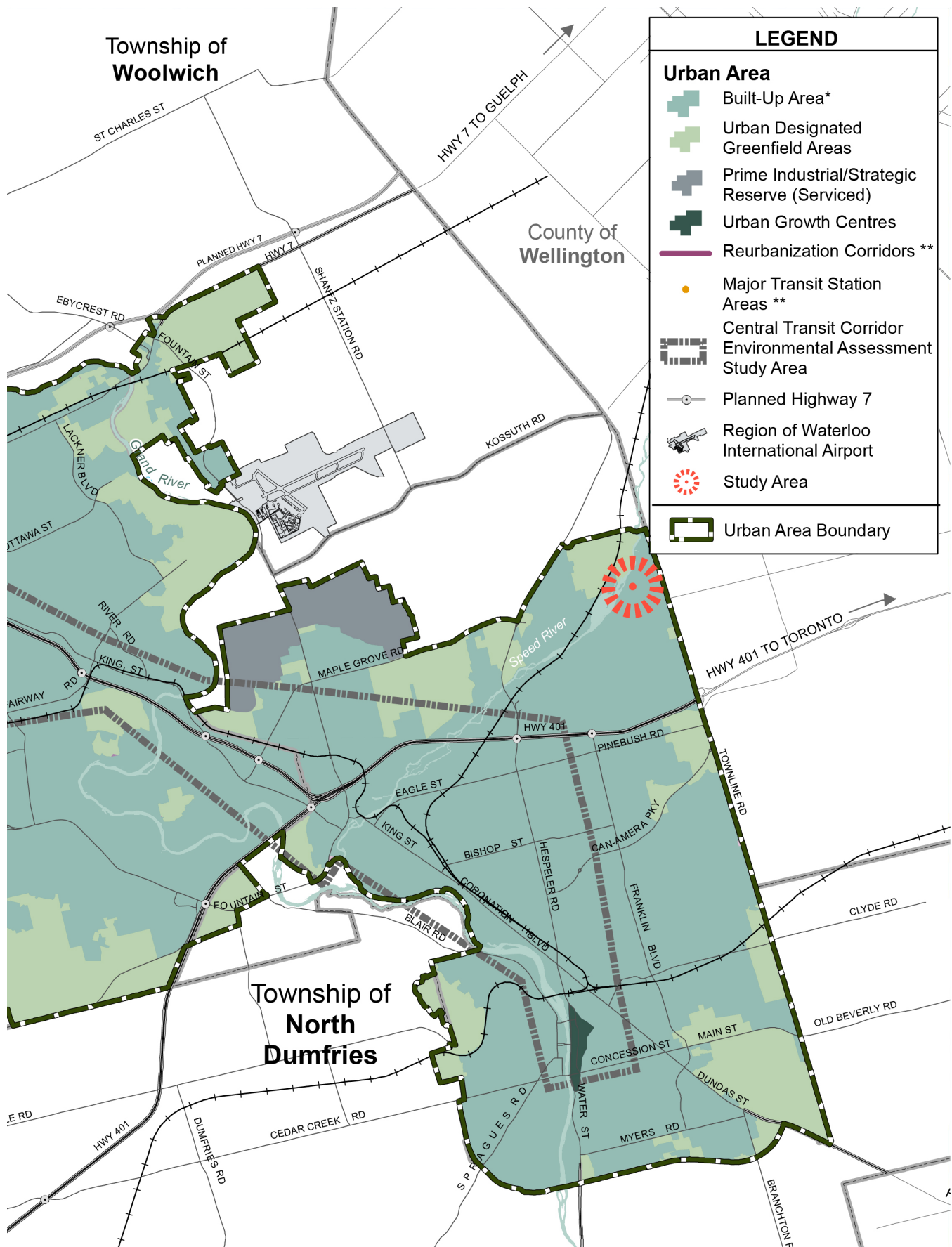


Figure 2: Region of Waterloo Official Plan - Map 3a Urban Area

- h. promotes building designs and orientations that incorporate energy conservation features and the use of alternative and/or renewable energy systems.

The River Road Secondary Plan was prepared based on the principles supporting the creation of complete communities, and on the availability and planning of water and wastewater infrastructure. The policy framework was developed to ensure that the character of existing neighbourhoods is respected, and that natural and cultural heritage features are appropriately protected.

The proposed Official Plan Amendment conforms to the Regional Official Plan.

#### **City of Cambridge Official Plan (2018)**

The River Road Secondary Plan area is located in the Built-Up Area according to the City's Official Plan, and is designated Low/Medium Density Residential and Natural Open Space System.

Uses such as single detached dwellings, townhouses and/or walk up apartments are permitted on lands designated Low/Medium Density Residential where municipal water supply and wastewater systems are available (8.4.6.9).

The City's Official Plan directs that secondary plans may be prepared for specific areas of the City to provide more detailed planning objectives and policies to direct and guide development (10.2.1) provided the secondary plan is in conformity with the City and Regional Official Plans (10.2.2), and in consideration of the following Section 10.2.4 criteria:

- a. patterns of land use;
- b. population and employment projections;
- c. mix and range of housing types and densities;
- d. phasing of development in an efficient manner;
- e. provision for adequate and appropriate areas for commercial, institutional and community-oriented facilities;
- f. provision for trails, parks and open space;
- g. natural features;
- h. cultural heritage resources;
- i. incorporating intensification opportunities;
- j. development of a transportation network that facilitates the efficient movement of vehicular and pedestrian traffic including active transportation considerations;
- k. municipal services;
- l. incorporating opportunities for mixed-use and higher density development in appropriate locations;
- m. designation of land; and
- n. any other matters as deemed appropriate by the City.

Section 5.2.1 states that the design of the built environment will promote sustainable, healthy, active living through:

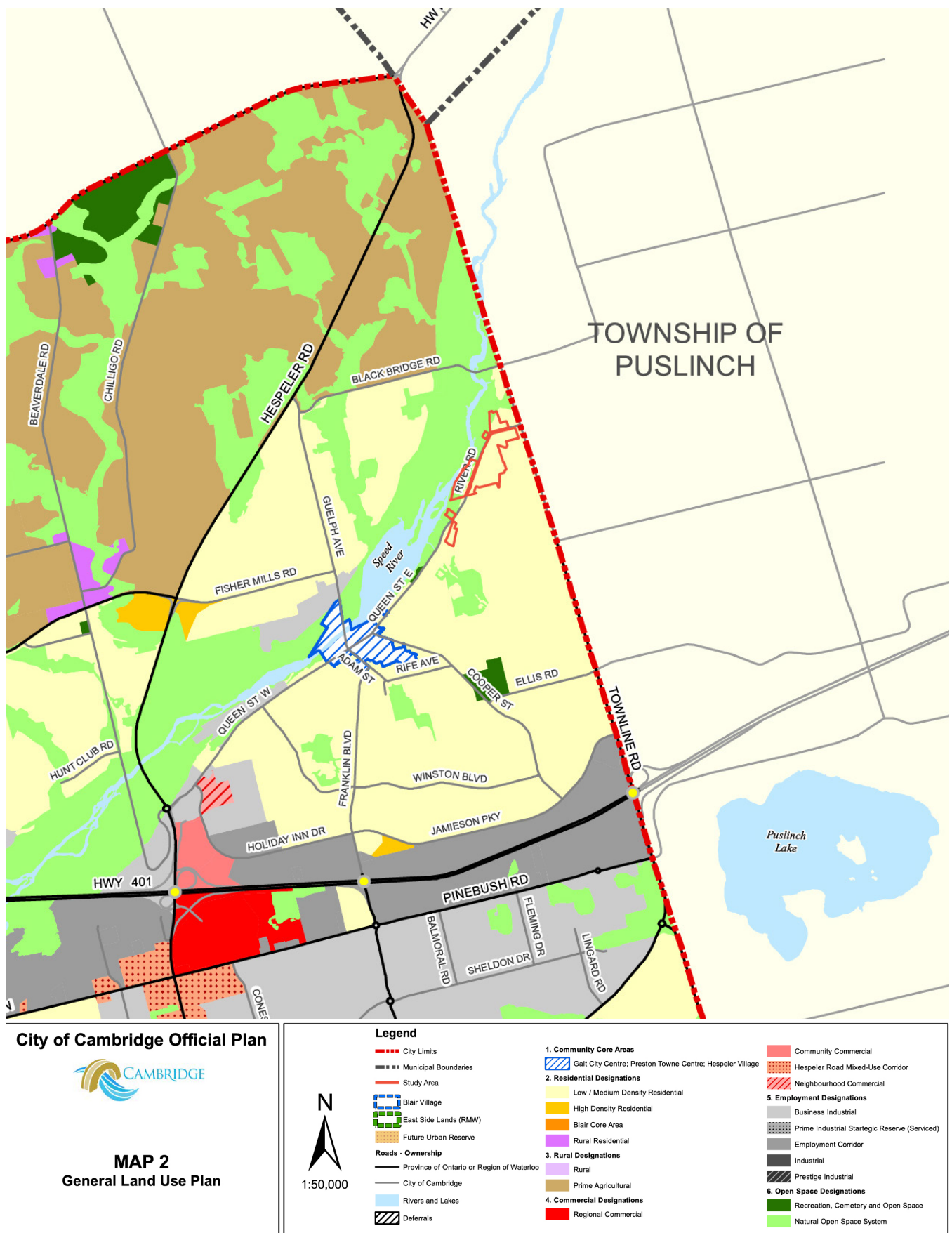


Figure 3: City of Cambridge Official Plan - Map 2 General Land Use



- a. well-connected and maintained streets, paths and trails that are able to safely accommodate different modes of transportation;
- b. safe, accessible, aesthetically pleasing, well-serviced and inclusive developments;
- c. resilient natural environments that support wildlife and their habitat and are better connected to residential areas; and
- d. walkable neighbourhoods that offer a mix of uses and range and variety of housing types with convenient access to public transit.

Section 5.4 of the Official Plan speaks to preserving and enhancing views and vistas, and protecting views to natural or cultural heritage elements within the vicinity of the Speed River. The Official Plan also states that development will be “compatible in terms of massing and scale with the existing and planned streetscape; and provide appropriate transitions in height to adjacent buildings” (5.7). Energy efficiency and sustainability is encouraged in neighbourhood and building design (5.8).

Section 8.4.6.3 (a) sets a maximum residential target of 40 units per gross hectare for the Low/Medium Density Residential designation, excluding only provincially constrained environmental areas as defined in the Regional Official Plan and accessory units. However, Section 8.4.6.7 states that a lower density may be permitted, without amendment to this Plan, where it is warranted by at least two listed criteria including: the nature of the soils, topography, geomorphology, vegetation or other environmental features of the lands proposed for development; the established character of the neighbourhood in which the lands

proposed for development are located; the projected negative impact of the proposed development on adjoining environmental features or cultural heritage resources.

The River Road Secondary Plan was developed, in part, based on the criteria in Section 10.2.4 of the Official Plan. The land use designations and policies in the River Road Secondary Plan are based on an assessment of the character of the area and intensification opportunities and are intended to ensure that new development provides a mix and range of housing types and densities, community trails and parkland, and appropriate infrastructure including roads and water and wastewater services. Planned densities are within the maximum residential target of 40 units per gross hectare.

The Secondary Plan conforms to the City of Cambridge Official Plan.

### 2.1.3 Site Context and Character

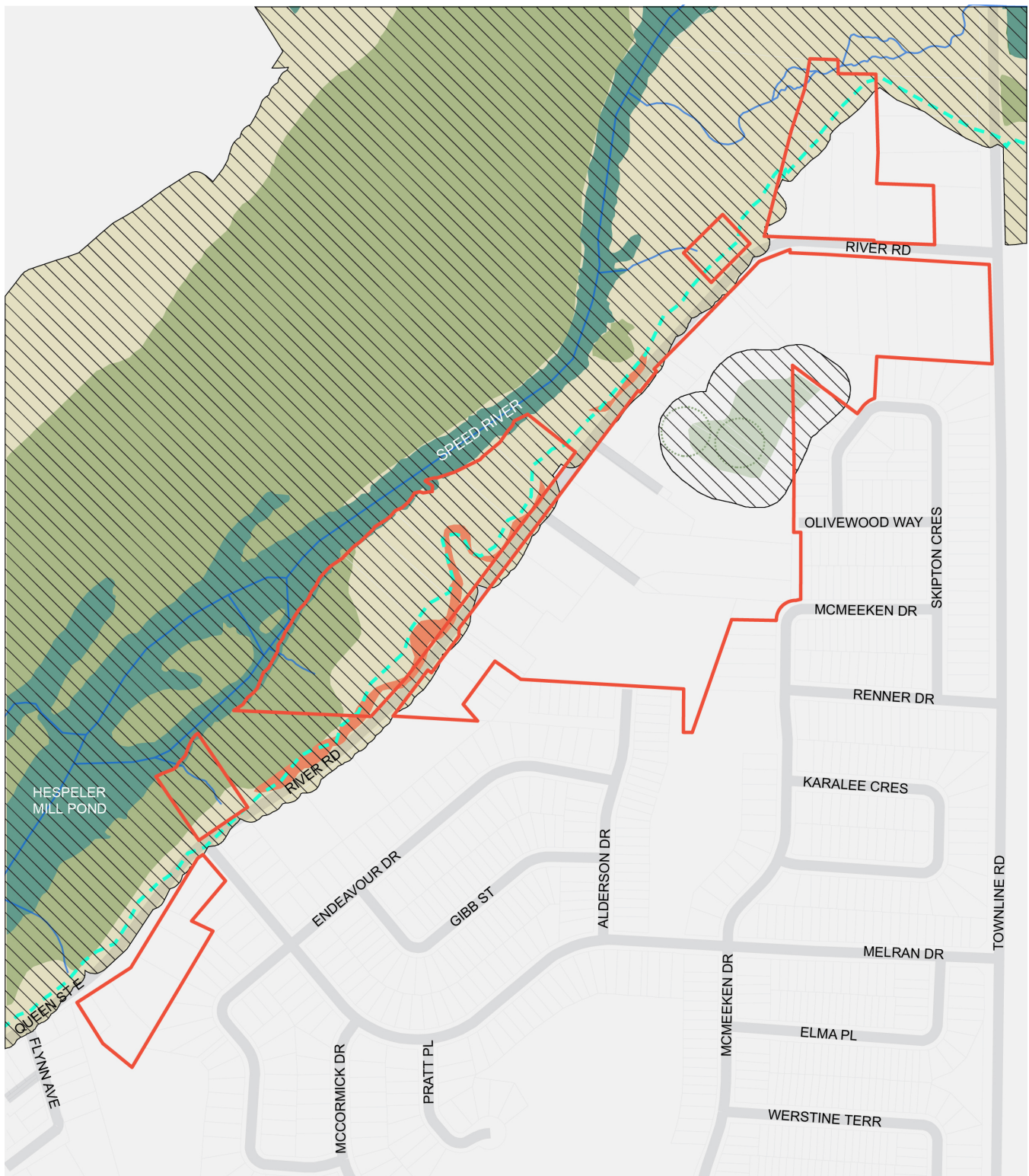
River Road is a rural neighbourhood within the Hespeler community. The study area extends along River Road from 284 Queen Street East and Townline Road. The lands are within the watershed of the Speed River. The main road that serves this area is River Road, which has a rural cross section without sidewalks or municipal services. A series of local roads also intersect with River Road and the study area. The predominant built form character along River Road consists of residential properties. Although River Road is currently described as rural, it is designated for urban development with the 2012 City of Cambridge Official Plan.

### 2.1.4 Natural/Environmental Areas (Existing Condition)

There are several natural and environmental features, as identified in Figure 4: Existing Natural Environmental Areas, found within the study area, including:

- a. Locally Significant Natural Areas  
- Locally Significant Natural Areas (LSNAs) are those natural features not meeting the criteria for recognition as being provincially or regionally significant. LSNAs play an important role in maintaining the ecological functions provided by the Natural Heritage System.
- b. Butternut Tree Buffer (25.0m) - Tree protection buffers around butternut trees (retain a proportion of suitable regeneration habitat within a 25 metre radius)
- c. Watercourse (GRCA) - The watercourse layer represents surface water hydrology features such as streams, culverts, ditches and more. The Speed River watercourse has been identified along the western portion of the study area.
- d. Regulation Limit (GRCA) - The regulation limit is regulated under Ontario Regulation 150/06: Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation and affects what and where a Conservation Authority can regulate.
- e. Regulatory Floodplain (GRCA)- The floodplain depicts the regulatory flood which is the greater of the 100 year flood or regional storm. Much of the lands west of River Road and within the floodplain.
- f. Wetland (GRCA) - Wetlands are subject to a series of permit regulations and policies under Ontario Regulation 150/06. Lands adjacent to the watercourse as well as a wetland east of River Road have been identified.
- g. River Slopes and Erosion (GRCA)  
- Areas with steep slopes which fall under Provincial and GRCA policies dealing with Slope Erosion Hazard, Steep Slopes, and Valleys. Development activities on or adjacent to steep slopes can be hazardous due to potential for slope failures and erosion. Steep slopes have been identified along the portions of River Road (west side of River Road).
- h. Significant Valley Lands (Regional Official Plan) - Includes the valley lands of the Speed River, and comprises of the entire river channel within the region and run up to the point where the slope of the valley begins to grade into the surrounding upland.

Any future development will be refined through an Environmental Impact Statement that may be required as part of the development application.



**Figure 4:  
Existing Natural  
Environmental Areas**

- |  |  |
|--|--|
| <span style="display: inline-block; width: 20px; height: 10px; background-color: #90EE90; border: 1px solid black;"></span> Regulated Wetland/Environmental Feature(s) | <span style="display: inline-block; width: 20px; border-bottom: 2px solid blue;"></span> Watercourse (GRCA)  |
| <span style="display: inline-block; width: 20px; height: 10px; background-color: #4682B4; border: 1px solid black;"></span> Regulated Watercourse (GRCA)               | <span style="display: inline-block; width: 20px; border-bottom: 2px dashed red;"></span> Regulation Limit (GRCA)   |
| <span style="display: inline-block; width: 20px; height: 10px; background-color: #FFFFE0; border: 1px solid black;"></span> Significant Valley Lands (ROP)             | <span style="display: inline-block; width: 20px; height: 10px; background-color: #FFA500; border: 1px solid black;"></span> Slope Erosion Hazards (GRCA) |
| <span style="display: inline-block; width: 20px; border-bottom: 2px solid red;"></span> River Road Secondary Plan Boundary   | <span style="display: inline-block; width: 20px; border-bottom: 2px dotted red;"></span> Floodplain (GRCA)   |

## 2.2 Opportunities and Challenges

Through the consultation process on site specific development applications, the City has heard concerns that new infill development needs to be considerate of the surrounding rural and lower density character and that the infrastructure (sanitary services, transportation etc.) needs to be properly planned to accommodate new growth.

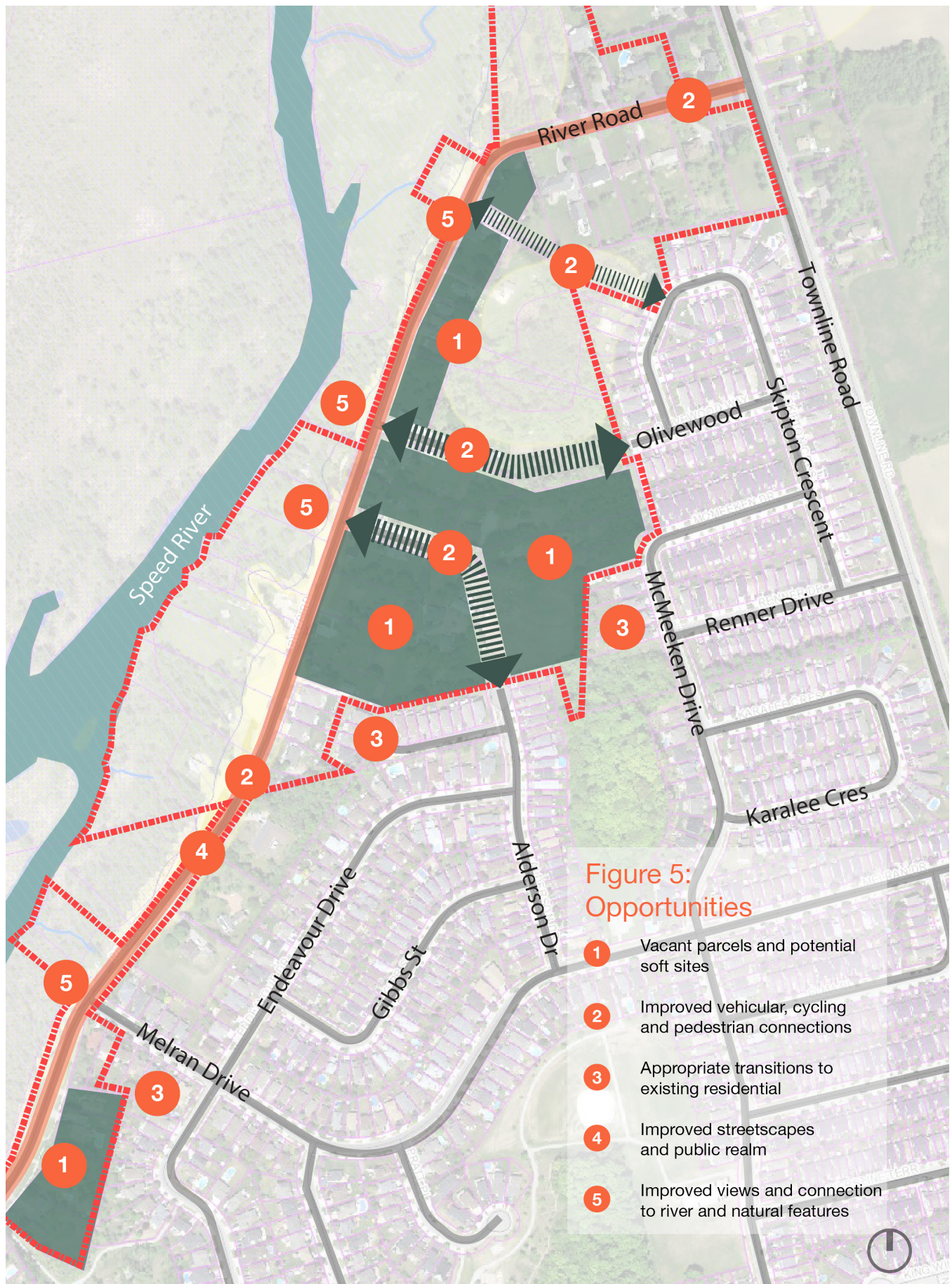
In response, City Council enacted an interim control by-law in accordance with the Planning Act in June of 2019 to restrict development in the area for a period of one year to allow for studies to be completed to address long term planning and servicing for the area. In the Fall of 2019, the River Road Secondary Plan, Zoning and Servicing Study was initiated to evaluate potential servicing alternatives and to develop an updated policy and regulatory framework to guide new development through a Secondary Plan and Zoning By-law Amendment for the area. The Study also included a review of area stormwater management, traffic/transportation and cultural heritage.

The study also involved the identification and consideration of opportunities and constraints, including:

### Opportunities

- a. Vacant parcels, larger parcels, underutilized sites, and lot consolidation provide an opportunity for new development and neighbourhood amenities or improvements.
- b. Road upgrades, new streets, enhanced streetscapes, and traffic calming measures can improve vehicular, cycling and pedestrian connectivity and circulation. To guide future development and built form to ensure appropriate transitioning between existing and future development, as well as River Road.
- c. Future development and road upgrades can improve streetscapes, the public realm, and the interface along River Road.
- d. Road upgrades may bring municipal servicing to existing and future development along River Road.
- e. Enhance and improve neighbourhood views and connections to the River and other natural features.
- f. Reinforce and enhance the natural and built form character along River Road.
- g. Incorporating existing heritage homes into a new streets and block pattern for the community.
- h. The existing character of River Road may merit being designated as a Scenic Route.

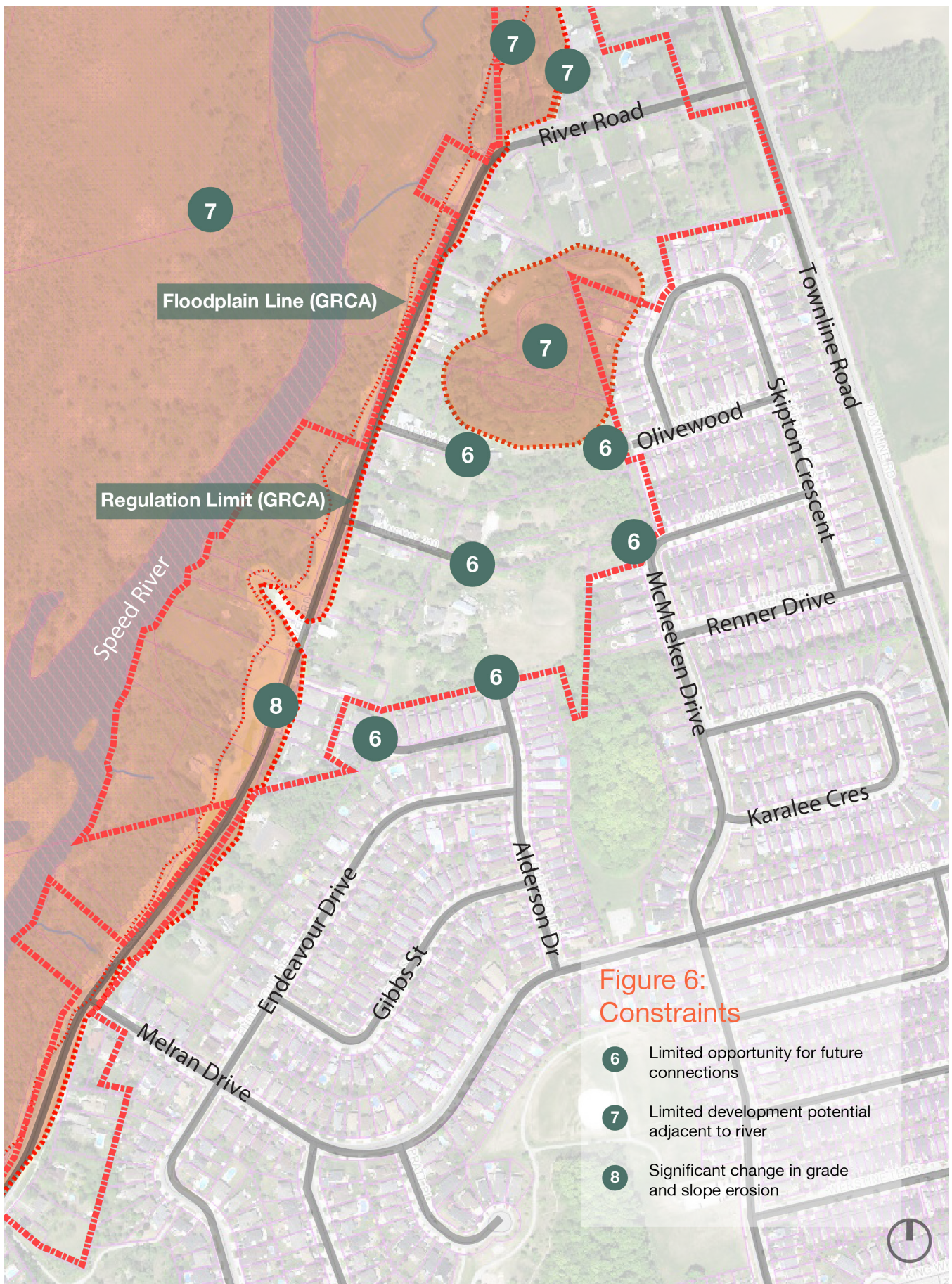




### Constraints

- a. Limited opportunity for future road connections between existing and future residential development.
- b. Development potential on parcels adjacent to the speed River are limited due to GRCA Regulation Limit, Regulatory Floodplain, and Wetlands.
- c. Limited development potential on parcels adjacent to Speed River due to change in grade and slope erosion hazards.
- d. Road improvements and traffic calming that do not widen the paved area, take way from the rural character of the road, and increase traffic along River Road.





# 03

## Vision





### 3.1 Vision for Change

The River Road Secondary Plan Vision is based on the growth and development tenets found in the City of Cambridge Official Plan to:

*“Develop a community that supports and emphasizes River Road’s unique rural character and built heritage features, in a way that allows residents and visitors to better enjoy the natural environment and scenic views associated with the Speed River.*

*The community is envisioned as a sustainable and accessible neighbourhood with a range of housing, a pedestrian friendly environment and multiple recreational opportunities.”*

Furthermore, the Vision for the River Road Secondary Plan lands is intended to encourage and manage development in a manner that delivers a vibrant and diverse community. Key components of the Vision reflected in the policies herein and related schedules include:

- a. The enhancement of River Road as a scenic “promenade” route along the Speed River through the implementation of a distinctive streetscape and built form character strategy fronting onto the street;
- b. The preservation and enhancement of the existing River Road streetscape through the implementation of a special residential character zone that seeks to maintain the area’s character through the implementation of a River Road typical streetscape and built form character elements such as building setbacks, height and massing;
- c. The development of a neighbourhood scaled open space network that is sensitive to the site’s existing natural features and provides pedestrian links to adjacent public open space amenities;
- d. The development of a community with a diverse range of housing types; and,
- e. The implementation of a connected street system.

## 3.2 Secondary Plan Objectives

The goals of the River Road Secondary Plan are to:

- a. establish a community structure and land use strategy for lands within the secondary plan area;
- b. identify a vision and design principles for the community;
- c. ensure orderly development of the community by providing direction and guidance to the review and approval of development applications and by providing a phasing strategy that provides for the logical development of the community;
- d. identify, preserve and enhance, where possible, significant natural features and areas;
- e. deliver an enhanced River Road street cross section that preserves the views of the Speed River and associated natural features and areas as well as the road's character;
- f. efficiently use infrastructure and community facilities;
- g. ensure an appropriate mix of housing types and sizes, and promote opportunities for affordable housing;
- h. ensure the conservation of identified built heritage resources (refer to Appendix B Cultural Heritage Resource Assessment Study – Existing Conditions Report (January 2020, updated March 2020, May 2020, March 2021));
- i. identify a road network and identify opportunities for transit and active transportation;
- j. identify the optimal River Road cross section to safely and comfortably accommodate for pedestrian and cycling movement while introducing traffic calming strategies;
- k. ensure compatible built form and transition in densities relative to surrounding existing development; and,
- l. enhance the existing character of the River Road corridor.

### 3.3 Demonstration Plan

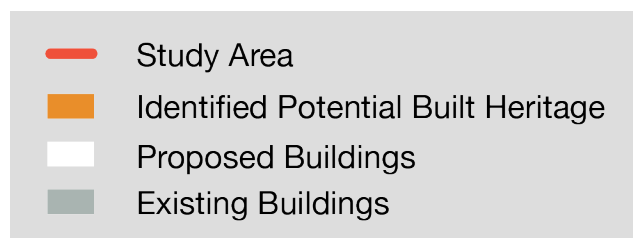
Figure 7: Demonstration Plan, illustrates how the vision could be achieved. Development in the River Road Secondary Plan area is defined by two distinctive land use character areas:

- a. River Road Character Area – Located along River Road, the River Road Character Area is envisioned to be residential in nature defined by a built form and massing representative of the road’s historic development pattern. This is to primarily include generous front setbacks, a combination of landscaped and naturalized front yards and a minimal presence and street edge interruption by garages and driveways.
- b. Neighbourhood Character – Located internal to the largest potential development parcel, lands are envisioned to be residential in nature with a compact intense form to include the full range of single to stacked townhouse residential typologies. Siting and massing best practices are further defined in the accompanying Zoning By-law to ensure active frontages dominate the neighbourhood streetscapes and the location of all car related impact on the future streetscapes are minimized.

Further to the character areas, a complete interconnected open system is envisioned where existing and future potential parks, pedestrian walkways and pedestrian friendly streets are part of a comprehensive open space system. Particular attention is paid to the future enhancement of River Road as a safe and comfortable pedestrian and cyclist route.

Figure 7:

## Demonstration Plan







04

# Neighbourhood Policies



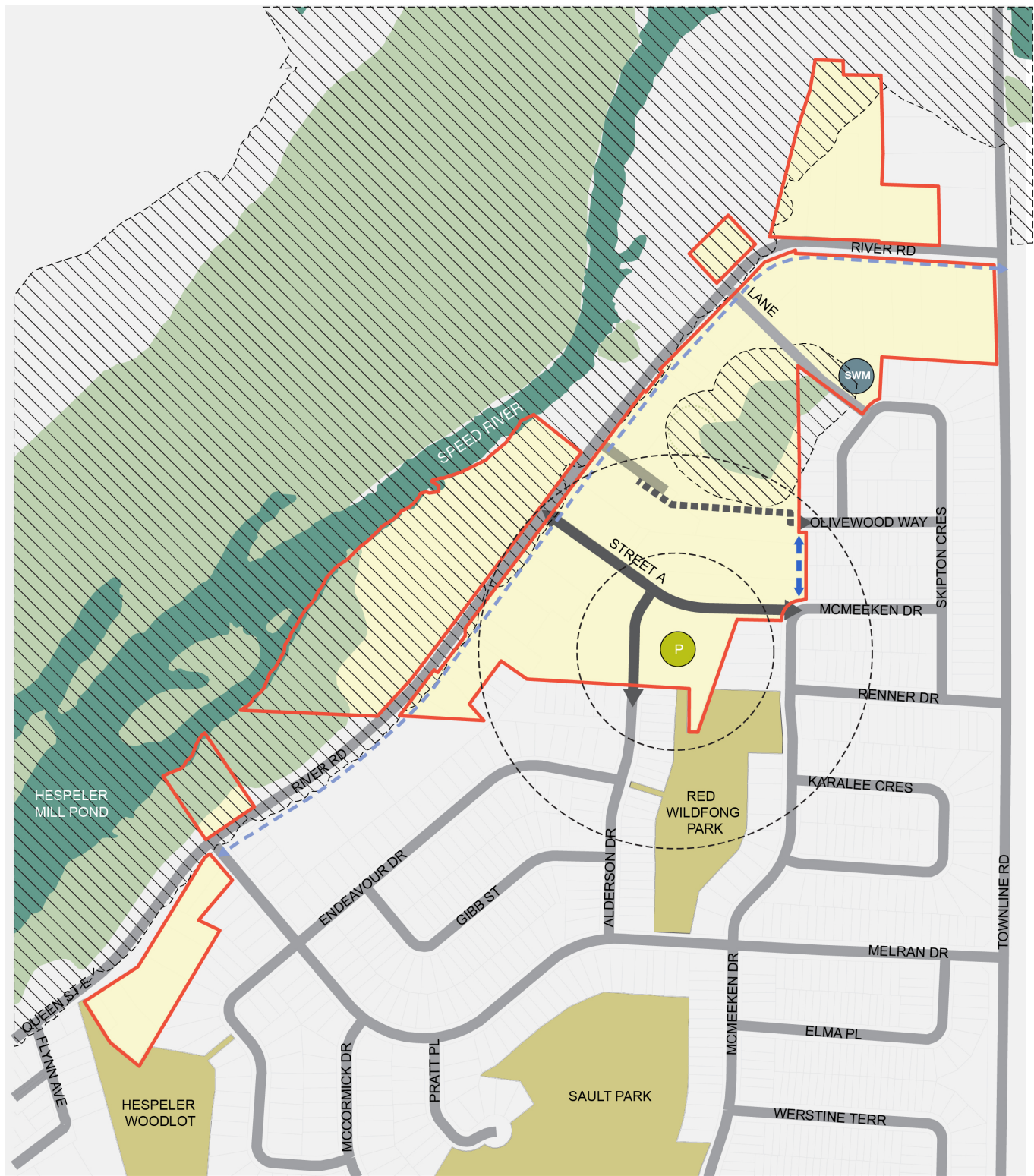
## 4.1 Relationship with the Official Plan and Use Categories and Permissions

The land use designations for the Secondary Plan are intended to complement the broader land use designations provided in the Official Plan. In most cases, the land use policies and permissions described in the Secondary Plan are more detailed than those provided for within the Official Plan. Where there are inconsistencies between a particular policy in the Official Plan and the Secondary Plan, the policies of the Secondary Plan will prevail.

## 4.2 Neighbourhood Structure

The Secondary Plan, as illustrated in Figure 8: Neighbourhood Structure, is framed by River Road as the key character defining element with a central internal neighbourhood. The neighbourhood is inter-connected by an active transportation system, a local road network and an integrated open space network.





**Figure 8:**  
**Neighbourhood**  
**Structure**



- Residential Neighbourhoods
- Existing Natural Open Space System
- Existing Recreational, Parks/Open Space
- Proposed Open Space/Park
- Stormwater Management
- Proposed Multi-Use Trail Network

- Existing Roads
- Proposed Local Municipal Road
- Proposed Private Road
- Pedestrian Connection
- Walking Radius (200-400m)
- River Road Secondary Plan Boundary
- Special Policy Area

The objectives of the Neighbourhood Structure are to:

- a. Plan for a complete community that is walkable and can accommodate various modes of transportation by means of compact form and densities that contribute to achieving the density targets of the approved Regional and City of Cambridge Official Plans.
- b. Provide an open space network of integrated and connected public spaces, including a new park (minimum 0.6 ha up to 1.5 ha in size), and links to the Red Wildfong Park, an enhanced active transportation network along River Road, existing mid-block connectors, and existing stormwater management facilities and natural features.
- c. Locate higher density development along internal roads and on larger landholdings in the Secondary Plan area.
- d. Ensure compatibility of scale and form along River Road to further compliment the road's character and scenic road qualities.
- e. Promote active streetscapes.
- f. Design the built form in a manner which is sensitive to the adjacent natural heritage system and mitigates impacts on natural features, functions and linkages through buffers, development setbacks, and stormwater management and other infrastructure best practices.
- g. Promote sustainable design throughout the built environment to promote efficient use of energy, land, and infrastructure.
- h. Design spaces that provide safe living and working environments through the consideration of Crime Prevention Through Environmental Design (CPTED) principles.
- i. Coordinate the location of parks with the active transportation network including community trails, walkways and bike lanes.
- j. Coordinate traffic calming and pedestrian protection measures with the open space network and other public spaces including the trail system along River Road.

The River Road Neighbourhood Structure is shown on Figure 8: Neighbourhood Structure and the main elements are:

- k. **Residential Neighbourhoods -**  
Comprised of lands designated Village Residential and Neighbourhood Residential as identified on Figure 9: Land Use Structure and described below:
  - i. **Village Residential** – Fronting onto River Road, these lands are to frame and enhance the road’s character through River Road specific built form and siting parameters while accommodating new active transportation infrastructure.
  - ii. **Neighbourhood Residential**  
– Located within the larger landholdings internal to the secondary plan, these lands might be anchored by open space at its centre (pending draft plan application process) and are planned to contribute to the overall housing mix and density targets of the community as a whole.
- l. **Existing Natural Open Space System** – Comprised of existing natural heritage features such as wetlands, the natural areas associated with the Speed River as well as the flood plain. The system frames the overall structure of the Plan by maintaining and conserving these areas.
- m. **Existing and Proposed Recreational and Parks/Open Space System** – Comprised of a centrally located proposed open space amenity and active transportation links to the adjacent existing open spaces of Red Wildfong and Sault Parks.
- n. **The Road System** – Comprised of an internal north-south and east-west public road system and a private road system intended to facilitate pedestrian, cycling and vehicular movement. The Street System extends into existing roads intersecting the study area, such as McMeeken and Alderson Drives.



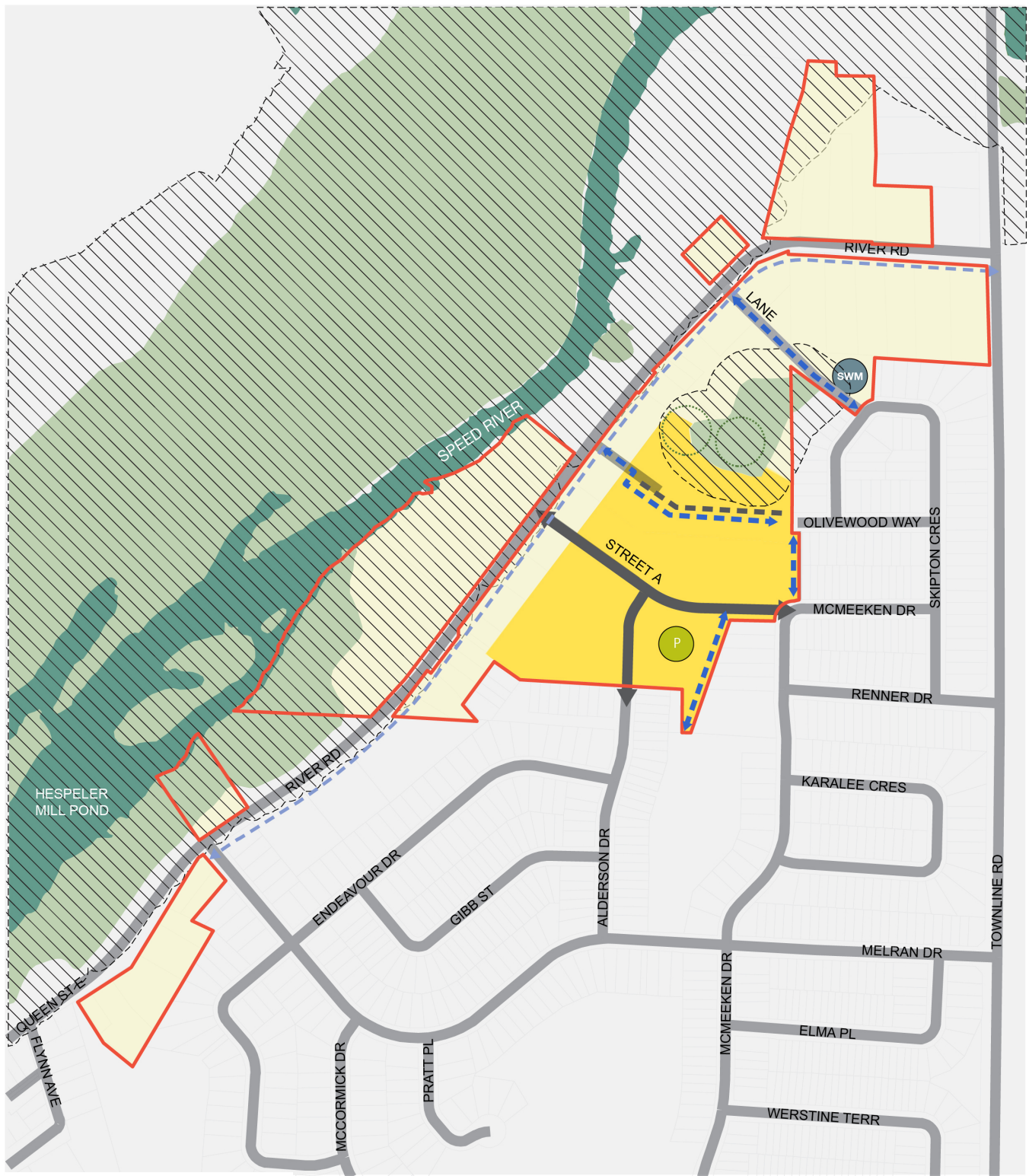


Figure 9:  
Land Use  
Structure



- Village Residential
- Neighbourhood Residential
- Regulated Wetland/Environmental Feature
- Regulated Watercourse (GRCA)
- Special Policy Area
- Butternut Tree Buffer (25.0m)

- Proposed Local Municipal Road
- Proposed Private Road
- Proposed Open Space/Park
- Pedestrian Connections
- Stormwater Management
- River Road Secondary Plan Boundary
- Proposed Multi-Use Trail Network

### 4.2.1 Sustainable and Healthy Neighbourhood

The community design elements structuring the River Road Secondary Plan area the physical land use planning manifestation of a strategy that seeks to achieve social, economic, and environmental sustainability.

Development in the River Road Secondary Plan is envisioned to achieve:

- a. **Social** sustainability by encouraging a diverse community that offers a wide range of housing mix to a wide range of people with different backgrounds, age, lifestyles and socio-economic status;
- b. **Economic** sustainability by continuing to encourage home occupation related employment and increasing the local population that can support Cambridge's business community; and,
- c. **Environmental** sustainability by:
  - i. integrating active transportation infrastructure that encourages walking and cycling;
  - ii. encouraging environmentally responsible design and construction practices;
  - iii. the integration, protection and enhancement of natural features and landscapes into building and site design;
  - iv. promoting practices, which conserve water, and protect or enhance water quality;
- v. on a site-specific basis, certain techniques such as the use of roof top gardens and the re-use of grey water will be encouraged provided that groundwater infiltration targets are not compromised; and,
- vi. design of stormwater management should consider Low Impact Development (LID) measures, on-site infiltration on a distributed basis, adaptive management design, reduced chloride impacts, and resiliency of wetlands and resilient stormwater management strategies, as outlined in the Functional Servicing Report.

### 4.2.2 Cultural Heritage

Cultural heritage resources within the River Road Secondary Plan area shall be conserved. Development in the River Road Secondary Plan is envisioned to promote development which respects and reflects the physical and cultural identity and the heritage attributes of the area. Development that is adjacent to significant cultural heritage resources are to be of an appropriate scale and character.

The cultural heritage policies of the River Road Secondary Plan have been developed based on the Cultural Heritage Resource Assessment (CHRA) Study prepared by Archaeological Services Inc. as background to the River Road Secondary Plan and Servicing Study. Based on the results of the background research and field review, there are eighteen (18) existing and potential cultural heritage resources within or adjacent to the study area. Fourteen (14) properties have also been identified as potential cultural heritage resources that may merit further evaluation to determine whether they contain cultural heritage value. As illustrated in Figure 10: Cultural Heritage Resource Assessment, the River Road Cultural Heritage Assessment confirmed existing and potential cultural heritage resources are found within or adjacent to the study area to include:

- Two properties (CHR1 and CHR11) are designated under Part IV of the Ontario Heritage Act
- One Cultural Heritage Landscape (CHL1) identified by the City of Cambridge is adjacent to the study area.
- One Canadian Heritage River (CHR12) is adjacent to the study area.
- Fourteen properties (CHR2 to CHR10, and CHR13 to CHR17) are potential cultural heritage resources that merit evaluation under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria to determine whether they contain cultural heritage value.
- Using guidance on Scenic Roads and Special Character Streets from the Region of Waterloo, River Road merits recognition as a City of Cambridge "Scenic Heritage Road" as a scenic route with a "moderate" scenic quality. In addition, River Road has been identified as a potential cultural heritage landscape under the Region of Waterloo's Cultural Heritage Landscape evaluation criteria.





Figure 10:  
Cultural Heritage  
Resource Assessment



Based on the results of the assessment, the following recommendations have been developed:

- a. The River Road Secondary Plan and Servicing Study should incorporate policies that promote the conservation of existing cultural heritage resources and consider the presence of the potential cultural heritage resources identified in this report.
- b. The City of Cambridge should evaluate all properties identified as potential cultural heritage resources in this report under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria to ensure that all potential cultural heritage resources that merit designation are conserved.
- c. If the City of Cambridge is unable to evaluate potential cultural heritage resources under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria, a planning application on or adjacent to an identified existing or potential cultural heritage resource or any demolition application should require a heritage impact assessment to further assess the cultural heritage value of the identified potential cultural heritage resources under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria, and to ensure that the existing cultural heritage resources in the study area are conserved.
- d. Should future work require an expansion of the study area, a qualified heritage consultant should be contacted to confirm the impacts of the proposed work on potential heritage resources.

The following mitigation measures and/or alternative development approaches should be considered in development design to reduce the potential for adverse impacts to potential cultural heritage resources in the area:

- e. Avoidance and mitigation to allow development to proceed while retaining potential cultural heritage resources in situ and intact;
- f. Avoidance protocols to isolating development and land alterations to minimize impacts on significant built and natural features and vistas;
- g. In support of Section 4.2.1(b) and other policies of this Plan, the City encourages design approaches that:
  - i. Ensure compatible lot patterns, situating parks and storm water ponds near a heritage resource;
  - ii. Limit height and density of buildings on adjacent and nearby lots;
  - iii. Ensure infill development that is compatible with heritage resources; and,
  - iv. Use vegetation buffer zones, tree planting, and other design measures, implemented through the review and approval of plans of subdivision and site plan applications.



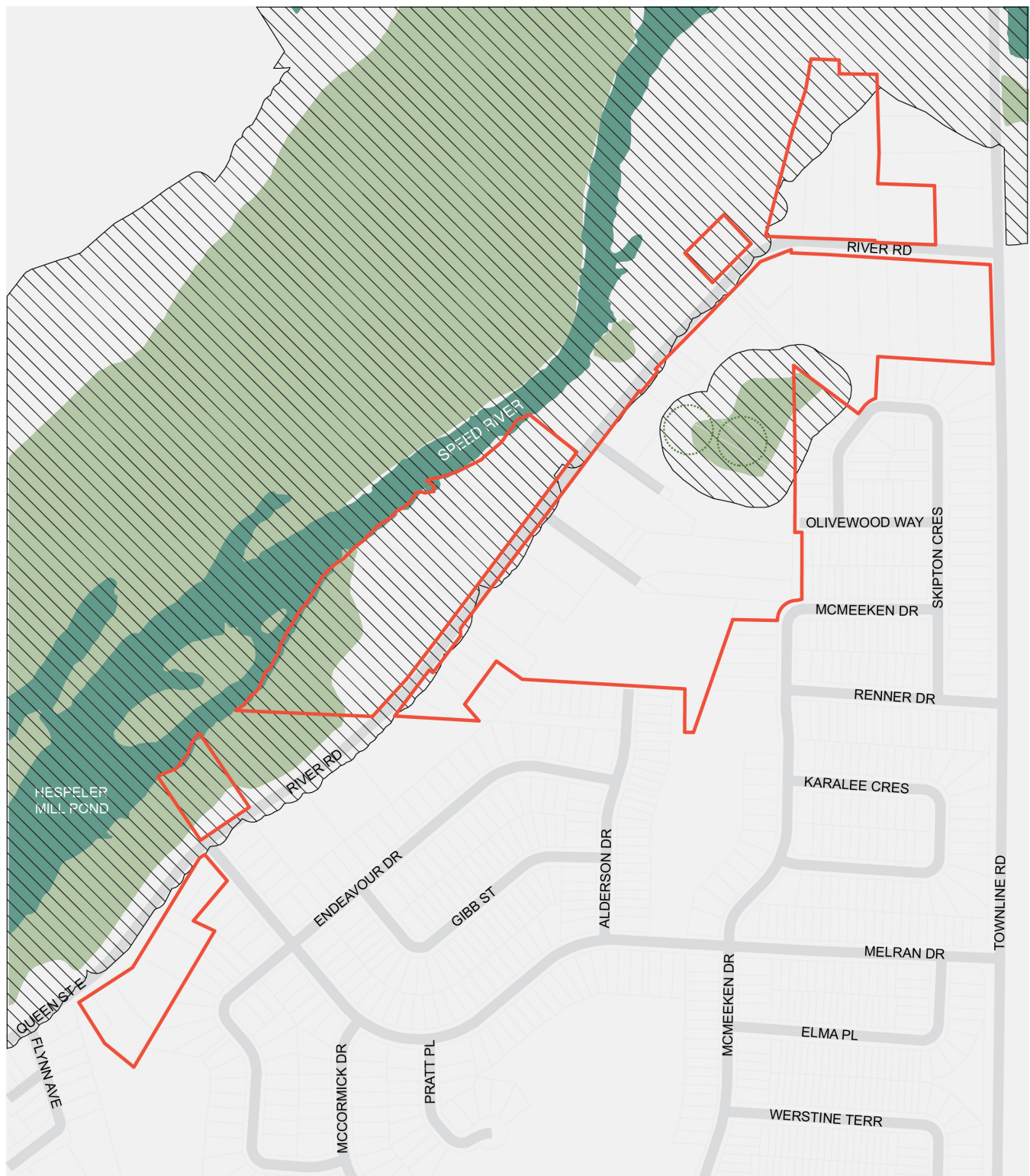
- h. Where appropriate, architectural design guidelines may be required for buildings on adjacent and nearby lots to help integrate and harmonize mass, setback, setting, and materials.

#### **4.2.2.1 River Road – Potential Scenic Heritage Road and Cultural Heritage Landscape**

It has been determined that River Road merits recognition as a City of Cambridge Scenic Heritage Road as a scenic route with a ‘moderate’ scenic quality, and has been identified as a potential cultural heritage landscape under the Region of Waterloo’s evaluation criteria. The City will consider identifying River Road as a scenic heritage road upon the establishment of policies in accordance with Section 4.14 of this City Official Plan.

Furthermore, the following recommendation has been developed:

- a. The City of Cambridge should consider adopting a Scenic Heritage Road policy and once created, consider evaluating and/or including River Road as a scenic heritage road. Until such policies are created, River Road was found to meet the criteria for recognition as a potential cultural heritage landscape under the Region of Waterloo’s criteria and could be recognized as such by the municipality. A proposed Statement of Significance and list of attributes has been included within Section 5.2 of the CHRA report. These attributes should be considered in concert with roadway improvements intended for improving public safety within the Secondary Plan area.



**Figure 11:**  
Existing Natural Open  
Space System

- Regulated Wetland/Environmental Feature(s)
- Regulated Watercourse (GRCA)
- River Road Secondary Plan Boundary
- Regulation Limit (GRCA) / Spacial Policy Area

### 4.2.3 Natural Open Space System

The lands designated Existing Natural Open Space System are shown on Figure 11: Existing Natural Open Space System. The goal of the Existing Natural Open Space System is to maintain, restore and enhance existing natural features and associated ecological functions and ensure their continued existence within the urban landscape

The Existing Natural Open Space System retains and protects all provincially, regionally and municipally designated natural features including provincially significant wetlands, river slopes erosion allowances and the regulatory floodplain associated with the Speed River.

#### Objectives

- a. To conserve and protect the area's natural open space system as an integral component of the community by providing clear and unobstructed visual and physical links to natural features where feasible and appropriate;
- b. To ensure a connected open space system is implemented and comprised of the existing natural features, stormwater management facilities, streets, trails, new public park with a formal and accessible trail connection through Red Wildfong Park to Melran Drive and McMeeken Drive;
- c. To encourage the conservation and integration of existing hedgerow and woodland features into the community's overall design and development.

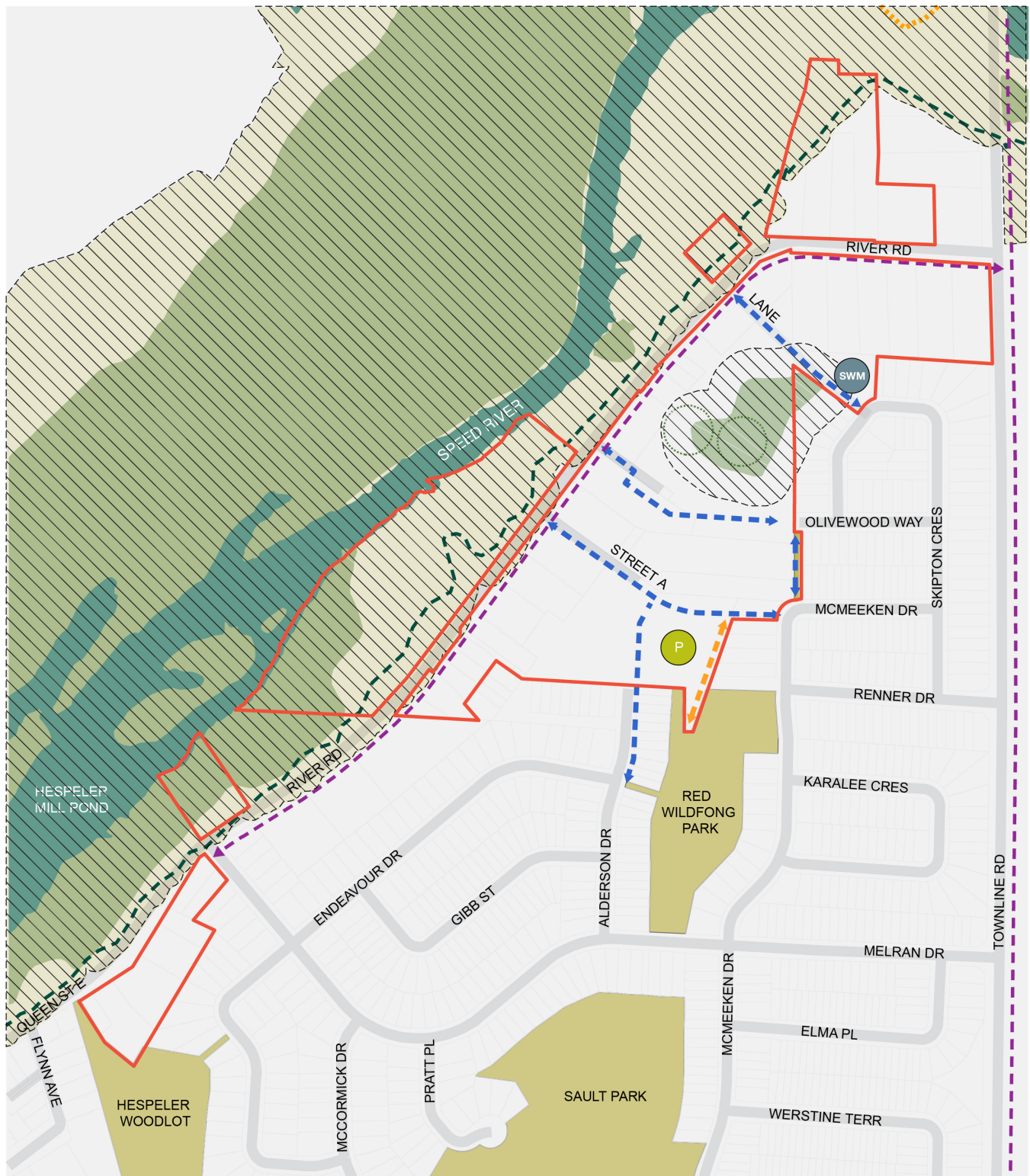
#### General Policies

In addition to the policies in Section 8.8 of the City of Cambridge Official Plan, the following policies shall apply to the lands designated as Existing Natural Open Space System on Figure 11 of this Secondary Plan:

- d. The goal of the Existing Natural Open Space System is to maintain, restore and enhance existing natural features and functions and ensure their continued existence within the urban landscape.
- e. Buffers and development setback distances will address potential impacts and protect and enhance natural features. Setback distances and buffer widths vary by natural feature to ensure development does not interfere with the hydrologic function of the feature. This is to include areas within 120 metres of all provincially significant wetlands and wetlands greater than or equal to 2.0 hectares in size, and areas within 30 metres of wetlands less than 2.0 hectares in size. O. Reg. 150/06, s. 2 (1); O. Reg. 57/13, s. 1 (1-3).
- f. Development and site alteration within the buffers areas is subject to further detailed study and evaluation in a future site-specific Environmental Impact Study.

- g. Development, infrastructure and site alteration which may be considered within the buffer areas include:
  - i. Limited portions of stormwater management facilities and associated grading.
  - ii. Recreational trails and associated grading.
  - iii. Site alteration associated with grading incursions to address landscape; and anomalies as required to achieve sound engineering practice and design.
- e. Development or site alteration within natural features and buffers will be prohibited except as may be required for the permitted uses specified in Section 4.2.3.1 (d).
- f. The Existing Natural Open Space System will maintain or enhance existing linkages within and between large natural areas and restore linkages between natural areas where existing connectivity is degraded. Linkages are intended to provide movement areas for a range of wildlife and provide suitable dispersal corridors.
- g. The Existing Natural Open Space System will provide opportunities for ecological enhancements to improve plant and wildlife habitat.
- h. Opportunities for additional enhancements (wildlife crossing enhancements) should be considered during the future detailed design of River Road and at the development planning stage.
- i. Environmental Impact Studies, Hydrogeologic Assessments and Stormwater Management Plans will be required to form part of a “complete application” and as supporting information for draft plans of subdivision, plans of condominium and major zoning by-law amendment applications.





**Figure 12:  
Open Space  
System**

- |  |                                    |
|--|------------------------------------|
| Regulated Wetland/Environmental Feature(s) | Existing Open Space/Parkette       |
| Regulatory Storm Floodplain (GRCA)         | Proposed Open Space/Park           |
| Regulated Watercourse (GRCA)               | Pedestrian Connection              |
| Special Policy Area Overlay                | Trail Connection                   |
| Butternut Tree Buffer (25.0m buffer)       | Stormwater Management              |
| Proposed Multi-Use Trail Network           | Significant Valley Lands (ROP)     |
|  | River Road Secondary Plan Boundary |

## 4.3 Open Space System

The River Road Community will provide opportunities for residents to live, work, learn and play and will contribute to the achievement of a complete Cambridge community. The proposed open space network encourages active transportation, recreation, and opportunities to connect across the neighbourhood. See Figure 12: Open Space System.

### 4.3.1 Regulated Environmental Features

#### 4.3.1.1 Regulated Wetland/ Environmental Features

Regulated Wetland/Environmental Features on Figure 11: Existing Natural Open Space System and Figure 12: Open Space System recognize the limits and associated minimum buffers of a Provincially Significant Wetland. The ecological and hydrologic functions of the lands designated Regulated Wetland/Environmental Features shall be protected, maintained, or, where possible, improved for the long-term, recognizing linkages between and among natural heritage features and areas.

The Regulated Wetland/Environmental Features designation is intended to:

- a. protect the health and water quality of the natural heritage feature;
- b. conserve biodiversity;
- c. protect significant natural heritage features and their associated functions; and,
- d. protect surface and ground water resources.

#### 4.3.1.2 Special Policy Area Overlay

The Special Policy Area Overlay recognizes natural heritage features, which contribute to the Natural Heritage System.

- a. Additional environmental study is required to determine the exact boundaries and buffers to protect the features and functions to the satisfaction of the City of Cambridge, the Grand River Conservation Authority and Region of Waterloo.
- b. The proponent shall be required to submit an Environmental Impact Study as part of any future development application to demonstrate that, there will be no significant negative impact on the natural heritage features or their functions.
- c. Should it be demonstrated through an Environmental Impact Study that a natural heritage feature may be reduced or removed, the adjacent land use designation shall apply.

### 4.3.2 Existing Recreational, Parks/ Open Space and Proposed Parks/ Open Space

The River Road Community will provide opportunities for residents to live, work, learn and play and will contribute to the achievement of a complete Cambridge community. The proposed open space network encourages active transportation, recreation, and opportunities to connect across the neighbourhood. A new park, a trail system along River Road, a trail connection through Red Wildfong Park, existing pedestrian connections and other elements of the active transportation network (such as sidewalk and on-road cycling routes) are illustrated on Figure 12: Open Space System and Figure 13: Street Structure.

#### 4.3.2.1 Open Spaces and General Policies

- a. A new park is envisioned to be located central to the community's larger land holdings and may serve as a community hub. The new park will also feature a trail connection through the Red Wildfong forested area to connect to Melran Drive and to McMeeken Drive.

#### 4.3.2.2 Existing Trees

The rural character of the River Road Secondary Plan area is established by the abundance of trees. They occur throughout the study area with a large concentration located at the east edge of the plan area immediately adjacent to the existing community. Trees are an important component of the River Road Secondary Plan area as they enhance the character of the area, provide wildlife habitat, nourish and protect soils from wind and water erosion.

- a. Development of the Secondary Plan area should seek to maintain canopy and the Urban Forest Plan target. The area as a whole should meet the Urban Forest Plan canopy target (30% canopy) by protecting existing trees (in the natural area), adding new street trees, buffer compensation plantings, and anticipated private property landscaping plans/trees in condominium blocks.
- b. A tree management plan / tree by-law permit for removals, street tree plans, as well as applicable Landscape Plans on Site Plan submissions, will be requirements for development applications.
- c. Integration of the treescape into future development plans as part of rear yards or as important element of front yards and future streets is required.
- d. Tree plantings should include a variety of native trees, appropriate to the local habitat.





**Figure 13:**  
Street/Active  
Transportation  
Structure



## 4.4 Street and Active Transportation Structure

The vision for the Street / Active Transportation Structure on Figure 13: Street Structure, is to deliver complete streets designed to enable safe access for all users with particular emphasis on vulnerable road users in accordance with the City standards street guidelines with a clear road hierarchy based on a connected modified grid network that accommodates for all modes of travel fundamental in the implementation of an identifiable, great community.

The objectives of the transportation network are to:

- e. Develop a well-connected integrated network of streets, paths and trails that enhances connectivity and safely accommodate various modes of active transportation, including walking and cycling.
- f. Design the community at the pedestrian scale to promote social interaction, active streetscapes and a walkable neighbourhood;
- g. Preserve River Road's character through the implementation of a special land use character (Village Residential) zone and a specific road solution based on a modified functional rural cross section;
- h. Preserve River Road's character through the implementation of a native planting strategy where street trees and a combination of local ground cover such as grasses are an integral component of the street;
- i. Incorporate traffic calming measures, where warranted, and pedestrian crossings within the neighbourhood and along River Road;
- j. Facilitate the implementation of an interconnected road network extending to adjacent neighbourhood areas;
- k. Distribute traffic to the surrounding road network;
- l. Minimize traffic impacts on adjacent communities; and
- m. Minimize impacts on the surrounding natural features.

### General Policies

- n. The planning, design and construction of all roads and walkways within the River Road Neighbourhood will have regard to the conservation of significant natural and cultural heritage features and contribution to the overall streetscape design.
- o. The design of the street network will provide for all forms of travel including walking, cycling, public transit and the automobile through:
  - i. The creation of a modified-grid street network with short walkable blocks that are generally not to exceed 250m in length;
  - ii. The use of site planning and urban design techniques that foster attractive and safe pedestrian-friendly streetscapes and built form;

- iii. The implementation of a River Road unique functional rural cross section that includes active transportation amenities;
  - iv. The provision of continuous pedestrian systems (sidewalks) and linkages to community trails and open spaces; and
  - v. The provision of multi-use pathways including cycling facilities, improved public and private pedestrian amenities, well planted greenways, and uses which encourage walking.
- f. The implementation of Transportation Demand Management measures shall be considered as part of every application for new development or redevelopment within the Secondary Plan area.
  - g. Development applications may be required to provide Transportation Impact Studies to the satisfaction of the City and Region.
  - h. Further south, existing conditions indicate that the intersection at Queen Street West/East and Guelph Avenue/Adam Street is experiencing a low level of service due to heavy southbound right turn volume. Future opportunities to improve capacity at this intersection should be addressed.

#### 4.4.1 Streetscape Policies

Streetscapes throughout the River Road community are important components of the public realm. Streetscapes will be designed to enhance community character and sense of place. Streetscape design shall be focused along the community's public and private streets to include: River Road, McMeeken Drive, Alderson Drive and any other new local public and/or private streets.

- a. A high-quality public realm along streets will be achieved by coordinating the design of individual site elements, the overall transportation network and individual development applications along the streetscape.
- b. Backlotting along the three primary streets (River Road, McMeeken and Alderson Drives) will be prohibited. On corner lots, the main building face shall be oriented towards the higher-order street.
- c. Residential streetscapes shall be designed to ensure the provision of sufficient on-street parking through creative design solutions such as varying housing types and lotting patterns.
- d. Streets shall be designed to be more pedestrian friendly through various means, including human scale massing of built form along the street, pedestrian-scale lighting, landscape treatments and street furniture. Streetscape plans for all public and private streets shall be required as a condition of draft plan approval.

- e. Local streets shall be designed to encourage on street cycling safety consistent with the City's Cycling Master Plan.

#### 4.4.2.1 River Road

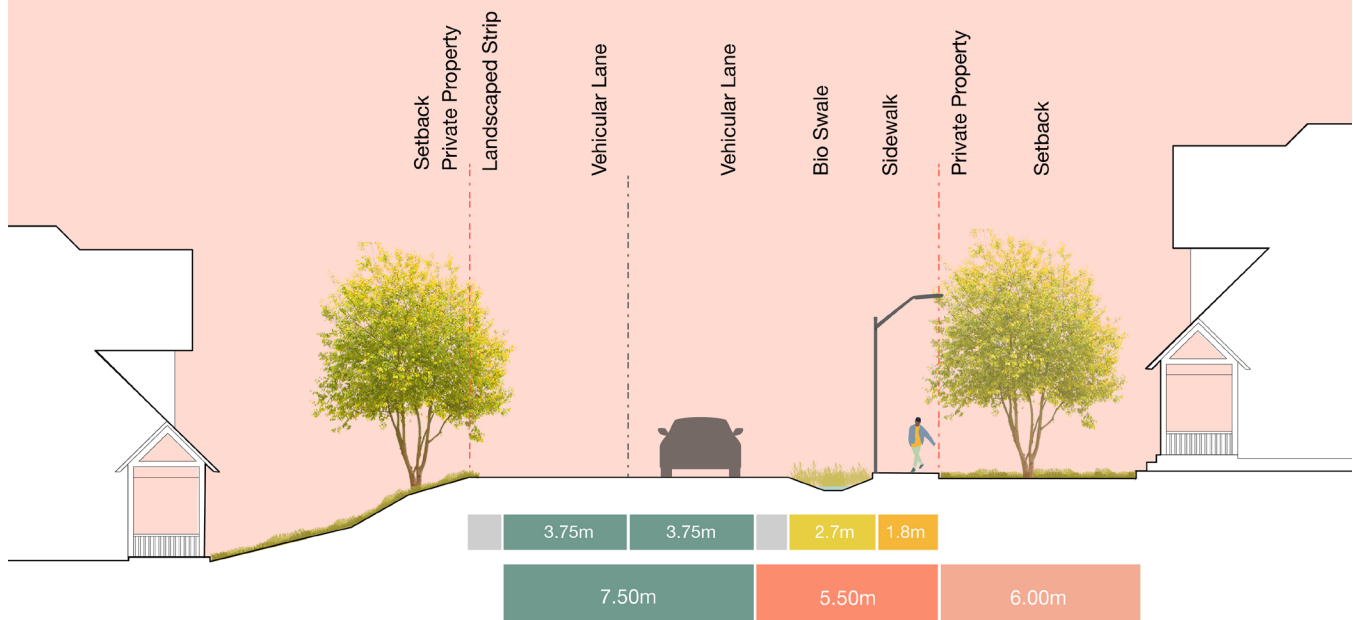
As one of the most iconic scenic routes in the City of Cambridge, River Road is envisioned as a future pedestrian and cyclist friendly corridor. Its design is to:

- a. Accommodate for an east multi-use trail accessible to pedestrians and cyclists;
- b. Explore the implementation of a rural cross section with deciduous trees and swales as an essential component of its engineering and its streetscape character;
- c. Be framed by a relatively consistent building edge in order to provide spatial definition and containment to the street consistent with historic lotting patterns as per Zoning By-law directives; and,
- d. Implement a utility strategy where utilities are clustered or grouped where possible to minimize visual impact.

The preliminary design of River Road's future cross section is based on the intent of preserving the road's rural scale while ensuring a sustainable storm water management strategy, servicing upgrades (as needed) and a safe and comfortable active transportation route (i.e., pedestrian and cycling trail) are part of the road's ultimate design. The preliminary road solution is based on a modified functional rural cross section to be implemented as development occurs. Future road enhancements will require an average of an additional six (6.0) metres to be added to the right of way.

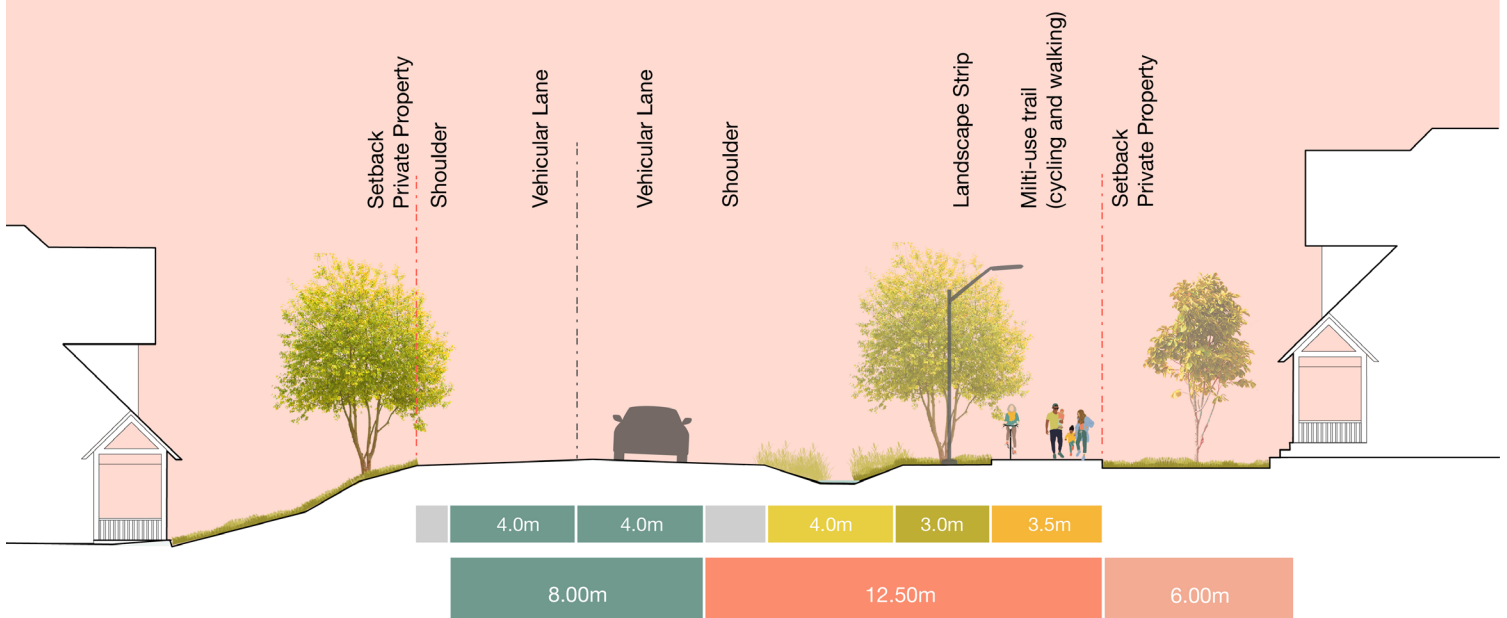
**a. Phase 1 - Figure 14: Short Term  
(current 14.0m right-of-way)**

- i. Maintain the road's rural character (i.e. slopes shoulders) and a 7.5m pavement width.
- ii. Any upgrades to River Road should include the replacement of the 3-cable guiderail system along the embankment.
- iii. Ensure sustainable water management measures are introduced as part of the interim road upgrades such as the illustrated bioswale.
- iv. Introduce a 1.8m pedestrian (AODA complaint) sidewalk on the east side of the road.
- v. The proposed interim upgrades (i.e. sidewalk and bioswale) are important visual traffic calming elements. Further opportunities to buffer pedestrians from traveling vehicles should be explored to include the planting of native grasses (along the bioswale) and/or street trees where feasible.



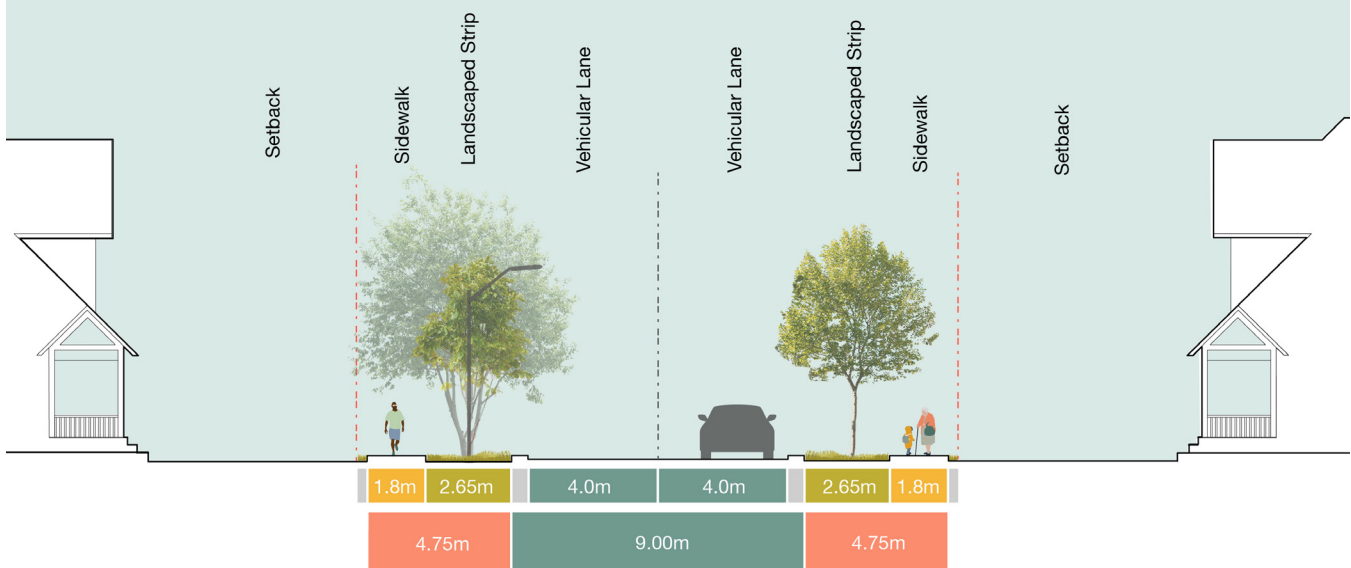
**b. Phase 2 – Figure 15:  
Mid to Long Term (20.0m right-of-way)**

- i. Maintain the road's rural character (i.e. sloped shoulders) and widen pavement to 8m.
- ii. Ensure sustainable water management measures remain as part of the road design solution by widening the interim bioswale to comply to City standards (refer to 20m rural cross section City standard C108).
- iii. Introduce a 3.5m combined pedestrian and cyclist trail on the east side of the road.
- iv. The proposed ultimate upgrades (i.e. multi-use trail and bioswale) are important visual traffic calming elements. Further opportunities to buffer pedestrians from traveling vehicles should continue to be explored to include the planting of native grasses (along the bioswale) and street trees immediately adjacent to the multi-use trail as further pedestrian and cyclist protection.
- v. Introduce a street lighting strategy primarily focused on illuminating the multi-use trail. The future street lighting strategy will need to ensure no light spills over the westerly side of the road along the wetland areas.



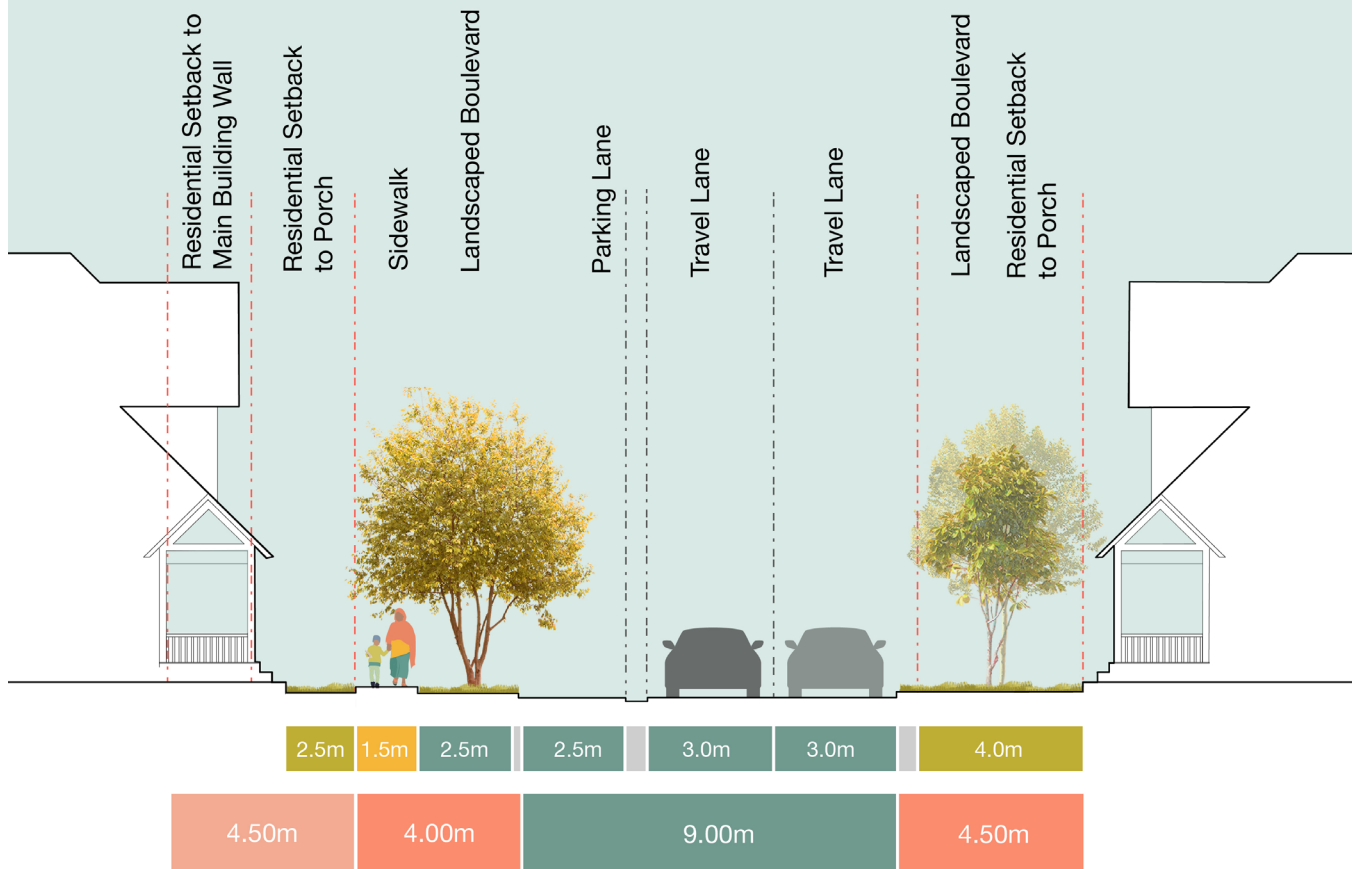
#### 4.4.2.2 Local Roads - Figure 16: Local Road Cross Section

- The local street network should facilitate vehicle, cycling and pedestrian connections throughout the neighbourhood.
- The local street network should be based on a modified grid form or other road pattern that results in a high level of connectivity. A discontinuous street pattern and dead-end streets are discouraged.
- All local roads will have a minimum width of 18.5 metres.
- Pedestrian sidewalks shall be provided on both sides of all local streets.
- On-street parking is encouraged on one side of the street.



#### 4.4.2.3 Private Roads - Figure 17: Private Road Cross Section

- a. Private roads are permitted in specific conditions subject to City's standards and to the approval of the City of Cambridge.
- b. Private roads should facilitate vehicle and cycling connections and must provide for pedestrian links throughout the neighbourhood.
- c. When present, private roads are an integral component of a neighborhood by providing for a continuous street system. A discontinuous street pattern and dead-end streets are
- d. An on-street parking lane is not necessarily required if suitable dedicated visitor parking areas are provided within the development site to the City's satisfaction.





#### 4.4.2.4 Active Transportation

- a. A network of bicycle routes and multi-use trails will be provided throughout the community. Refer to Figure 13: Streets/Active Transportation Structure.
- b. River Road should provide for an off-road pedestrian and cycling route. Multi-use trail facilities should be designed to be consistent with the City of Cambridge Bikeway Network plan.
- c. The local road network should provide for on road cycling routes. On road cycling facilities should be designed to be consistent with the City of Cambridge Bikeway Network Plan.
- d. Sidewalks will be provided on both sides of all streets located internal to the community.
- e. Priority pedestrian crossings identified conceptually in Schedule 2 will be further defined and implemented through the plan of subdivision approval process.
- f. Traffic calming measures should be coordinated with pedestrian crossings and other elements of the active transportation network as shown in Schedule 2. Traffic calming measures should be shown on a consolidated plan in support of draft plans of subdivision with special emphasis given to pedestrian connections to planned and existing open space amenities as well as a safe pedestrian crossing.



## 4.5 Land Use Structure

The land use framework as illustrate in Figure 9: Land Use Structure was developed on the basis of building on the rural character of River Road by generally maintaining the existing lot fabric along the roadway, while planning for a mix of low to medium density residential development east of the River Road corridor and west of existing developed areas.

The land use structure and planned densities for the River Road Secondary Plan area are based, in part, on the policies of Section 2.6.1.8 of the Official Plan directing that infill, intensification and redevelopment within existing neighbourhoods will be minor in nature and will be designed to respect existing character. While planned infill, intensification and redevelopment is more limited when compared to other areas in the City, the policies of the River Road Secondary Plan will allow for a mix of residential dwelling types and forms with new development.

Development throughout the River Road Secondary Plan area shall be encouraged to create a sense of identity and place through the use of various means including attractive built form, architectural design treatments, high quality building materials, decorative lighting, decorative street signs, boulevard treatments, and landscaping elements. The following policies are intended to apply to all new development or redevelopment within the River Road Secondary Plan area to ensure the overall design vision and objectives for the area are achieved.

### General Policies

- a. Residential lands in the River Road Secondary Plan area are designated Village Residential and Neighbourhood Residential. The Village Residential designation applies primarily to lands located along River Road, while the Neighbourhood Residential designation applies to lands that are located more centrally in the broader River Road community (i.e., east of River Road and west of the existing residential neighbourhood). The location of the Residential designations is shown on Figure 9: Land Use Structure.
- b. Lands within the River Road Secondary Plan area will be planned to achieve an overall minimum density of 12 units per hectare and a maximum density of 16 units per hectare. Accessory apartments are not included in the calculation of density. Individual development applications may have densities lower than 12 or higher than 16 units per hectare provided that the City is satisfied that the total overall density within the Secondary Plan area, at full build-out, will conform to the overall planned density range.

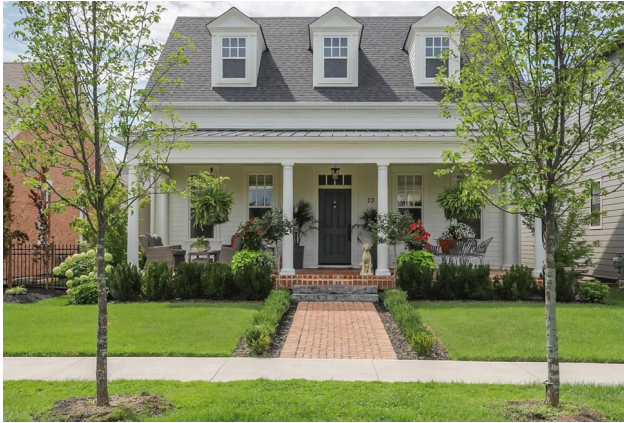
- c. The River Road Secondary Plan area will be planned, designed and zoned to achieve a variety of housing types, styles and lot sizes. Single detached lots will be planned to have a variety of lot widths, unless specified otherwise in this Plan.
- d. Secondary residential units and special needs housing are permitted within Village and Neighbourhood Residential designations and will be regulated by the Zoning By-law.
- e. Community gardens and other compatible forms of urban agriculture may be permitted in all residential areas.
- f. Residential streetscapes shall be designed to allow for on-street parking.

#### 4.5.1 Affordable Housing

The City encourages opportunities to locate affordable rental and ownership housing within the Secondary Plan Area (as defined in the Provincial Policy Statement). The City encourages a diverse range of tenure types and unit sizes scaled to meeting the city's housing needs.

### 4.5.2 Built Form and Lotting Policies

- a. With the exception of lots fronting onto River Road, a mix of lot frontages along all street blocks will be encouraged. Long blocks of similar frontages will be discouraged.
- b. Laneways are encouraged throughout the community, in particular to provide vehicular access to lots fronting onto River Road in order to minimize driveway and vehicular access off River Road.
- c. A variety of building forms are encouraged. Harmonious built form typologies are encouraged within the River Road community to collectively establish a sense of place and neighbourhood character.
- d. Built form should frame intersections and provide attractive and defining entrances to the community.
- e. All building forms are encouraged to address the street in a manner that supports an active streetscape for pedestrians. Primary building entrances will be required to be oriented towards the public realm and garage walls and driveways will be required to minimize their presence on the streetscape.
- f. Consideration will be given to the location, orientation and design of Priority Lots. Through the development application review process, priority lots should be identified. Priority Lots may include: corner lots; terminating view/vista lots; lots along elbow streets; and open space lots.
- g. Buildings on corner lots will be required to articulate façades on both street frontages. Buildings on corner lots shall be oriented towards the higher-order street where possible and appropriate.
- h. Buildings at terminating vistas will be encouraged to be designed as landmarks, with architectural innovation and quality urban design that reflects the community character.
- i. The built form in close proximity to the lands designated Natural Open Space System or Open Space will be designed to be sensitive and complementary to these areas and maintain and create opportunities for views and vistas to these areas, where appropriate.
- j. Variation and excellence in building design, including architectural features, building materials, style, colour and other individual design elements, is encouraged and will be reviewed through applicable development applications. Continuous, blank facades are discouraged. Built forms may be further defined and implemented through the City's Zoning By-law and site plan approval.
- k. Design briefs are required in support of future development application and will identify priority lots such as vista, terminating view, corner lots, etc. Prior to the issuance of permits, enhanced elevations will be prepared for the identified lots in accordance with the River Road Urban Design Policies with the requirement imposed as a condition of approval.



Single Detached, Niagara-on-the-Lake, ON



Semi-Detached, Niagara-on-the-Lake, ON

### 4.5.3 Village Residential

- a. The planned intent of the Village Residential designation is to complement River Road's historic development pattern and scale in the form of ground-oriented housing in a low-rise format.
- b. The Village Residential land use designation will permit single detached, semi-detached, and triplex dwellings with a target density of 9 units per hectare and a maximum density of 13 units per hectare. Accessory dwelling units are also permitted but are not included in the calculation of density.
- c. The maximum building height in the Village Residential land use designation will be 2 storeys and will be regulated by the Zoning By-law.
- d. In addition to the built form, landscape and vegetation are an important character element of River Road. Generous naturalized front "meadows" are intended to ensure the area's distinctive rural character is maintained as it accommodates for new development.



#### 4.5.4 Neighbourhood Residential

- a. The planned intent of the Neighbourhood Residential designation is to provide opportunities for a range of residential typologies in a medium density format that deliver on the diversity of dwelling types and densities encouraged to support the City's range of accommodation options.
- b. The Neighbourhood Residential land use designation will permit single detached, semi-detached, triplex, fourplex, townhouse, stacked townhouse, back-to-back townhouse and stacked back-to-back townhouse dwellings to a maximum density of 40 units per hectare. Accessory units are also permitted but are not included in the calculation of density.
- c. Building heights will be a maximum of 3 storeys and will be regulated by the Zoning By-law with specific consideration being given to maximum building height and compatibility relative to existing development in the immediate vicinity to ensure appropriate transition between existing and proposed development.



Semi-Detached, Niagara-on-the-Lake, ON



Fourplex, Cobourg, ON



Townhouses, Markham, ON

05

# Urban Design Policies



## 5.1 Urban Design Vision

The Urban Design Vision for the River Road Secondary Plan builds on the overall Vision and objectives for the River Road Secondary Plan, and is based on the City of Cambridge's Official Plan and aims to deliver a new community that:

*“(...) supports and emphasizes River Road’s unique rural character and built heritage features, in a way that allows residents and visitors to better enjoy the natural environment and scenic views associated with the Speed River.*

*(...) Is envisioned as a sustainable and accessible neighbourhood with a range of housing, a pedestrian friendly environment and multiple recreational opportunities.”*

The urban design vision encourages and manages development in a manner that delivers a vibrant and diverse community. Integral to this vision is the design and integration of the Village Residential and Neighbourhood Residential areas, each with a variety of residential housing options. In addition, the urban design vision delivers policies to ensure a high- quality, sustainable and accessible public realm and private realm.

## 5.2 Development Block Design

Block length and design define and structure neighbourhoods, influencing development opportunities, movement options and neighbourhood character. Each guideline may not be appropriate for all instances and an alternative may be better suited for a specific development. Therefore, variations to these provisions may be considered by City staff and where applicable Council, when it is demonstrated that an exception may be appropriate, provided that adjustments to block lengths maintain and facilitate pedestrian movement throughout the community through the provision of clear and safe pedestrian connectors and walkways.



The following are design guidelines for block design in the River Road Secondary Plan area:

- a. To maximize connections and permeability for both vehicular and active transportation modes, the street should be based on a grid pattern that is modified in response to natural open space, built heritage or existing street conditions.
- b. Block lengths should generally range between 60 to 100 metres to promote walkability.
- c. New developments that are adjacent to existing built form should pursue opportunities to integrate with or connect to existing streets. The number of connections should be maximized to increase permeability.
- d. Blocks longer than 100 metres may be considered on an individual basis where other block alternatives are not feasible. In such cases, publicly accessible pedestrian walkaways should be provided to facilitate pedestrian circulation and access.
- e. Draft plans shall be designed to ensure that residents are generally located within a 200 to 400 metre radius (2.5-to-5-minute walk) to a parkette or to other passive recreational elements such as trail heads, neighbourhood park, community park, or school.
- f. Where possible, existing hedgerows should be conserved and integrated into the neighbourhood's local street and block pattern.
- g. Where rear lanes are used, the maximum lane length should be no more than 180 metres in order to provide for a maximum 90-metre hose length from fire hydrants located on road connections. Where the 180-metre length is exceeded, an on-street hydrant location and fire hose access to the rear lane shall be provided.
- h. The use of cul-de-sacs should be minimized, except where necessary due to grading and topography or at view terminus sites. Where cul-de-sacs are used a pedestrian and/or bicycle through-connection should be provided to promote active transportation.
- i. Significant hedgerows within public open space or parks and/or rear backyards should be protected where possible and feasible. Any grading activities around the hedgerows should be minimized.

## 5.3 Public Realm Improvement

### 5.3.1 Streetscapes

Where appropriate and feasible, collector, local and private roads streetscape design will include sidewalks on at least one side of the street; on-road or separated bicycle lanes and trail connections.

Standard street design guidelines put forward by this document include:

- a. The careful evaluation of the alignment of streets adjacent to existing hedgerows and wooded areas to ensure the site's existing natural features are visible and part of the community's open space network;
- b. Construction of LID SWM Galleries can be implemented along the proposed roadway boulevards to provide both quality and quantity control for the proposed development. Subject to groundwater levels and soil conditions, these could consist of bio-swales and/or sub-surface infiltration galleries or sub-surface storage cells. (Refer to Appendix: Functional Servicing Assessment)
- c. Streets should have a relatively consistent building edge in order to provide spatial definition and containment to the street. A minimum and maximum front and exterior setback zone is defined in the accompanying Zoning By-law
- d. A utility strategy where utilities are clustered or grouped where possible to minimize visual impact should be developed as part of the draft plan process.

### 5.3.2 Street Tree Guidelines

The combination of both public realm improvements as well as the design guidelines for the private realm are intended to increase the overall tree canopy along River Road and support the overall City-wide target for a 30% tree canopy. To support the achievement of the target, the City will:

- a. Endeavour to create an extensive tree canopy over all pedestrian connections in the Secondary Plan Area;
- b. Ensure street trees are spaced every 12 meters to continue the treed streetscapes of the surrounding neighbourhood as well as meet the standards of the Engineering Development Manual; and,
- c. Promote the use of innovative systems which allow for best practices in arboricultural maintenance.

### 5.3.3 Existing Trees

The conservation of trees aims to provide the best conditions for growth, to encourage stand renewal and to improve the health, diversity and appearance of the landscape, through three basic approaches: cleaning, thinning and stand improvement.

- a. Cleaning is done in young stands to regulate composition and favour valuable species.
- b. Thinning can remove up to 35% of the medium age (25 years) stand to allow the remaining quality trees more room and light to grow fuller and faster.
- c. Stand improvement harvests large mature trees for sale and removes defective, crooked and leaning specimens and inferior species. A few large dead trees and snags are left for wildlife habitat.

## 5.4 Open Space

The River Road Secondary Plan area is envisioned to include a centrally located park as well as a system of interconnected pedestrian walkways and trail facilities (refer to Figure 12: Open Space Network). The Natural and Recreation Open Space Systems are consistent with the Official Plan designations (Section 8.8) and permitted uses.

The following are general guidelines for the community's open space:

- a. New public spaces should be designed to be barrier free and to include a mix of design elements, including but not limited to: enhanced landscaping; shade trees; and seating.
- b. New public spaces should be located close to the street and be connected to the pedestrian network.
- c. New open space should be designed to in a manner that minimizes grade changes to the extent possible.
- d. Public spaces should be designed through Crime Prevention Through Environmental Design (CPTED) principles by ensuring clear view into and out of surrounding areas.
- e. Where residential side yards abut a park, a combination of decorative fencing, landscaping, and privacy fencing should be provided to demarcate the public and private realm, and to ensure the privacy of the residence is maintained.
- f. The stormwater facilities should be a key focal/visual feature within the community, contributing to its appearance and ambiance. Therefore, careful consideration should be taken for the design and integration with the adjacent neighbourhood and open space.
- g. Pedestrian and cycling trails should be incorporated into the design of stormwater facilities, where feasible.

## 5.5 Active Transportation

### 5.5.1 Public Pedestrian Network

New active transportation connections are illustrated in Figure 13: Active Transportation. The network is comprised of a system of pedestrian street sidewalks, walkways and trails with the intent of providing pedestrians with options to get to their destination using the most direct route. The following active transportation connections have been identified:

- a. **Sidewalks along all public and private streets** – As integral components of the community open space system and active transportation network, sidewalks are required along all streets in the community.
- b. **Multi Use Trail along River Road** – Envisioned as a pedestrian and cycling multi use facility on the east side of the road, the River Road multi-use trail is intended to improve the road conditions by introducing a much needed safe and comfortable pedestrian and cycling route.
- c. **Existing pedestrian walkway connecting Skipton Crescent to River Road** - Located in the north side of the secondary plan area, this pedestrian connection runs east-west and is to be maintained as development along River Road occurs.
- d. **Future pedestrian link at Laneway 211** – This pedestrian connection is intended to provide a pedestrian connection to River Road through the future proposed private road to Olivewood Way. An easement for public pedestrian access over the private road to Olivewood Way will be conveyed to the City as a condition of any development.
- e. **Existing pedestrian and cycling connection between Olivewood Way and McMeeken Drive** – Located on the easterly edge of the secondary plan area, this connection is to remain as development occurs to ensure a pedestrian mid-block connector is maintained and transverses the long block defined by Skipton Crescent to the east and River Road to the west.
- f. **Future trail connections to Red Wildfong Park** – The new park will provide a trail connection through the wooded area of Red Wildfong Park through to Melran Drive and McMeeken Drive.

In order to encourage walkability within the neighbourhood, and improve circulation and pedestrian access, the following guidelines are intended to create accessible, safe and inviting pedestrian connections:

- g. All public streets are to provide for sidewalks on both sides of the street with the exception of River Road where a multi-use trail on the east side of the road is to be located.
- h. All private streets are to provide for a sidewalk on one side of the street.
- i. In order to accommodate the needs of persons with disabilities, and the elderly, sidewalks should be designed to applicable municipal and AODA standards with a preferred 1.8 metres minimum width and should be continuous throughout the community.



- j. Pedestrian walkways providing a link across blocks should be 6.0 metres wide to avoid creating narrow pedestrian alleys.
- k. Trails will be 3 metres wide and asphalt to conform with the Trails Master Plan. To minimize disturbance in natural features (e.g. Red Wildfong Park forested area) the City may consider a narrower trail width of 2 metres to minimize disturbance to the natural features while still maintaining the intent of the Trail Master Plan and compliance with the Accessibility for Ontarians with Disabilities Act (AODA) regulation.
- l. Crime Prevention Through Environmental Design (CPTED) principles should be used in the design of public pedestrian walkways, such as:
  - i. Adequate lighting;
  - ii. Clear sight lines, allowing view from one end of the walkway to the other;
  - iii. Provide appropriate landscaping, but avoid landscaping that might create blind spots or hiding places;
  - iv. Adequate fencing and fenestration adjacent to public walkways to provide opportunities for casual surveillance; and,
  - v. Integrating activity generating uses such as seating areas or other amenities.



Streetscape/Sidewalk, Markham, ON



Pedestrian Connection, Brampton, ON



Pedestrian Mews, Toronto, ON



Multi-Use Trail, Etobicoke, ON



Share the Road  
(Beacon Lite)



Shared Pathway  
(Barricade Traffic Services Inc)

### 5.5.2 Cycling Network

On-street and multi-use trails have been proposed for the Secondary Plan area. Where possible, new bicycle lanes will be provided in accordance with the Bike Your City: Cycling Master Plan (2020) and link with the existing cycling network in Cambridge. The following are guidelines for the cycling network:

- a. Where cycling routes intersect major intersections, route signage should be provided.
- b. Access to cycling routes should be clear and unobstructed.

## 5.6 Natural Heritage System

A Natural Heritage System (NHS) has been established, which is comprised of a Provincially Significant Wetland and Speed River Waterway. In addition, the Regional Official Plan identifies Regionally Significant Core Environmental Features and Significant Valleys in the Secondary Plan Area. The designated areas are subject to further Environmental Impact Studies that must be reviewed and approved by the Region and City in consultation with the Grand River Conservation Authority. The following guidelines are for land designated in the NHS:

- a. Integrate the NHS as a key structural element of the neighbourhood by providing appropriate views, vistas and connections, as well as providing a range of development interfaces that offer opportunities for public visual and physical access while also limiting access where necessary.
- b. Physical barriers, such as lot fencing, should be reviewed and considered in areas where access and encroachment are to be restricted. Fencing should be coordinated as a condition of draft plan approval to ensure consistent fencing design is established.
- c. For homes adjacent to the NHS, homeowner education and stewardship should be encouraged through the distribution of a homeowner's pamphlet as a condition of draft approval. Information in the pamphlet may include conservation practices and protection of the surrounding NHS, which include the identification of native plant species appropriate to the existing ecosystem, and the avoidance of organic waste dumping.
- d. Carefully consider the design and location of parking areas, three stream receptacle bins or street furniture to ensure that the edge of the NHS is not degraded. This can be achieved through larger setbacks or landscaped buffers with private and decorative fencing.

## 5.7 Private Realm Guidelines

New residential developments within the River Road Secondary Plan will integrate with the existing built form and scale of its context. It is essential that future community and architectural design solutions address matters of scale, siting and massing through the implementation of the design best practices contained in the Secondary Plan. These Urban Design policies aim to reinforce the structure and character of River Road and provide general direction for housing typology, housing mix, building form and landscaping.

To ensure an attractive streetscape is delivered, architectural controls shall be developed to address detailed building design aspects such as: massing, grading differentials, elevation articulation, garage articulation, materials colour and quality, roof design as well as the proposed siting strategy. An Architectural Control process can be implemented as part of the Draft Plan of Subdivision conditions and agreement phase to be coordinated between City staff and the proponent. In addition, site plan control should apply to any development on lands along the River Road frontage in accordance with the policies of Section 6: Implementation.



### 5.7.1 Housing and Mix Diversity

A variety of residential typologies and architectural styles, elements, and material detailing should be considered to create distinctive and complementary character, as well as provide visual interest.

Universally accessible housing options should be provided to accommodate the widest spectrum of people to live in the community, regardless of age, ability and socio-economic status. Life-cycle housing options should be provided to support a variety of age groups, including houses designated especially for seniors (ex: bungalows).



Single-Detached Typologies, Markham, ON



Townhouse Typologies, Richmond, BC

### 5.7.2 Street Interface

The interface between the public realm of streets and the private realm of developments is a transitional space that should be designed to promote and enhance walking and a sense of personal safety and comfort throughout this community. The following guidance pertains to the treatment of these interfaces.

- a. Houses should be designed to frame the street edge with a consistent front setback, and have front doors, windows, verandas, and entry features facing the road.
- b. Front entry elements should be articulated through the use of framing materials, colour and built form including porches, arches or articulated front steps.
- c. Porches should be designed to be functional and usable. Porches are permitted to encroach into the front yard setback.
- d. Front porches should not be enclosed as primarily open structures, not solid walls. Solid, low fences can be used if made of local stone. House entry features should be articulated through detailing and/or a variation of materials such as porticos and/or decorative treatments around the main door including stone, mill work, sidelights, accent lighting, or house number sign.

- e. House entry features should be articulated through detailing and/or a variation of materials such as porticos and/or decorative treatments around the main door including stone, mill work, sidelights, accent lighting, or house number sign.
- f. Decorative fencing around front and/or exterior side yards adjacent to the dwelling should not block the view of the sidewalk from the house. Their height should be limited to 1.2 metres and they should offer a level of transparency as primarily open structures, not solid walls. Solid, low fences can be used if made of local stone.
- g. No two adjacent buildings should exhibit the same architectural style.
- h. Facades fronting onto the public realm should be designed using high quality and durable cladding materials. Brick, stone and siding are encouraged.



Street Interface, Collingwood, ON

### 5.7.2.1 Meadows and Lawns

Maintaining generous front setback areas in a naturalized manner is intended to ensure the area's distinctive rural character is maintained as it accommodates new development. The following are some environmentally friendly, neighbourhood character building guidelines for front lawn design and maintenance.

- a. Minimize lawn areas and substitute with naturalized, meadow like areas for wildlife.
- b. In the process of designing naturalized front lawns include visible design cues of human intention such as:
  - i. Mow the edges along paths such as sidewalks or trails as a clear cue to onlookers that the property is actively managed.
  - ii. Cluster plants to create rhythm.
  - iii. Introduce crisp edges and bold patterns as strong cues of vegetation management. One way to achieve this is to add both high (like large shrubs and trees) and low (like flowering annuals and grasses) plants.
- c. For lots fronting onto River Road a combination of trees (to provide further shade to the proposed multi use trail), as well as a combination of native vegetation and ground cover planting is encouraged.
- d. Avoid herbicide sprays. If absolutely necessary, use selective herbicides and selectively spot spray the largest and most obnoxious weeds.

- e. Use drought-resistant grasses. They require little watering and do not brown during dry spells.
- f. Cut grass to a minimum of 2" high. This reduces stress on the grass and shades bare patches to discourage weeds.
- g. Sow in a clover mix to cover 1/4 to 1/3 of the lawn area. Clover provides needed nitrogen to the lawn, adds colour and scent and out-competes weeds.
- h. Sow wildflowers into the lawn as was common in medieval times and known as "flowery medes." Low-growing flowers that co-exist with grass include English daisies, clover and chamomile.
- i. Sow small bulbs such as crocuses or scilla into the lawn for spring showing.
- j. Plant native plant species that provide continuity with the naturalized landscapes of the rural character of River Road.
- k. Locate non-native exotic plants around the residence and separated from native plants.

### 5.7.3 Garages and Driveways

The design of garages and driveways can have a major impact on the visual character of the individual dwelling and the collective streetscape. These guidelines intend to enable a cohesive streetscape where attached garages and driveways complement instead of dominating the streetscape.

#### 5.7.3.1 Front Garages

The following guidelines shall be applied in order to minimize the presence of attached and detached garage buildings accessed from the front of the lot:

- a. Vehicular driveways and garage wall for residential buildings fronting onto River Road are may be located offside streets or a rear laneway. When driveway and front-loaded garages are proposed, their presence on the landscape should be minimized thought the consolidation of driveways and the setback of garage walls.
- b. The front elevation of the house shall be designed so that its front entrance design and architectural elements reduce the visual dominance of the garage and the front driveway. Garages shall not protrude beyond the main front wall of the dwelling unit.
- c. Garages must be a natural extension of the design, massing, and material of the main dwelling.
- d. For buildings within the Village Residential designation, garages should be set behind the main building face notwithstanding the presence of a porch feature.



Extension of Design to Garage, Niagara, ON



- e. Garages should be paired to allow for more substantial front yard green space.
- f. A variety of garage door configurations and styles should be provided.
- g. Tandem garage designs are encouraged to help minimize the impact of garage width on the elevation and in turn on the streetscape.

### 5.7.3.2 Lane-Accessed Garages

Garages that are accessed from a laneway can either be detached or attached to the main dwelling at the rear. Attached garages can either be set into the house with access at the rear, or they can be attached to the main dwelling through a breezeway, which forms a side courtyard for amenity space.

- h. Where possible, garages should be paired to allow for increased rear yard, or an outdoor parking pad to accommodate resident parking.
- i. Accessory units located above detached garages are encouraged on wider corner units.



Lane-Accessed Garage, Markham, ON

### 5.7.3.3 Driveways

- a. Driveways should be designed to reduce the amount of pavement on front yards and enhance the visibility of the street.
- b. Driveway widths should generally be no larger than the interior width of the garage.
- c. Driveways off River Road should, when feasible, be paved with gravel as an appropriate rural character material. Avoid the use of asphalt paving as it represents an urban character material.
- d. The use of semi-permeable or permeable paving materials and treatment is encouraged.
- e. Driveways are encouraged to be paved with light-coloured or permeable materials to reduce storm water run off and reduce heat island effect.
- f. Driveways should be located as far as possible from parks, open space features, public walkways, and intersections.



Driveway/Rural Character, Pelham, ON

## 5.7.4 Priority Lots

Priority Lots have high public exposure and include: buildings facing and flanking River Road; buildings adjacent to parks and open space; corner lots; gateway corner lots; and T-intersection / key view terminus lots. The identification and treatment of Priority Lots should be determined through the Draft Plan Subdivision process and Site Plan Agreement. The following provides guidance on the allocation of such lots.

### 5.7.4.1 Buildings facing and flanking River Road

- a. Units facing or flanking onto River Road should be given special consideration in architectural design, massing, orientation, and siting. Materials and should be of high architectural quality.
- b. For dwellings flanking River Road, the main front door should be visible from, and oriented to, the exterior side elevation of the house with access to the sidewalk. The entries should be articulated through the use of entry features such as projecting porches facing the street.
- c. Side elevations flanking River Road should be consistent with the front elevation in terms of materials, fenestration style and detailing.
- d. Façades should be highly articulated through coordinated fenestration, masonry detailing, accent gables, dormers, and/or other special treatment.



Articulated Corner Unit, Cornell, ON

### 5.7.4.2 Buildings Adjacent to Parks and Open Space

- a. Front, side and rear elevations exposed to public spaces, should be well articulated. Articulation of buildings may also include changes in material colours and texture, changes in building plane, and variation of roof lines. The rear portions of buildings may benefit from rear yard covered porches. A combination of fenestration, bay windows, material changes and dormers may be used in addition to other design elements to achieve the objective.
- b. Side and rear elevations should have a similar design and use materials that are consistent in architectural design and material with those used on front elevations. Architectural detailing such as corbeling should continue from front to side elevations, where visible to the public.
- c. For units flanking onto public spaces, a highly articulated side face is encouraged. Architectural design and material should match the front facade's quality. Side main entrances are an alternate means to achieve this.
- d. The location of porches, windows, and entry doors for units should maximize opportunities for overview and safety.
- e. Projecting porches should emphasize the entrance as well as to reduce the presence of the garage.
- f. Driveways of adjacent homes should be located as far away as possible from public space.
- g. It is encouraged that medium to high density housing typologies are located adjacent or nearby open space so as to provide those residents with access to green space as well as increase the opportunities for social surveillance.



Articulated Facades, Port Credit, ON



Park Interface, Montreal, QC



### 5.7.4.3 Corner Units

- a. Side or rear elevations visible from the street should have windows, materials, and other architectural treatments equal in quality to the front elevation of the house.
- b. Corner windows and wrap-around porches should be included to emphasize a corner location. Where possible, the entry door should be located on the exterior side elevation of the house with direct access to the sidewalk.
- c. Townhouse units should be encouraged as bookends to residential blocks.



Corner Unit Interface, Markham, ON



Corner Unit Interface, Markham, ON

### 5.7.4.4 Gateway Corner Units

Gateway corner units are typically dwellings that are located at the entry to the community from adjacent areas. These units should be designed with the following principles in mind:

- a. Gateway dwellings should be given special consideration in architectural design, massing, orientation, siting and materials, and shall be of high architectural quality.
- b. Entry elements and porches are encouraged to produce interest in the façade as well as to help define the entrance to the neighbourhood.
- c. Pairing of similar model dwellings on lots directly opposite each other is encouraged to establish and enhance a gateway condition.
- d. It is encouraged that landscaping and landscape features are provided as a means to accentuate the unit and create an inviting atmosphere.



#### 5.7.4.5 T-Intersections/Key View Terminus

T intersections occur when one road terminates at right angles to another. Consideration should be given to homes at the top of the T intersection and the last two lots on either side of the road that terminates at the intersection.

- a. Architecture on lots at the end of T intersections should have facade designs that utilize elements such as coordinated fenestration, masonry detail, and entry elements.
- b. Pairing of side yards is encouraged to form a landscaped area at the terminus of the T intersection.
- c. Buildings sited at the end of the view corridor should be designed with architectural elements that address these views.



View Terminus of T-Intersection, Niagara, ON

#### 5.7.4.6 Potential Character Contributing Buildings

The following existing or potential cultural heritage resources are found within or adjacent to the study area (as per the Section 4.2.2):

- a. Two properties (CHR1 and CHR10) are designated under Part IV of the Ontario Heritage Act
- b. One Cultural Heritage Landscape (CHL1) identified by the City of Cambridge is adjacent to the study area.
- c. One Canadian Heritage River (CHR12) is adjacent to the study area.
- d. Fourteen properties (CHR2 to CHR10, and CHR13 to CHR 17) are potential cultural heritage resources that merit evaluation under Ontario Regulation 9/06 to determine whether they contain cultural heritage value.

Development of existing and potential cultural heritage resources should:

- e. Promote the conservation of existing cultural heritage resources and consider the presence of the potential cultural heritage resources identified.
- f. Any proposed development on or adjacent to an identified existing or potential cultural heritage resources should require a heritage impact assessment to further assess the cultural heritage value of the identified potential cultural heritage resources under Ontario Regulation 9/06 and the City of Cambridge's Cultural Heritage Value Evaluation Criteria, and to ensure that the existing cultural heritage resources in the study area are conserved.

## 5.8 Green Infrastructure and Building

The following green infrastructure and building best practices are a compilation of recommended sustainable strategies to strengthen the design of River Road's new community.

### 5.8.1 Energy Efficiency

- a. All new developments are encouraged to be built adhering to the following requirements for sustainability:
  - i. Grade related (3 storeys) residential buildings achieve a minimum performance level that is equal to ENERGY Star U or equivalent standards.
  - ii. Residential buildings to be designed with a "net-zero" ready strategy.
- c. Energy related studies (e.g. district energy studies, etc.) should be considered at the development stage to understand how energy efficiency may be built into a proposed development.
- d. Where feasible, alternative community energy systems such as geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy should be provided.
- e. Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect. Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect.
- f. To minimize energy consumption and encourage the integration of passive building systems, buildings should be oriented to maximize the potential for sunlight, solar generation and natural ventilation.
- g. The strategic use of deciduous trees is strongly encouraged as part of a free cooling strategy to help with evapotranspiration and shading.
- h. Building design is encouraged to utilize opportunities associated with large expanses of roof areas to implement solar thermal, photo-voltaic systems, green roofs or water harvesting systems.
- i. Energy Star compliant or equivalent appliances are encouraged to be provided in cases where the developer provides appliances.
- j. Alternative energy sources such as solar thermal, photo voltaic panels and/or geothermal technologies or the inclusion of "roughed in" facilities to accommodate such alternative energy sources are encouraged to be provided.

## 5.8.2 Water Efficiency and Stormwater Management

- a. All new buildings are encouraged to achieve a 20% or greater water conservation efficiency than that required by the Ontario Building Code.
- b. Irrigation of all public open spaces/ structures should implement a rainwater harvesting program, and can include the use of rain barrels, and/or infiltration trenches provided water balance objectives are met.
- c. Grade-related residential unit driveways should be paved with permeable materials to reduce stormwater run off where supported by agencies.
- d. At site-plan level, developments with five or more dwellings should be encouraged to increase the level of perviousness in order to promote at-source stormwater management, reduce peak flows and lessen the dependence on end-of-pipe facilities such as stormwater management ponds. Pervious areas can include landscaped areas and/or areas containing permeable paving. In order to reduce the volume of run-off into the storm drainage system, surface water run-off flows should be directed to landscaped areas and the use of hard surfaces should be minimized.
- e. Innovative sustainable technologies in the capture, conveyance, and treatment of storm run-off to reduce potential pollutants/ contaminants are encouraged.
- f. New residential units should be designed to incorporate gray water pipe infrastructure, where permitted, in accordance with the Ontario Building Code and health regulations.
- g. Indoor water usage should be minimized in new buildings through the installation and use of water-efficient fixtures, fittings and appliances, such as dual-flush toilets, faucets, and shower heads.
- h. Low maintenance and drought-resistant landscaping is encouraged. To conserve and protect the area's natural heritage features, stormwater runoff from all new developments shall be collected and treated to an Enhanced standard (as per Ministry of Environment, Conservation and Parks guidelines).
- i. In order to avoid the negative impacts of flooding in the downstream watershed, all new development post development peak stormwater flows shall be attenuated to pre-development conditions for up to and including the 100 year return period design storm event.
- j. All downspouts from residential development shall discharge to grade.
- k. Adequate and safe minor and major storm conveyance systems shall be required for all new development.



Stormwater Planter, Portland, OR



Green Street, Portland, OR

- l. Low Impact Development (LID) methods will be strongly encouraged to be utilized in this new community in conjunction with traditional stormwater management techniques.
- m. The target is to capture, detain, utilize, or infiltrate all stormwater runoff from all storms up to the 25mm design storm event where soil conditions permit.
- n. Increasing the size and capacity of the major and minor storm systems, and the associated stormwater management systems and facilities, should be considered in order to increase the community's level of resilience against the impacts of climate change (i.e. severe storm events).



### 5.8.3 Material Resources and Solid Waste

- a. A minimum of 25% recycled/reclaimed materials is encouraged to be used for new infrastructure including roadways, parking lots, sidewalks, unit paving, stormwater management facilities, sanitary sewers, and/or water pipes.
- b. All buildings should utilize best practices for design and construction techniques in order to reduce the amount of construction waste produced.
- c. Where feasible, streetscape and building materials should be chosen and determined by suitability, durability, ease of maintenance, and cost effectiveness.
- d. Green building materials should be used to reduce impacts on the environment.
- e. Building materials should be purchased and/or obtained from responsible, ethical, and whenever possible, local sources.
- f. The use of recycled and reclaimed material for new buildings is encouraged in order to reduce the negative environmental effects of extracting and processing materials.
- g. Waste volumes should be reduced through the provision of recycling and/or re-use and compost stations as well as drop off points for potentially hazardous waste.

### 5.8.4 Lighting

- a. Lighting should be downcast to reduce light pollution and address night sky condition.
- b. Exterior lighting spill-over onto the street or adjacent properties should be minimized.
- c. Renewable energy sources should be considered for electric energy supply in the public realm (i.e. lighting for natural trails and park pathways).
- d. High efficiency street lighting should be used so as to reduce energy consumption by at least 15% of the baseline annual energy use.
- e. For residential buildings, external lighting should incorporate lighting controls that use motion sensors and/or timers to improve energy efficiency.

### 5.8.5 Sustainable Program

Sustainable programs should encourage opportunities for community-based initiatives such as car pooling, community composting, waste reduction, educational, and stewardship programs. Some examples of such programs are as follows:

#### a. Education Packages

- i. Owner/tenant education packages regarding household activities to conserve household energy and water resources, access to transit, recycling and composting programs, and depots should be provided at the time of purchase or rental.
- ii. Encourage homeownership afford-ability for low-income families through the provision of sites for non-profit or charitable Homeownership Programs.

#### b. Mobility Programs

- i. Carpooling or car share programs are encouraged for residents of multi dwelling residential buildings. Dedicated parking spaces for carpooling or car share programs should be provided with clear signage.

The City of Cambridge's Climate Adaptation Plan (2019) outlines a series of action items and organizational implementation items needed in place to facilitate the delivery of Sustainable practices.

- a. A Sustainability Plan may be required as part of a development application. A Sustainability Plan would indicate how a development integrates social, economic and environmental sustainability into the site and built form design.

### 5.8.6 Innovative Design

In order to encourage exemplary performance above the requirements set out in this document, innovative design is strongly encouraged.

Innovative elements and performance should identify the intent of the proposed innovation, while demonstrating and describing the design approach and strategies utilized to achieve sustainable performance measures that exceed those set out in this document.

## 5.9 Servicing Strategy

### 5.9.1 Water and Sanitary Servicing

All development in the River Road Secondary Plan area shall occur on full municipal water and wastewater services. As part of the implementation of this Secondary Plan, the City will work with the Region of Waterloo to ensure that there is adequate water and sanitary servicing capacity to accommodate the long term planned development for the River Road Secondary Plan Area. The River Road Neighbourhood Plan Functional Servicing Assessment (June 2021) completed in conjunction with this Secondary Plan has provided a long-term servicing strategy for providing water servicing and gravity sanitary sewer outlets for developments within the study area.

### 5.9.2 Municipal Servicing

As part of the implementation of this Secondary Plan, and the policies of Chapter 6 of the Official Plan, the City will undertake an analysis of local infrastructure to ensure that adequate servicing is in place to accommodate the planned growth for the area. The River Road Neighbourhood Plan Functional Servicing Assessment (June 2021) has provided both interim and long-term servicing strategies for the study area.

### 5.9.3 Development Applications and Servicing Requirements

The City may also require development applications to be supported by site-specific servicing studies.

### 5.9.4 Sustainable Stormwater Management

The municipality encourages innovative measures to help reduce the impacts of urban run-off and maintain base groundwater flow. Such measures may include bioswales, permeable pavers, rain barrels, green roofs and green buildings.

### 5.9.5 Coordination of Public Work

The City will work with the Region to ensure that planned public works for the area are coordinated to minimize the impacts of construction on the residents within the Secondary Plan Area. Coordination efforts will consider the phasing of streetscape improvements, any future road works and maintenance, as well as any upgrades to water and sanitary networks.

# 06

## Implementation





## 6.1 General Implementation

The River Road Secondary Plan shall be implemented through a variety of tools, including but not limited to:

- a. The planning and development application process, through tools such as site plan approval, plans of subdivision and condominium, and land severances;
- b. The City shall amend the site plan control by-law to include new development on properties fronting River Road;
- c. The City shall acquire drainage easements over existing stormwater outlets on to the Speed River;
- d. The City of Cambridge Transportation Master Plan;
- e. The City of Cambridge Zoning By-law;
- f. The City's Parks Master Plan (to be completed);
- g. The City's Cycling Master Plan; and,
- h. Other tools as described in this Chapter.

### 6.1.1 Municipal Works within the Secondary Plan

All future municipal works undertaken by the City of Cambridge within the Secondary Plan Area shall be consistent with the policies of this Plan and the Region of Waterloo's Official Plan. Official Plan Amendments

Unless otherwise stated in this Plan or the City's Official Plan, applications for development which do not align with the Policies or Maps of this Plan shall require an Official Plan Amendment. Amendments to the Official Plan shall be subject to policies of the Official Plan and shall require a planning justification report, along with any other supporting studies identified through the pre-consultation process.

### 6.1.2 Zoning By-Law

#### 6.1.2.1 Alignment with Zoning By-Law

The City will update its zoning by-law to ensure that the land use and design policies for this Secondary Plan are reflected in the City's zoning by-law.

#### 6.1.2.2 Amendments to Zoning By-Law

Applications for development within the Secondary Plan Area shall be subject to the policies of this Secondary Plan and the City's Official Plan (where applicable). Amendments to the zoning by-law shall be subject to policies of the Official Plan/Secondary Plan and shall require a planning justification report, along with any other supporting studies identified through the pre-consultation process.

### 6.1.3 Site Plan Review, Plans of Subdivision, Plans of Condominium and Severances

Applications for site plan review, plans of subdivision, plans of condominium and land severances to sever shall be consistent or conform with Provincial policies, and the policies of this Secondary Plan, City of Cambridge's Official Plan and the Regional Official Plan. All forms of development fronting on River Road shall require site plan approval and the City shall amend the existing site plan control by-law.

### 6.1.4 Supporting Studies

Prior to site plan approval, the completion and implementation of various studies may be required as conditions of a site plan or subdivision agreement depending on the location and nature of the development proposal. Studies may include but are not limited to: Scoped Environmental Impact Statements; Heritage Impact Assessments; Tree Management Plans; Hydro-geological Studies; Chloride Impact Studies and Salt Management Plans; Spill Prevention, Response and Contingency Plans; Stormwater Management Reports; Traffic Impact Studies; Functional Servicing; and/or Stationary Noise Studies. Any required on-site implementation measures identified in the study will be secured as part of a site plan agreement with the City.

### 6.1.5 Coordination with the Region of Waterloo

The City will work with the Region of Waterloo, who is the approval authority for this Plan, to ensure that the policies of this Plan are implemented, including any opportunities to implement the urban design and public realm improvements through any future Regional works. The City also encourages the Region of Waterloo to consider opportunities for affordable housing development within the Secondary Plan area.

## 6.2 Phasing and Financial Tools

### 6.2.1 Municipal Capital Improvements

The City will prepare a phasing strategy to assist with the implementation of this Secondary Plan and more particularly to any streetscape and/or servicing improvements to River Road. The Phasing Strategy should consider the following:

- a. The expecting timing of development, including the expected built-out of vacant lands and redevelopment of existing areas;
  - b. The timing and funding approach for River Road's infrastructure and public realm improvements; and,
  - c. Any other projects or initiatives which may impact the timing of development.
- ### 6.2.2 Development Charges

The City will include any growth-related infrastructure identified in this plan as part of the next Development Charges By-law update in accordance with the City's local service policy.

### 6.2.3 Parkland Dedication

The new park as noted in the maps, will be acquired through parkland dedication, purchase of lands, cash-in-lieu of parkland dedication, and/or expropriation if necessary. A minimum park size of 1 ha is generally pursued but given constraints in the area, and the size and location of the preferred park site, the City will accept an approximately 0.7 ha park. Cash-in-lieu of parkland dedication will be required from all other Secondary Plan subject sites as they develop.

### 6.2.4 Monitoring of Program

The City will prepare a monitoring program to track the implementation of the Secondary Plan and report on the progress of its implementation. The monitoring program should identify development statistics for residential intensification and the status of the various actions identified in this Plan (such as the upgrades to River Road).

The Secondary Plan monitoring should be undertaken in conjunction with other monitoring activities described in the Secondary Plan chapter of the Official Plan.

### **6.2.5 Updating the Plan**

The City will comprehensively review the policies of this Secondary Plan at the 10-year review of the City's Official Plan. Depending on the outcomes of the review, the City may decide to update the Plan

### **6.2.6 Conflicts with Official Plan**

In the event of a conflict between the City of Cambridge's Official Plan and this Secondary Plan, the policies of the Secondary Plan shall prevail.

### **6.2.7 Boundaries**

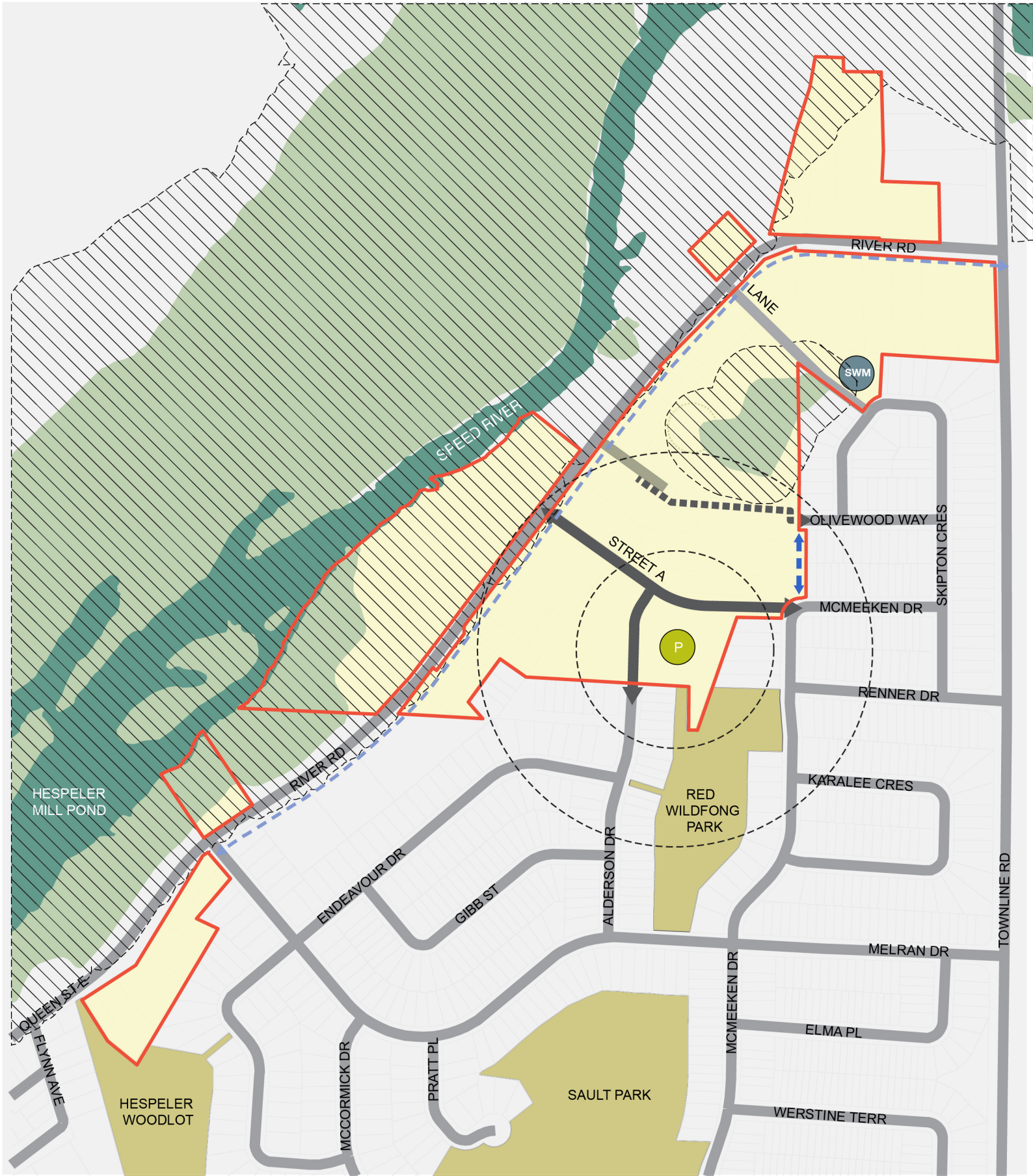
The boundaries shown on the Secondary Plan Maps are approximate, except where they meet with existing roads, river valleys or other clearly defined physical features. Where the general intent of this Secondary Plan is maintained to the satisfaction of the Municipality, minor boundary adjustments will not require an amendment to this Secondary Plan.

# SCH

## Schedules







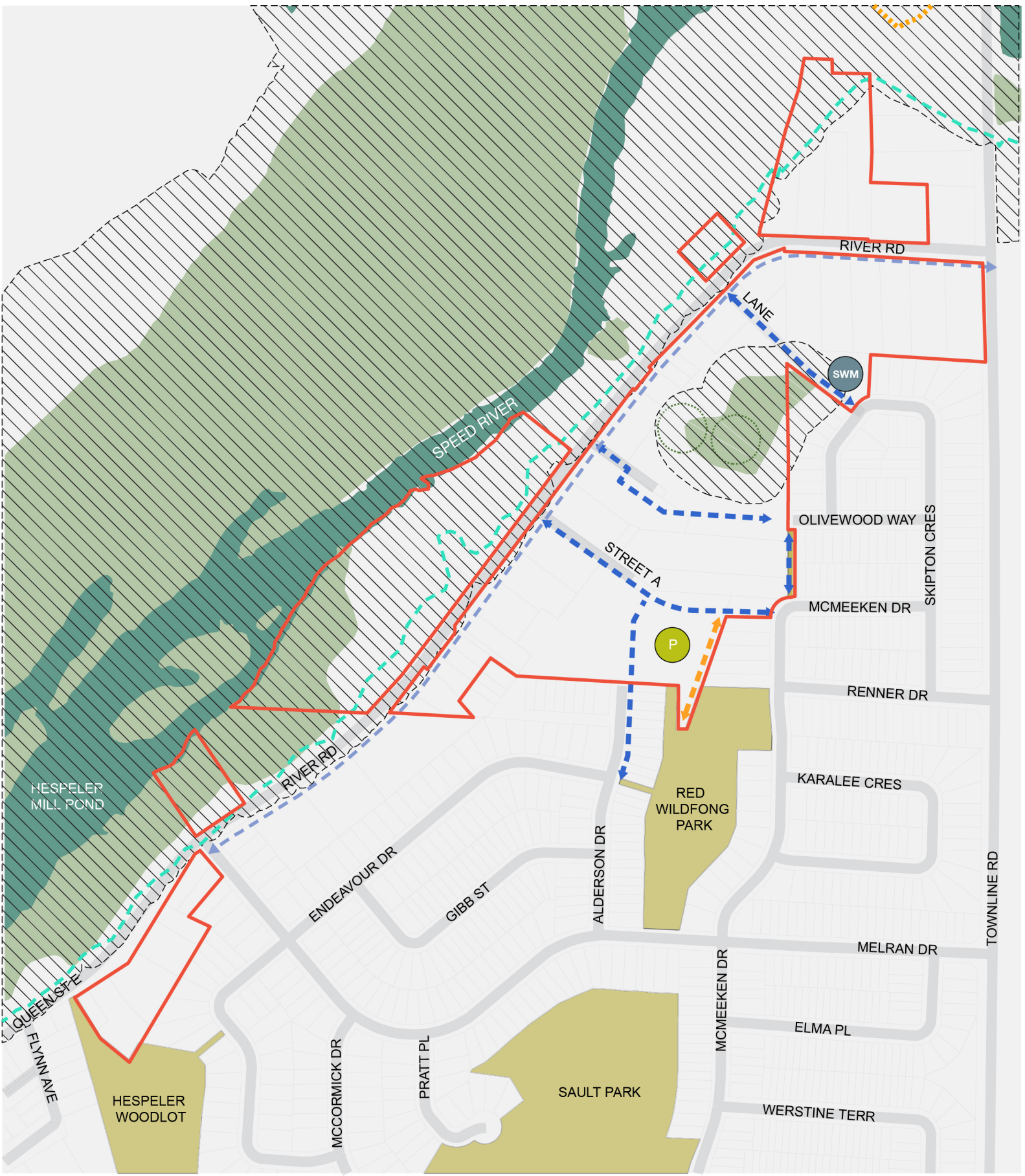
## Schedule 1 Neighbourhood Structure



- Residential Neighbourhoods
- Existing Natural Open Space System
- Existing Recreational, Parks/Open Space
- Proposed Open Space/Park
- Stormwater Management
- Proposed Multi-Use Trail Network

- Existing Roads
- Proposed Local Municipal Road
- Proposed Private Road
- Pedestrian Connection
- Walking Radius (200-400m)
- River Road Secondary Plan Boundary
- Special Policy Area



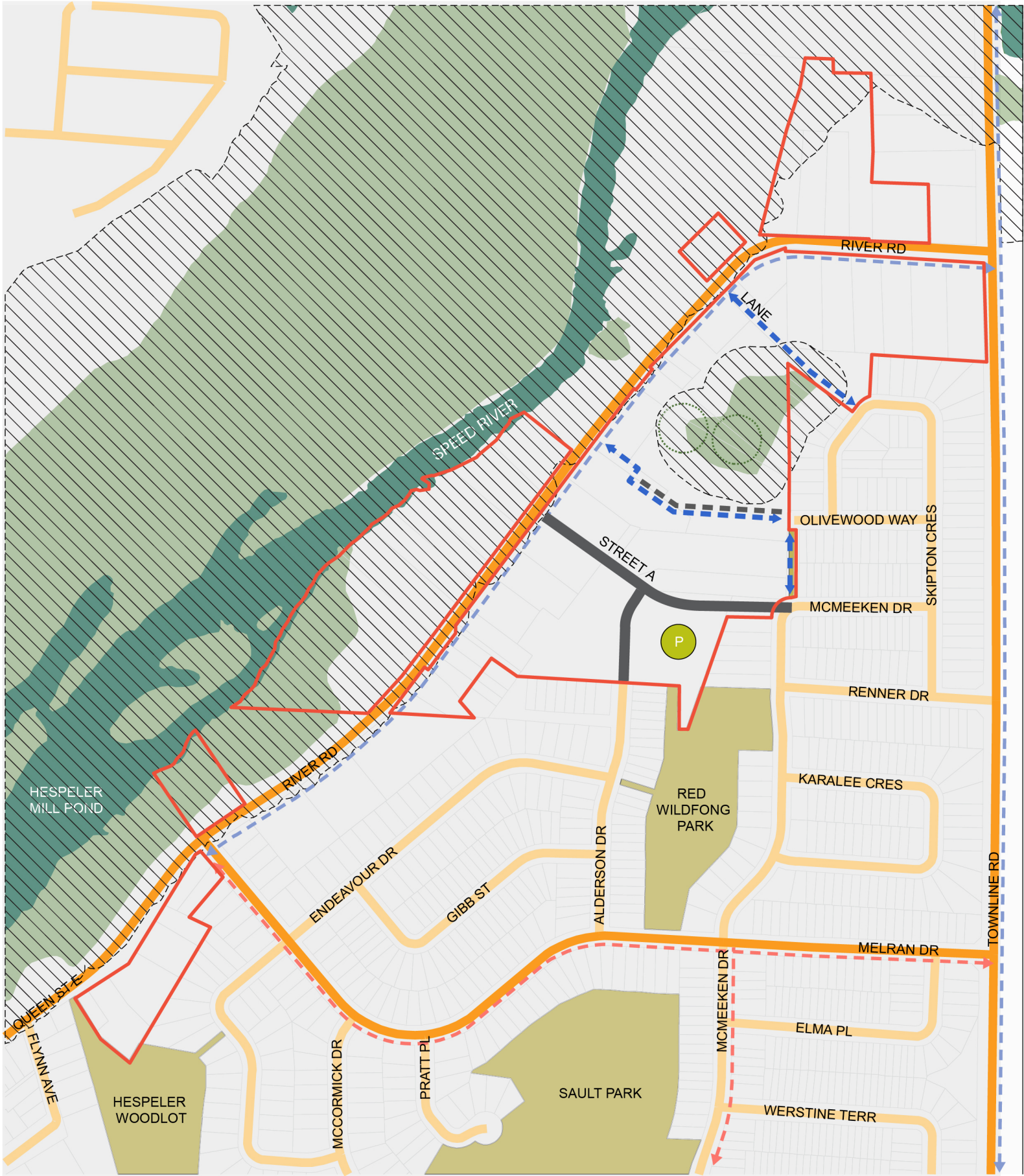


# **Schedule 2** **Open Space** **Structure**



- Regulated Wetland/Environmental Feature(s)
- Regulatory Storm Floodplain (GRCA)
- Regulated Watercourse (GRCA)
- Special Policy Area Overlay
- Butternut Tree Buffer (25.0m buffer)
- Proposed Multi-Use Trail Network
- Existing Open Space/Parkette
- Proposed Open Space/Park
- Pedestrian Connection
- Trail Connection
- Stormwater Management
- River Road Secondary Plan Boundary





### Schedule 3

#### Street/ Active-Transportation Structure

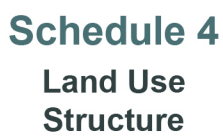


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- Collector Road
- Local Road
- Proposed Local Municipal Road
- Proposed Private Road
- Proposed Bikeway Network
- Proposed Open Space/Park

- Proposed Multi-Use Trail Network
- Proposed Pedestrian Connections
- Special Policy Area
- Butternut Tree Buffer (25.0m buffer)
- Regulated Wetland/Environmental Feature
- River Road Secondary Plan Boundary





-  Proposed Local Municipal Road
-  Proposed Private Road
-  Proposed Open Space/Park
-  Pedestrian Connections
-  Stormwater Management
-  River Road Secondary Plan Boundary
-  Proposed Multi-Use Trail Network

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## Attachment No. 11 – Conceptual Demonstration Plan





Attachment No. 12 – Potential Cultural Heritage Landscape Study Boundary

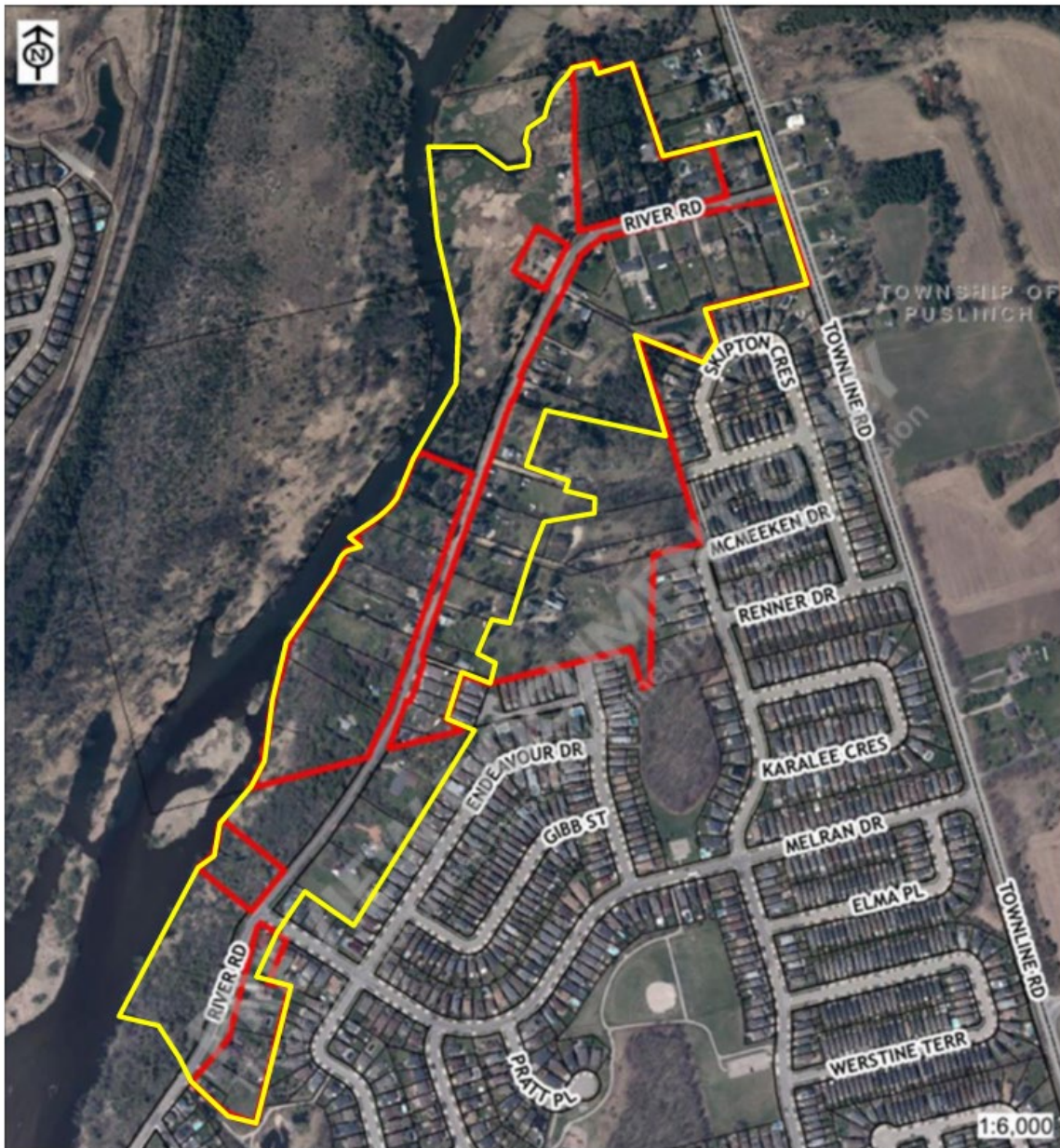


Figure 18: Proposed River Road Cultural Heritage Landscape Boundary. The proposed boundary is identified in yellow (ASI :)