

To: **COUNCIL**

Meeting Date: **01/18/22**

Subject: **Milling Road Streetscape Plan and Urban Design Guideline Development and Consultation**

Submitted By: **Kevin De Leebeeck, Director of Engineering**

Prepared By: **Matt Holland, Project Engineer**

Report No.: **22-010 (CD)**

File No.: **A/01030-20**

Recommendations

THAT Report 22-010 (CD) - Milling Road Streetscape Plan and Urban Design Guideline Development and Consultation - be received;

AND THAT the Milling Road Streetscape and Urban Design Guidelines contained in Appendix B of which outlines the preferred streetscape concept (Concept 2 - Phase 2A) be approved;

AND FURTHER THAT \$40,000 in additional funding from the Economic Development Reserve Fund be transferred to existing project A/01030-20 Milling Road Conceptual Development to carry out Phase 1 and Phase 2 Environmental Site Assessments for the City owned 30 Milling Road property.

Executive Summary

Purpose

- The purpose of this report is to provide Council an update on the placemaking project A/01030-20 Milling Road Streetscape Plan and Urban Design Guideline Development and Consultation and request approval of the guideline's preferred concept for future design and construction.

Key Findings

- Development, revitalization and placemaking opportunities have been identified for the Milling Road and Jacob's Landing area in Hespeler.

- The 2013 Hespeler Village Streetscape Plan and Urban Design Guidelines outlined some basic features for Milling Road to be considered in a further detailed study (Appendix A).
- The City retained GSP Group to perform a detailed study, including consultation, of the area to create a Streetscape Plan and Urban Design Guidelines (SUDG) document (Appendix B).
- Consultations included three stakeholder meetings and over 300 combined comments received between the initial public feedback questionnaire and Public Information Centre (PIC).
- After initial consultations were completed, two draft Streetscape Concepts were developed for consideration in conjunction with the guidelines. The two concepts were then presented for further public consultation at a PIC and one preferred concept is being recommended for Council endorsement.
- Next steps include commencing detailed streetscape design and subsequent construction of the preferred concept in a future budget.

Financial Implications

- A high-level preliminary cost estimate for the detailed design and supporting work of the preferred concept is \$290,700, which is intended to be included as part of a future budget cycle.
- A high-level preliminary cost estimate for the construction and implementation of the detailed streetscape design is \$2,900,000. Note that this cost is a pre-design estimate, and is subject to further scope review and refinement during detailed design. This includes streetscaping and park improvements only, and does not include improvements or repurposing of the 30 Milling Road city owned building.
- A building condition assessment for 30 Milling Road was recently completed by the City's Sustainable Design and Development Group and noted that required repairs are in the order of \$800,000 if Council wishes to keep the building operational into the future.
- A high-level cost estimate for a complete renovation of the 30 Milling Road building is approximately \$2,500,000. This would include the repairs noted above, in addition to an expanded renovation to allow improved building function for more commercial lease type use. Note that this renovation cost is a pre-design estimate prior to the completion of a Phase 1 and Phase 2 Environmental Site Assessment, and further design review would be warranted to refine this high-level preliminary cost estimate prior to initiating detailed design. Phase 1 and Phase 2 Environmental Site Assessment costs for this building and site are

estimated to be \$40,000 and proposed to be funded through the Economic Development Reserve Fund. The building's renovation cost is also subject to final end use needs for the building.

Background

The October 2013 Hespeler Village Streetscape Plan and Urban Design Guidelines identified the Milling Road Industrial lands as an area of opportunity for future development. The 2013 document outlined two options that involved very basic streetscape improvements on city owned land. Prior to detailed design, staff initiated a specific study with public consultation to build upon those initial guidelines.

The study area included Milling Road, Jacob's Landing on the west side of Guelph Avenue, and three properties which front on to Milling Road/Guelph Avenue as follows: 54 Guelph Avenue (privately owned), 25 Milling Road (privately owned), and 30 Milling Road (City owned). This area is considered to have a high degree of potential for revitalization and future as a Woonerf (living street).

There is a rich history in the study area including the Hespeler Furniture Factory building established in 1901. The area provides unique opportunities for placemaking in Hespeler Village with a people-centred approach to planning and design. The Milling Road and Jacob's Landing area could be developed into a pedestrian friendly street and provide an inclusive environment for people to gather and explore while enjoying one of Cambridge's historic areas.

The City completed a Request for Proposal procurement process and awarded the project to GSP Group. Additionally, the project team worked closely with internal staff, external agencies (i.e. GRCA), and public stakeholders to undertake this study.

The project included the development and evaluation of various streetscape design options to support a design guideline document. Eventually, two options were developed and each option included a high level cost estimate (class D). Several constraints were explored including private properties, GRCA approvals, roadway reconfiguration, zoning, and parking. These options were also developed and refined through the consultation sessions and public engagement. A final Streetscape Plan and Urban Design Guidelines document with the preferred concept has been submitted by GSP Group.

Analysis

Strategic Alignment

PLACE: To take care of, celebrate and share the great features in Cambridge that we love and mean the most to us.

Goal #3 - Arts, Culture, Heritage and Architecture

Objective 3.2 Conserve and make positive contributions to our heritage districts and buildings throughout the community.

Milling Road and Jacob's Landing are ideally located on the Speed River with proximity to residential, commercial, and industrial properties. The local architecture and historic significance provide a compelling story for visitors and a unique opportunity to create an alluring sense of place to celebrate unique features in Cambridge.

Comments

GSP Group has submitted the final Streetscape Plan and Urban Design Guidelines document for the Milling Road study area (Appendix B). The document includes sections on background information, inventory analysis, planning policy review, public engagement, built form, streetscape design, park design, phasing, costing, and future considerations.

Two concepts were developed for comment and consideration during the consultation process for the study area. Each concept has a two phased approach to allow for flexibility with implementation over a period of time. Phase One of each concept is for street redevelopment only, through a streetscaping project. Phase Two of each concept includes two options, "A" or "B". Phase 2A is for park development with the City building remaining. Phase 2B is for full park development with the complete removal of the City building. After consultation and public engagement, it was determined that Concept 2 Phase 2A would be the preferred concept for implementation.

Concept 1 (Alternative Concept)

- Straightened streetscape alignment with all parking on the north side. Refer to Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix A) for streetscape plans.
- The streetscape is curbed with a continuous row of trees along both the north and south sides of the road.
- Planting in raised planters beneath the trees softens the transition between Milling Road and Jacob's Landing.
- The proposed sidewalks are wider than the existing sidewalk to improve pedestrian circulation through the site.
- The sidewalk added to the south side aligns with the Mill Run Trail connection to the east.

Concept 2 (Preferred Concept)

- Curving streetscape that provides traffic calming measures with sufficient space for larger vehicles to manoeuvre the road. Refer to Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix A) for streetscape plans.
- The streetscape is curbless to provide accessible north-south access for flexible programming and events.
- The parking is split between the north and south sides to best serve the businesses and park visitors.
- A wide sidewalk on the south side has been aligned with the Mill Run Trail connection to the east, and provides enough space for a double row of trees.

Opportunities & Challenges

- **Private Property:** Private property owners are currently satisfied with their current property usage. These lands are currently zoned Industrial (M3) with the new proposed zoning by-law changing to Core Mixed Use One (CMU1). The CMU1 Zone provides future opportunities and broadens the range of permitted uses from the current M3 Zone, including a mixture of different commercial uses and residential uses all subject to floodplain provisions and requirements.
- **Pedestrian Only Street:** Both concepts have the potential to become pedestrian only with some further developments (i.e. no vehicular traffic on Milling Rd). Deliveries and traffic would need to be managed by a newly developed rear access road behind 54 Guelph Avenue. Refer to Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix A) which provides a rendering of a potential future rear access to the 25 Milling Road and 54 Guelph Avenue properties. This would require land acquisition, construction, and most importantly support from private property owners. Future budget forecasts do not include this scope of work at this time, due to limited short term interest from private property owners.
- **Mill Run Trail:** A potential connection through Milling Road for the Mill Run Trail that runs along the shores of the Speed River was explored. This trail connection would need to run through private property and across CN Rail tracks creating a number of challenges. Ultimately a trail connection was opposed by affected property owners at this time and is not included as a part of the preferred concept.
- **Parking:** Milling Road currently has approximately 20 on-street parking spots split between both sides of the road. The stalls are not formally marked with paint and the layout does not follow the latest City standard. Both proposed concepts

provide parking on a single side to match current standards and was supported by the majority of survey respondents. Future land use changes may create parking challenges for Milling Road.

- **GRCA:** The study area is within the Grand River Conservation Authority's regulated storm floodplain for the Speed River. Development within floodplain zones is restricted or prohibited, while recreation and conservation activities are permitted subject to approval by the GRCA. These restrictions may create challenges on future development opportunities.
- **Speed River:** The study area's proximity to the Speed River presents both opportunities and challenges. The beautiful scenery of the river attracts residents and tourists to visit, making the area a hotspot destination in Hespeler. The proximity also warrants careful environmental consideration and Low Impact Development practices will need to be considered further during detailed design.

City Building (30 Milling Road)

- **Overview:** The 30 Milling Road City owned building is approximately 3,400 square feet. The building includes a workshop and garage, a lunch room, an office, washroom facilities and storage rooms. It is currently used as a storage facility for the Advanced Metering Infrastructure Implementation (Smart Meters) project and following the completion of the Smart Meter project would no longer be needed as a storage facility for this project. There was strong stakeholder interest to keep the City's building facility at 30 Milling Road, although it requires substantial renovations. Several building condition assessments have been completed and note architectural, mechanical, and electrical components of the building are all in poor condition. Although the building does not pose an immediate risk, major renovations will be required to allow long-term use of the building.
- If the building remains in City ownership there are future opportunities for commercial operations such as food & beverage, an ice cream shop, or kayak rental depot with the current zoning. However, if the City decided to sell the building to a private developer then a private owner would be restricted in their development opportunities due to the current zoning restrictions. A private owner would likely require an Official Plan and Zoning By-Law Amendment in order to allow commercial operations. The GRCA floodplain policies would apply to both public and private owners of the property. If the City building is demolished it is highly unlikely that a new building would be permitted to be built.
- The most recent building condition report completed in 2021 (Appendix C: Completed by Read Jones Christofferson Engineers) notes that the building is currently in fair to poor condition. The primary concern noted is regarding the

perimeter load bearing masonry walls and foundation walls. This report indicates that there is at least \$800,000 of repairs that would need to be completed to keep the building operational into the future for it's current use.

- The long term future of the building is to be determined. A class D estimate has been prepared by the City's Sustainable Design and Development Group. This estimate approximated the total cost of work involved to get the building into a commercially viable state at \$2.5 million plus or minus 25 percent. This is similar to the building renovation estimate provided by the consultant, GSP Group. There is also the option for demolition at an estimated cost of \$75,000.
- In order for Council to make an informed decision about the future of this building staff are recommending that a Phase 1 and Phase 2 Environmental Site Assessment be completed for this property as the next step in the process to gain more certainty on the preliminary building renewal cost estimate. The estimated cost of this assessment is \$40,000 based on budgetary pricing received. If the results of the assessment conclude that large scale environmental remedial works are required in addition to building repairs, then the costs of building renewal will likely be higher than the estimate provided above.

Next Steps

- Council endorsement of the preferred streetscape conceptual plan
- Phase 1 and Phase 2 Environmental Site Assessment to be completed in 2022 to inform final Council direction for the 30 Milling Road building.
- A detailed streetscaping design assignment for the study area will be included as part of the City's future capital budget process.

Existing Policy/By-Law

The study area is subject to Cambridge Zoning Bylaw No. 150-85. A new comprehensive zoning by-law is currently under development that will replace the existing By-law No.150-85. The proposed by-law updates will change Jacob's Landing Park and 30 Milling Road building to a Floodway (F) Zone and the private properties portion of the study area to a Core Mixed Use One (CMU1) Zone.

Financial Impact

There is minimal cost difference between the two concepts. The biggest factor in terms of cost will be the decision on what will be done with the City's building at 30 Milling Road as this will influence whether or not full park development proceeds. All costs associated with the restoration of the building at 30 Milling Road have not been included

within the phased options of **Table 1**. All building costs are included in the Building Renovation section of **Table 1**.

A separate project is required to accurately estimate the total costs associated with building restoration and renovations. The project should consider the adaptive reuse recommendations provided by stakeholders and survey respondents in Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix C & D). Revenue may be generated through the future usage (rent, fees, etc.) of the 30 Milling Road building. There are no revenue estimates at this time and a business case study would be recommended prior to or during any future pre-design assignments for the building.

The summary found in **Table 1** includes the approximate financial impacts with HST of the streetscape concepts. Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix B) contains the full breakdown of the cost estimates for both concepts. **Note that Phase 2A and 2B do not include the costs of Phase 1.**

Table 1: High-level Cost Estimate Breakdown

Phase	Concept 1 Alternative	Concept 2 Preferred
Phase 1 – Streetscape Only (No park or building renovation)	\$1,790,000	\$1,890,000
Phase 2A – Jacob’s Landing Park Renovation (No building renovation or demolition)	\$550,000	\$550,000
Phase 2B – Full Park Development (Building demolition included)	\$960,000	\$960,000
Building Renovation* (Additional cost to phases above)	\$2,500,000	\$2,500,000

* The building renovation estimate is subject to change depending on a number of factors including site contamination and regulation restrictions. A further pre-design assessment is required to further refine the building renovation costs.

Public Input

- The project included three (3) stakeholder consultation sessions, one initial public feedback survey, and one Public Information Centre. The intent of these sessions was to have discussions with stakeholders on the potential for revitalization for this study area. A full detailed summary of the results can be

found in Appendix B (Milling Road Streetscape and Urban Design Guidelines (2021) - Appendix C & D).

- The first stakeholder consultation session occurred with the private property owners of 25 Milling Road and 54 Guelph Avenue. Milling Road provides access to these private properties only and as such they are key stakeholders in any development. The second and third stakeholder sessions occurred with the Hespeler Village BIA, Ward 1 and Ward 2 Councillors, and local business owners. There were additional informal conversations with stakeholders during the process. It was clear from all stakeholder meetings that there is an expressed interest in preserving the City owned building for its historic value and economic opportunities to local businesses in Hespeler Village.
- An initial public feedback opportunity was set up through Engage Cambridge to gain feedback prior to finalizing the streetscape concepts. The initial feedback survey was open for 6 weeks in Spring of 2021. The public was informed through e-mails, letter notices, social media, and a newspaper advertisement. A significant response was received with 278 respondents.
- Staff presented the preferred concept and guidelines to the public during a virtual Public Information Centre on July 7, 2021. At this time an Engage Cambridge survey was also open for 3 weeks of feedback. The public was informed of the PIC and feedback survey through social media posts, e-mails, letter notices, and a newspaper advertisement. The PIC and Engage Cambridge posting included the draft guidelines document and streetscape concepts for comment. Staff also gathered feedback from individuals who reached out via email instead of the survey.

Summary of Consultation Results

- **City Building:** Through consultation with stakeholders it was clear that the City building at 30 Milling Road is a priority, and stakeholders would like to see the building preserved and restored for adaptive reuse. The BIA and Ward Councillors expressed interest in preserving the buildings for their historic value and economic opportunities to local businesses in Hespeler Village. During the PIC survey, 44% of respondents believe that significant efforts should be made to preserve the building, however the remaining 56% of respondents preferred removal for a larger park and more amenities. Zero respondents chose to keep the building at all costs. Note that this data is from 25 respondents.
- **Activities:** The most common current use for the study area is walking (with and without dogs), followed by: fishing, enjoying the views, relaxation, small gatherings, canoe/kayak launch, photography and access to Mill Run Trail.

Leisurely visits were the number one reason for future visits with almost 80% of respondents selecting this answer.

- **Commercial Opportunities:** During the initial public survey 74% of respondents would like to see commercial activity in the area, including a strong interest in a farmer's market and more shopping opportunities.
- **Parking:** During the initial survey respondents were asked if they would like Milling Road to have on-street parking, and 50% selected single-sided parking, 32% selected no on-street parking and 18% selected parking along both sides of the road.
- **Road Design:** The response for a curbed versus curbless road was evenly split with a preference of 52% for a curbless road. A similarly even split was found for the road geometry of curved versus straight with a preference of 52% for the straight configuration.
- **Environment:** During the initial feedback period 95% of respondents want to see sustainability measures put in place such as stormwater management, selective planting, and conscientious material choices. In addition, 93% of respondents want to ensure that trees be included as part of the streetscape design.
- **Heritage & Art:** The most common theme from all engagement was the preservation of the heritage in the study area including buildings. During initial feedback 57% of respondents selected a historical theme for the area with the remaining theme options receiving less than 10% each. Additionally, 77% of respondents believed that the study area could be more animated with a sense of identity through public art.

Internal/External Consultation

The project team consulted internally with City Staff including Infrastructure Engineering, Transportation Engineering, Policy Planning, Economic Development, Planning Services, Asset Management, and Parks and Recreation Operations. Grand River Conservation Authority has jurisdiction in the study area and was consulted regularly on proposed plans. Additionally, MartinSimmons Architects were consulted, including a site visit, on the 30 Milling Road building condition and options for renovations.

The Hespeler Village Business Improvement Area (BIA) was consulted during the project on the various draft concepts proposed. On June 9, 2021 the BIA ratified a motion in support of their preferred concept being Concept 2 - Phase 2A. This concept design is curved with staggered parking on either side and the BIA supports maintaining the existing City owned building.

The Milling Road Streetscape and Urban Design Guidelines were distributed for information to the Municipal Heritage Advisory Committee members.


Conclusion

Staff is recommending that Council endorse the preferred concept (Concept 2 - Phase 2A) of streetscaping improvements. It is intended to include a detailed design project and subsequent construction project as part of the capital budget forecast through a future budget cycle.

The City's Sustainable Design and Development Group recently completed an updated building condition assessment for 30 Milling Road. It is recommended that a Phase 1 and Phase 2 Environmental Site Assessment specific to the 30 Milling Road property will be completed to provide Council with additional information to make an informed decision regarding the future status the building. An internal staff working group has been formed to review the 30 Milling Road building and estimated costs to report back to Council.

Signature

Division Approval



Reviewed by the CFO

Reviewed by Legal Services

Name: Kevin De Leebeeck
Title: Director of Engineering

Departmental Approval

Name: Hardy Bromberg
Title: Deputy City Manager, Community Development

City Manager Approval



Name: Hardy Bromberg
Title: Acting City Manager

Attachments

- Appendix A - Hespeler Village Streetscape Plan and Urban Design Guidelines (2013)
- Appendix B - Milling Road Streetscape and Urban Design Guidelines (2021)
- Appendix C - Building Structural Condition Assessment (2021)
- Appendix D - Cost Estimate Class D – 30 Milling Road (2021)



Streetscape Plan & Urban Design Guidelines

Design Report

City of Cambridge - Hespeler Village

October 2013

Part

01 Executive Summary

01 Executive Summary

The Streetscape Plan and Urban Design Guidelines Project is an initiative undertaken by the City of Cambridge with the objective of injecting short term vitality and long term sustainability into Hespeler Village. The final goal of this study is the preparation of a Streetscape Plan for the City-owned land and the preparation of Urban Design Guidelines for privately owned building façades and infill development opportunities within Hespeler Village. This report has been prepared to summarize the background information of this project and to guide the development of the Streetscape Plan and Urban Design Guidelines through Vision and Guiding Principles.

IBI Group has been commissioned by the City of Cambridge to understand the context of the study area, engage the public in the design process and prepare a Streetscape Plan and Urban Design Guidelines for Hespeler Village that appropriately reflects the heritage significance of the area and promotes a pedestrian friendly and aesthetically pleasing environment. Key considerations of the Streetscape Plan include promoting investment opportunities, creating more attractive pedestrian oriented spaces, protecting and enhancing the heritage of these distinct areas and promoting sustainable and long term visions.

In order to complete the study, IBI Group undertook a comprehensive review and analysis of the existing conditions, the public perception, needs and aspirations of the area and formulated a vision and guiding principles from this analysis. Through a review of current land uses, regional context, street networks, urban fabric, pedestrian networks, heritage and cultural assets, parking and natural resources, IBI Group solidified an appreciation of the evolution and function of Hespeler and the opportunities available. Through the process, engagement of the public was a priority and was completed through a number of ways including a site walk, on-street survey, public open house, workshop and an online survey.

The site inventory, analysis, background review and public engagement revealed that the Speed River, the rich unique history of the area, and compact development in the area were strengths of the area. Alternatively, the vehicular dominated transportation, constrained roadways and high volumes of traffic, poorly maintained public infrastructure and the lack of visual continuity through the core area were weaknesses that require

improvement. There is tremendous opportunity to build on the Village's strengths and improve upon its weaknesses by further celebrating culture and heritage, promote a connected continuous design and to ensure the Streetscape Plan promotes an active streetscape and high quality aesthetically pleasing environment.

In developing any plan, it is vital to have a strong foundation. The following vision statement was developed to provide direction to the Streetscape Plan and Urban Design Guidelines:

Hespeler Village is a place for people to enjoy culture and services. The Village is inviting, vibrant, and universally accessible for all individuals and families to gather and experience. The Village provides amenities to all who live and work in the area, and is a destination for arts and culture, services and special events.

To ensure that design directions and guidelines align with the vision statement a series of overarching principles were created and are as follows:

- Create a climate of economic success;
- Embrace art, culture and heritage;
- Create an active and vibrant village destination;
- Inspire high quality and unified design; and
- Encourage environmental sustainability.

These principles evolved through inventory and analysis of the area, public engagement and review of previous studies, policies and current practices in streetscape design. The vision and guiding principles will guide all future design decisions that exist within the Plan's design, strategies and recommendations. The following document outlines in detail how the vision and guiding principles were developed and the various influential key findings that occurred during the design process.

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Part

02 Inventory and Analysis

2.1 Project Introduction

The City of Cambridge recently updated its Community Improvement, Renewal, and Revitalization section of its Official Plan (2012) with a number of improvement goals for the municipality. The City has designated Community Improvement Project Areas (CIPAs) for Hespeler Village. The objective of the CIPA is to assist with the revitalization, redevelopment and enhancement of these areas through various financial incentives. The financial incentives are intended to assist property owners within the designated CIPAs with private property improvements. A companion to the CIPA program is the development of a streetscape plan for public infrastructure improvements and urban design guidelines to assist with the implementation of financial incentives for privately owned property in these downtown areas.

The City's Vision Statement and numerous strategic master plans articulate the City's overall direction for future initiatives and provides a strong foundation for the Streetscape Plan in Hespeler Village. Key considerations of the Streetscape Plan will include promoting investment opportunities, creating more attractive pedestrian and transit oriented spaces, protecting and enhancing the heritage of these distinct areas and promoting sustainable and long term visions. The Streetscape Plan will solidify both an economic and social dimension for the future of Hespeler Village.

The Streetscape Plan and Urban Design Guidelines will capitalize on existing major assets within the core, such as the Hespeler Library, Johnson Centre and the Speed River as well as long standing businesses, new businesses and arts, culture and heritage organizations that centre themselves in this area. Hespeler Streetscape Designs are an "opportunity for place making" and a tool for the creation and enhancement of "space for people". An important part of this study is to understand and listen to the needs, ideas, and visions of the Hespeler community. It is therefore imperative that the stakeholders are engaged throughout this process, to ensure this project advances in the direction the community desires.



Historic Hespeler Post Office

Study Process

To achieve the goals of this project, the following work plan was undertaken. The plan is summarized in the table below. More details on each of the project's phases are provided below.

Phase 1: Research and Data Collection

To produce an effective Streetscape Plan and Urban Design Guidelines that focuses on the principles, services, character, values and vision for the future, it is important to first understand the policy, market, current trends and physical environments that influence and make up the study area. This was completed through an analysis of past studies, and coordination with current initiatives. Also, as part of this reconnaissance, Phase 1 involves public engagement to ensure that all necessary people, agencies, community groups, initiatives and previous work shape early thinking.

For this phase the following tasks were completed:

Task A: September 2012	Start-up Meeting and Site Walk Documents Review
Task B: October 2012	Information Gathering and Inventory Identify Strengths, Weaknesses, Opportunities and Threats
Task C: October 2012	On Street Interviews
Task D: October 2012	Design Workshop - October 9, 2012
Task E: December 2012	Public Open House - December 5, 2012 Online Survey and Data Collection

Phase 2: Direction and Implementation

The intent of Phase 2 is to synthesize the work undertaken in Phase 1 and focus on the conceptual design of the Streetscape Plan and Urban Design Guidelines. Based on the results of the public consultation, the 'early vision' of the options and design directions is further enhanced, modified and clarified in an attempt to refine and confirm the final options and directions for the Streetscape Plan and Urban Design Guidelines. The final Phase will assemble all of the consultation and work that was conducted and accumulated throughout the project process.

For this phase, the following tasks were completed:

Task F: March 2013	Preparation of Draft Streetscape Plan, Urban Design Guidelines, Preliminary Costing, and Draft Implementation Plan
Task G: June 2013	Finalize Streetscape Plans, Urban Design Guidelines, Costing and Implementation Plan
Task H: June 2013	Presentation to Steering Committee and CARAC
Task I: June/July 2013	Presentation to BIAs (Hespeler - June 27, 2013 & Preston- July 11, 2013)
Task J: October 2013	Final Product Delivered

Purpose and Objectives

The Streetscape Plan and Urban Design Guidelines for Hespeler Village intends to:

- Review current land uses, regional context, street networks, urban fabric, pedestrian networks, heritage and cultural assets, parking and natural resources to understand the evolution and functionality of the Study Area;
- Inventory, assess and analyze existing conditions (i.e., Strengths, Weaknesses, Opportunities and Constraints) as they relate to:
 - Culture and heritage;
 - Demographics (users and their needs);
 - Accessibility and connectedness (e.g. pedestrian safety, sidewalk conditions, connections to adjacent areas, universal accessibility, transit orientation, wayfinding);
 - Street design and physical environment (street furnishings and lighting);
 - Parking and user demand;
 - Architectural facades;
 - Existing land uses and existing facilities/services; and
 - Sustainable technologies.
- Provide a plan that builds upon the policy framework including Provincial, Regional and Municipal documents;
- Identify strategic future public investments that will inject short term vitality and long term sustainability into the core area and provide an implementation plan showing highest and lowest priorities with associated required funds and coordination with future infrastructure projects

Study Area

The Study Area is comprised of the Business Improvement Area (BIA) area which encompasses much of the Village core area bounded by Harvey Street to the west, Speed River/Milling Road to the North, Tannery Street to the south, and lands just past Cooper Street to the east. In addition, the Study Area also includes properties located along Milling Road along the north side of the river. Key Features of the Study Area include:

1. Trail Connections along the River (Mill Run Trail) and to adjacent linear open space
2. Hespeler Library
3. Hespeler Old Town Hall
4. Town Hall Parkette
5. Connection to Forbes Park
6. Jacob's Landing
7. Concentration of Retail Stores
8. Bridge over the Speed River





2.2 Site Inventory and Analysis

The following is an overview of the Core area's physical, social and economic components which define both character and opportunities. Understanding these layers and the urban structure that they create is the starting point for analysis of the physical form of the area and preparing responsive recommendations for any streetscaping improvements or recommendations.

A thorough site inventory and analysis was conducted in the beginning stages of this study, relying on the review of documents and data supplied by the City, supplemented by a site walk with the Steering Committee on September 17, 2012. The information gathered from the site analysis is an important reference for the development of an informed Streetscape Plan for Hespeler Village. An analysis of the existing conditions is summarized in the following section with supporting analysis maps and imagery. Opportunities and constraints are also discussed.

2.2 Study Area Inventory and Analysis

Demographics

The population of Cambridge according to the Community Development Profile is 132,300, and is expected to reach 173,000 by 2031 growing at an average growth rate of just over 1% per year. The 2011 Census data indicated that the average age of a Cambridge resident is 38 years, which is younger than the Ontario average of 40 years; however the largest percentage of the population is between the ages of 44 and 54 (about 16%). Using the 2006 Census data the largest percentage of the population was between the ages of 25 and 44 (about 30%). The large percentage of population aged between 25 and 44 and the shift of the population distribution to an older age, indicates it will be important to consider an aging population when planning for the future, especially with regard to the programming, design and accessibility of the subject sites.

Within the Hespeler Study Area, the population is approximately 3,860 people. Most people live in single detached households and in addition the dominant type of housing is single detached. In adjacent areas, there are approximately an additional 10,000 people within walking distance (generally 600m.) that live in single detached homes with children. This is an important consideration as opportunities to enliven the Hespeler core area exist within this surrounding population if both the attractions within the Core Area can be increased and linkages between the two areas can be strengthened.

Land Use and Character Areas

Hespeler is a mixed used destination combining a strong industrial past with parks, trail connections, retail, institutional, residential and leisure activities found within its boundaries. Historically it was the commercial centre of the Town of Hespeler and served the adjacent residential areas with all the traditional retail and service commercial uses. Today this area offers more specialized services and retail opportunities that cater to culture, entertainment and tourism. Within this area the urban fabric consists of a series of character areas with very diverse atmospheres as a result of building scale and massing, architectural features, function and overall visual aesthetics. In any streetscape or core area study it is important to recognize the different built forms and characters that exist in a given area.

In Hespeler Village, three distinct character areas have been identified: a compact core area, an adaptive reuse area and an area characterized by less compact and lower density built form. The focus of the Streetscape Plans and Urban Design Guidelines will be on the compact core area and how to link the other character areas cohesively. In the Hespeler compact core area there is a variety of uses including residential, retail, restaurants and other commercial uses located in close proximity to each other. This means that it is easy and convenient to walk to many different types of destinations within a relatively short period of time. There is an

opportunity to strengthen this compact area by improving the quality of the pedestrian environment within it and connecting to it.

Included in the study area is a group of industrial buildings that have been adaptively reused into commercial and retail spaces. The configuration of the buildings, their relationship to the street, their relationship to the river and the lack of vehicular traffic offer a unique atmosphere to create a pedestrian oriented destination much like the Lang-Tannery building in Kitchener, Distillery District in Toronto or Liberty Village in Toronto. The character of the old buildings coupled with the lack of vehicular traffic and the connection to the river and existing trails present a tremendous opportunity to create a high quality pedestrian area and event space.

The remainder of the study area is characterized by less compact development with many opportunities for redevelopment into a more compact form. The former American Standard site is proposed to be converted to residential development consisting primarily of condominiums. This is sure to bring additional activity to the Queen Street retail corridor, and highlights the importance of improving pedestrian connections between this new development and the rest of the commercial core.

Character Areas

The Character Area Plan identifies distinct areas within the study that are based on the unique, recognizable character that is different from neighbouring areas. The character areas are a result of differences in the types of buildings, form of building, age and style of building, density, and land use patterns.



Circulation and Parking

There are a number of access points to Hespeler Village from Hespeler Rd. and other adjacent areas namely: Queen Street from the west, Guelph Ave. from the north, and Franklin Boulevard/Tannery Street from the south, Queen Street from the east and Cooper Street from the southwest. One unique challenge for Hespeler Village is that Highway 401 separates this area from the rest of Cambridge and that the core area has been by-passed by Hespeler Rd. This has limited the volume of vehicular traffic that enters and exits the area. During most parts of the day congestion is not a problem but at rush hours, traffic is a problem. Although a challenge it also allows Hespeler to remain unique and identifiable.

Operationally, there are issues with the width of Queen Street which are particularly manifested at the intersection of Guelph/Queen during the rush hours. Franklin Blvd./Tannery St. is also operationally challenged, most noticeably during the evening rush hour. Franklin Blvd. is a major carrier of intra-city traffic and it disperses that traffic onto a local scale road (Adam St. to Tannery St.) that makes a circuitous route through the core. From an operational perspective, a safety perspective and from an urban design perspective, these roads are not an attractive element of the core and likely turn prospective users of the core facilities away.

Many of the sidewalks are narrow, curb-faced and close to traffic hence making these areas appear to be less safe and enjoyable. For this reason it is important to consider the safe coexistence of vehicles and transit oriented traffic circulation while providing a quality pedestrian environment. As assessed through site reconnaissance the current pedestrian facilities are not adequate and have a number of issues including:

- Constrained walkways and lack of continuous unimpeded path of travel (too many obstacles like light standards, business signs, site furnishings, etc.) No separation from vehicular traffic and the need for landscape and buffering;
- Signage and wayfinding need improvement as it is difficult to navigate;
- The need for universal accessibility (most shops are not barrier free);

- Significant grade changes throughout the study area.

An additional consideration for any streetscape design for a core area is the assessment of parking, particularly where it is provided on-street. In 2009 the City of Cambridge adopted the Core Areas Parking Master Plan that assessed parking in the Core Areas of Galt, Preston and Hespeler and provided recommendations for improvement. This assessment indicated that in the Hespeler core area there was adequate parking in both on-street and off-street spaces. The on-street parking in Hespeler on both sides of the street provides the premium on-street parking; however, it restricts traffic flow and transit operations. Given this assessment, on street parking should be evaluated and weighed against the opportunities for transit and a quality pedestrian environment. The Parking Master Plan proposes that on-street parking on Queen St. be offered on one side of the street only, throughout the core. This proposal was to provide for better vehicular, transit and pedestrian circulation, while identifying the City's Queen St. parking lot as the provider of the majority of parking.

The Hespeler Core Area is uniquely situated adjacent to natural areas and the Speed River. Its proximity to the Speed River and the numerous connections to parks (Forbes Park, Riverside Park Hespeler, Mill Pond) and trail connections, provides the opportunity to promote active transportation. Many residents visit Hespeler for leisure and recreation when they are walking through the trails and open space.



Tannery and Queen Intersection/ Towne Hall Parkette

Nodes and Gateways

Within the Hespeler study area Forbes Park, Old Towne Hall Parkette and Jacob's Landing are the hubs for activities both programmed and unplanned, including the Santa Claus Parade, Hespeler Healthy Community Coalition Speed River Clean Up, Hespeler Teen Music Society at Forbes Park Scout House, Annual Forbes Park Music Festival, and the Motorcycle Show. These areas are beacons for the community and should be fully integrated into the Streetscape Design and wayfinding considerations in Hespeler. Safe pedestrian routes should be provided from the core commercial area, parking areas and transit to these nodes of activity.

Although there are gaps in the urban fabric between activity nodes there are a number of features that can also be integrated in the streetscape fabric and provide opportunity for public amenity space including small parkette type spaces along the main circulation routes as well as continuity in the streetscape features that will help tie the gaps. These attractions and areas of activity need to be woven more tightly together with signage and wayfinding to present a consistent streetscape that is easy to navigate. Securing effective linkages across and through the core area between these attractions is an ambition of this plan. This can be further achieved through defined gateways to demarcate the entrance to this culturally significant area. Gateways provide a sense of enclosure and delineate specific spaces, as well as provide a traffic calming mechanism.



Towne Hall Parkette (corner of Tanner Street and Queen Street)

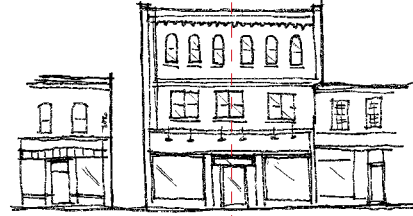
Architectural Façade and Heritage Assets

The presence of a historic environment - whether it is buildings, monuments or landscapes - helps to create a unique sense of place and local identity. In many cities, like Cambridge, historic buildings are valued and provide a distinct character to each of the core areas. Often the historic buildings have in some way played a central role in driving economic development, sustainable growth, and cultural development. Well maintained, historic environments can deliver a range of benefits to a city by:

- Creating a sense of place;
- Attracting visitors, activities and investments to the core area;
- Providing supportive pedestrian environments; and
- Offering a focus for leisure facilities, such as theatres, art galleries, studios, restaurants and bars.

Hespeler Village contains a large number of important heritage assets, including many heritage designated buildings and properties of interest. Most of these assets are in good condition and contribute positively to the image of the core area. In particular, the American Standard building at 19 Guelph Ave. is currently undergoing renovations to adaptively re-use elements of the cultural building for new residential dwellings; Old Town Hall is currently being used as a Fire Hall and a historical organization. Other industrial buildings along Milling Road also have potential to become significant and beautiful additions to the heritage resources of the community.

The Speed River / Mill Pond and industries immediately adjacent to the river are an apparent dividing physical separator for the community. Since these industries occupy prime real-estate and may have site contamination and being located in the floodway, they present unique opportunities for adaptive re-use and becoming a strong unifying factor. In addition, there is opportunity to use these heritage assets as a starting point for façade improvement. Facades should reflect the surrounding heritage character and should also represent contemporary themes. In Hespeler the buildings create a tangible sense of place through the varied architectural styles and periods. Many of the buildings in the core area have not been extensively altered from their original façade and largely preserve the existing character and history of the area.



Opportunity

- Axial symmetry of whole and of parts
- Sign board height consistent with street
- Back-lit sign replaced with surface mounted sign and snorkel lighting
- Retractable awning



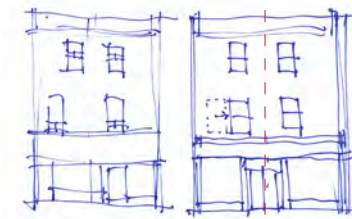
Queen Street - North Side (between Adam Street and Tannery Street)



Queen Street - South Side (between Adam Street and Tannery Street)

Opportunity

- Axial symmetry of whole and of parts
- Windows realigned and original openings restored
- Sign board height consistent with street
- Surface mounted sign and snorkel lighting
- Building Painted to complement Streetscape



Lighting

Queen Street, Tannery Street, Guelph Ave., Adam Street all currently serve the predominant function of moving road traffic in an efficient fashion. The on-street light standards are designed to support this predominant function and the general assessment is that the lighting levels on the travelled portion of the right-of-way are poor. Most of Hespeler Village is illuminated with top mounted “lantern style fixtures” that are mounted at a height that is too high to be efficient for pedestrian lighting. The fixtures are mounted at a height of approximately 16-18ft which is at the same level of the second storey windows. A number of the fixtures are not operational whether from a “burned-out” lamp or a defective fixture. The street lighting is serviced by underground cabling that is installed from pole to pole.

One section of Queen Street, located between Adam Street and Harvey Street is illuminated by standard “cobra-head” fixtures suitable for lighting the travelled portion of the right-of-way. The large, tall poles with “cobra-head” fixtures focus their lighting onto the road but do not provide effective levels of lighting for the pedestrian environment. Essentially, any sidewalk lighting on Queen Street comes from spillage from the “cobra head” street light fixtures. The street lighting is serviced by overhead cable that is installed from pole to pole.

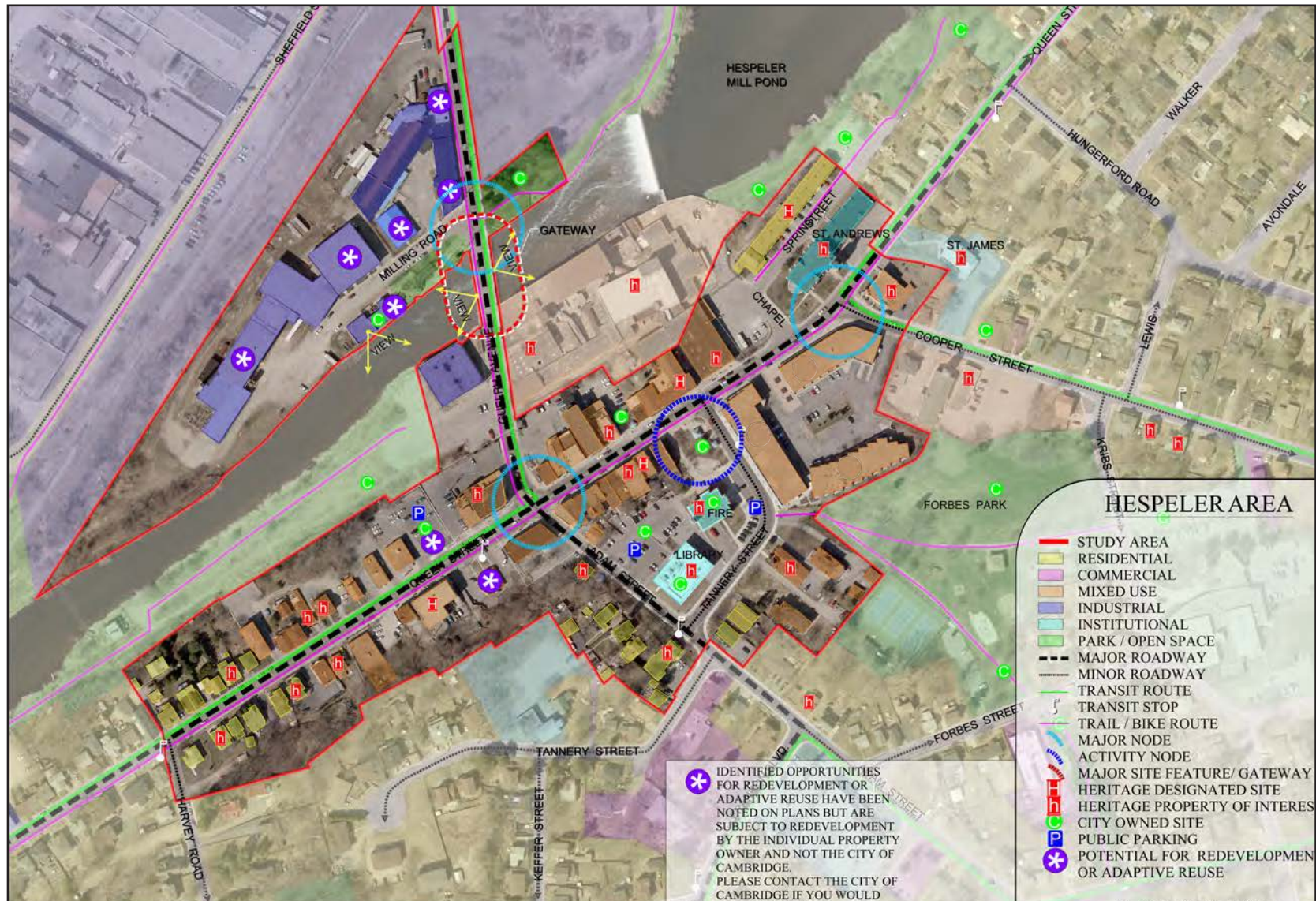
There are opportunities for providing better lighting solutions for both vehicular traffic as well as the public realm. Providing new underground infrastructure, replacement of fixtures with more cost efficient technologies while reworking pole and fixture spacing, provision of power for seasonal and event lighting are all opportunities that should be further examined.



Existing light standards - Hespeler Village

Study Area Inventory and Analysis

The study area is primarily focused upon the activity that occurs on Queen Street, Tannery Street, Adam Street and Guelph Avenue. It is comprised of the BIA area and much of the core area district along Queen Street. It is bounded by the railway at the northwest and is truncated just past Queen Street.





2.3 Policy Review

The Streetscape Plans must operate within the existing planning framework for the City of Cambridge, the Region of Waterloo and the Province. This section provides a summary of the general intent and ambitions of each of these levels of government with regard to the future of Hespeler.

2.3 Policy Review

Provincial Policy Framework

Provincial Policy Statement, 2005

The Provincial Policy Statement (PPS), provides policy direction on matters of provincial interest related to land use planning and development. It supports improved land use planning and management, which contributes to a more effective and efficient land use planning system. All decisions affecting planning matters “shall be consistent with” the PPS.

In the PPS, the vision for land use planning is stated as “the long-term prosperity and social well-being of Ontarians depends on maintaining strong communities, a clean and healthy environment and a strong economy”. To this end, the PPS provides for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural environment. The PPS supports the provincial goal to enhance the quality of life for the citizens of Ontario. The PPS also promotes the creation of “healthy, liveable and safe communities”, through efficient land use patterns, appropriate mix of land uses, cost-effective development standards to minimize land consumption and servicing costs, the avoidance of environmental or public health and safety concerns, improving accessibility, and ensuring infrastructure and public service facilities are available to meet current and projected needs.

An important emphasis of the PPS is that settlement areas be the focus of growth and that their vitality and regeneration shall be promoted. Policy 1.1.3 states that land use patterns within settlement areas are to be based on densities and a mix of land uses, as well as a range of uses and opportunities for intensification and redevelopment. Opportunities for intensification and redevelopment are encouraged, but must take into account existing buildings, the surrounding area and the availability of suitable existing or planned infrastructure and facilities. Policy 1.1.3.5 states that Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, unless already established through a provincial plan.

A coordinated, integrated and comprehensive approach should be used when dealing with planning matters within municipalities, including managing and promoting growth, development, natural and cultural heritage, the environment, resources, and projections of population, housing and employment. The development of transportation systems that are safe, accessible and efficient is particularly sought, with land use patterns that minimize vehicular dependence and maximize opportunities for public transit use.

Policy 1.3 of the PPS pertains to employment areas and the importance of promoting economic development and competitiveness by providing for a range and mix of employment, provide opportunities for a diversified economic base, protecting employment areas for current and future uses, and ensuring necessary infrastructure is provided.

Places to Grow: Growth Plan for the Greater Golden Horseshoe, 2006

In 2006, the Ontario Government approved the Places to Grow: Growth Plan for the Greater Golden Horseshoe (Growth Plan). A Vision of the Growth Plan is that urban centres will be characterized by vibrant and more compact settlement and development patterns and will provide a diversity of opportunities for living, working and enjoying culture. The guiding principles of the Growth Plan provide the basis for guiding decisions on how land is developed, resources are managed and public dollars invested, which include: to build compact, vibrant and complete communities; plan and manage growth to support a strong and competitive economy; optimize the use of existing and new infrastructure to support growth in a compact, efficient form; provide for different approaches to managing growth that recognize the diversity of communities; and protect, conserve, enhance and wisely use the valuable natural resources of land, air and water for current and future generations.

The General Intensification policies of the Growth Plan (2.2.3) provide for a minimum of 40 percent of all residential development occurring annually within each upper-tier municipality will be within the built up area. This is to be achieved by encouraging intensification and to recognize urban growth centres, intensification corridors and major transit station areas as a key focus for development to accommodate intensification. The Hespeler Village is within the built up area.

Regional Policy Framework

Regional Official Plan

The Regional Official Plan (ROP), is the Region of Waterloo's guiding document for directing growth and change for the next 20 years. It is important to note that the ROP is currently under appeal at the OMB, in its entirety. Section 2.C of the ROP pertains to reurbanization targets wherein reurbanization areas may be identified to focus higher densities to establish reurbanization targets as identified in the built-up areas. Section 2.D of the ROP pertains to urban area development policies, identified as where the majority of the Region's future growth will occur. This designation contains the physical infrastructure and community infrastructure to support major growth, including transportation networks, municipal drinking-water supply systems, municipal wastewater systems, regional transit system, and a broad range of social and public health services. For these reasons, lands within the Urban Area have the greatest capacity to accommodate growth and serve as the primary focus for employment, housing, cultural and recreational opportunities in the Region. Hespeler Village is located within the Built Up, Urban Area of the City of Cambridge.

The ROP also contains planning goals and priorities for a vibrant and sustainable community, and outlines specific strategies and policies to help achieve that vision. Generally, the ROP promotes the establishment of a full range of housing types and densities, efficient land patterns, and innovative lot patterns and housing design; opportunities for mixed-use developments; conserves cultural heritage resources and supports the adaptive reuse of historic buildings; respects the scale, physical character and context of established neighbourhoods in areas where reurbanization is planned to occur; promotes building designs and orientations that incorporate energy conservation features; the creation of land use patterns, street network patterns and design that support public transit usage; and transportation opportunities for pedestrians, cyclists and transit users.

Municipal Policy Framework

City of Cambridge Official Plan

The City's Official Plan provides a vision of its future growth and a policy framework for guiding the physical development within the City. On November 21, 2012 the Region of Waterloo issued the Notice of Approval of the Cambridge Official Plan (OP). Approval of certain parts of the OP has been deferred, and the remainder of the OP has been approved with modifications and subsequently appealed. Therefore, while the Streetscape Plan should take into consideration the current Official Plan (1999), it is most important to provide a framework for the future that conforms to the goals and objectives of the Approved Official Plan (2012) and reinforces the vision for the core area of Hespeler. The Official Plan will be utilized through the design process to help guide and identify opportunities for infill development in the study areas and will be used as a factor for recommending opportunities for redevelopment within the study areas.

The policies of the City of Cambridge Official Plan (2012) provide the framework for land use decisions for all development and public works projects within the City of Cambridge. Relevant to this study, the land use designations give direction to the type of development desired for each city area. Hespeler Village currently acts as a Community Core Area, however in the midst of the land use intensification planned for the City, the Streetscape Design is intended to establish guidelines by which the areas will continue to provide safe, attractive, pedestrian friendly, culturally rich and integrated spaces to serve more localized functions for the future.

Municipal Policy Framework

City of Cambridge Zoning By-law

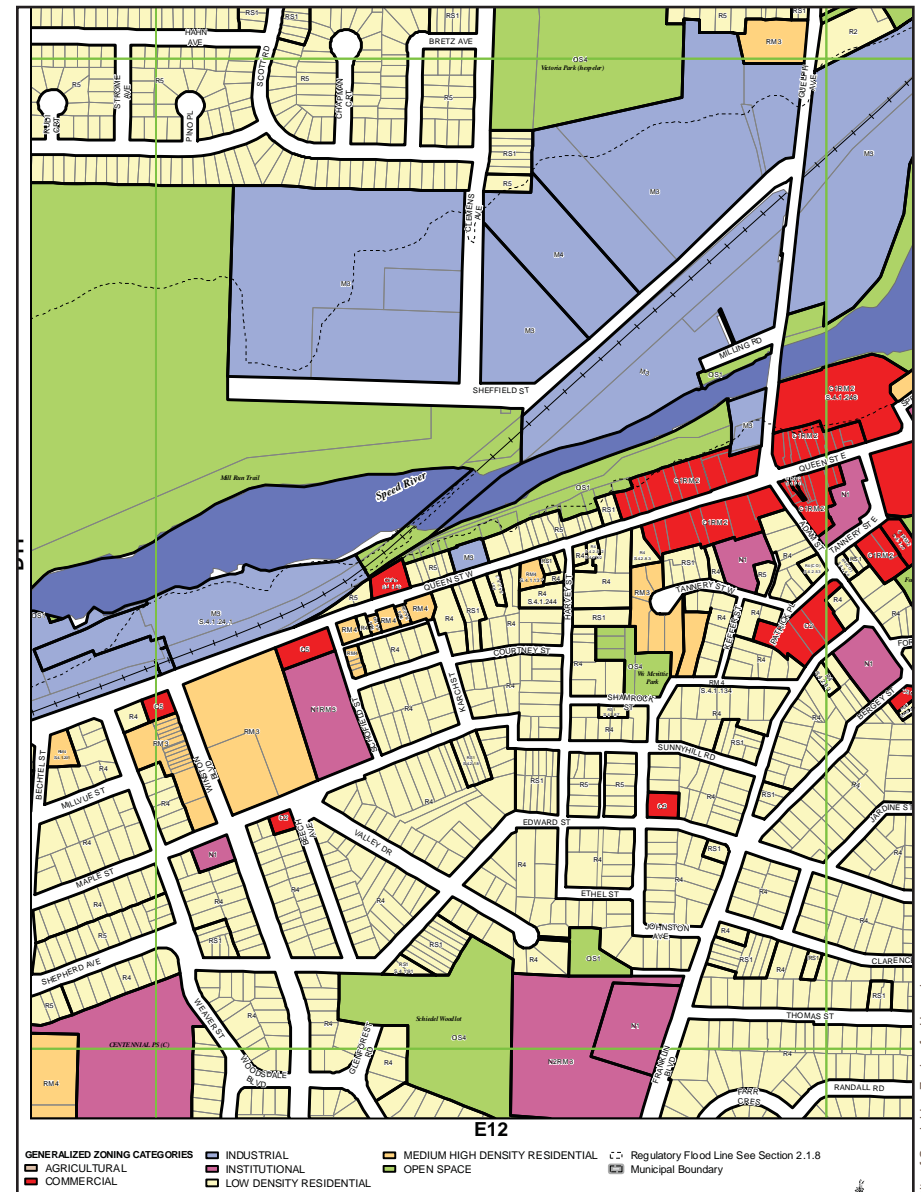
The City of Cambridge Zoning By-law sets out policies and regulations that apply to all new development of buildings, structures and sites within the City of Cambridge to ensure they are developed in accordance with the Official Plan and Regional Official Plan Policies, and meet the density targets set forth by the Places to Grow Plan.

The City of Cambridge has defined the majority of the zoning along the Queen Street East Corridor in the Study area as C1RM2 (reference attached map). The C1RM2 Zone generally plans for mixed used development. The Zoning permits both commercial uses and higher density residential developments (maximum of 150 units per hectare), ranging from Townhouses to Apartment Dwellings.

The area on Queen Street (just east Harvey Street spanning to Cooper Street) is all designated as mixed use development with a minimum height of 6.0m for any new development. This area has also been identified an area where no off-street parking or loading are required. A large portion of the study area can be identified as “opportunity for redevelopment” (at the land owner’s cost). Currently, the JG Group has plans to redevelop the American Standard Site (located at 19 Guelph Ave.), to add an additional 120 units to the study area – emphasizing the need for future convenient services and amenities within the core area.

The remainder of the Study area on the south side of the Speed River is zoned Institutional and Residential (where established single residential and townhouses are located). On both sides of the Speed River are general industrial uses - this includes the Milling Road area, which has recently been nominated to become part of the Hespeler Business Improvement Area and is a major feature within the Study Area for the Streetscaping Program.

The surrounding areas are well established neighbourhoods complete with single detached residential, open space and institutional uses.



Municipal Policy Framework

City of Cambridge Corporate Sustainability Plan (2011)

In the City of Cambridge Corporate Sustainability Plan, sustainability refers to “development that meets the needs of the present without compromising the ability of future generations to meet their own needs” (Brundtland Commission, 1987). Cambridge has regard for balanced decision-making on the basis of cultural, economic, environmental and social considerations.

The Corporate Sustainability Plan has a distinct focus on ensuring the sustainability of the City’s operations and the program and policies developed for the community at large. This Plan is meant to provide a lens through which to balance the City’s fiscal responsibility with cultural (including heritage), economic, environmental and social interests.

The overall goal of the Corporate Sustainability Plan is to foster a strong sustainability culture within the City’s operations that permeates through to its programs, policies, initiatives and practices that, in turn, support the four pillars of a sustainable community, namely:

- Culture – A vibrant cultural scene that embraces Cambridge’s history and generates new ideas for future growth.
- Economics – A diversity of sustained economic development opportunities for the community and fiscal sustainability within the City’s operations.
- Environment - A diversity of sustained economic development opportunities for the community and fiscal sustainability within the City’s operations.
- Social - A diversity of sustained economic development opportunities for the community and fiscal sustainability within the City’s operations.

The streetscape design aims to address the four-pillar model of sustainability by supporting 1) cultural 2) economic 3) environmental and 4) social initiatives.

City of Cambridge Arts and Culture Master Plan

The Arts and Culture Master Plan was completed in recognition of the increasing importance of arts and culture to the future growth of the community, and acknowledgement of the changing needs of its residents for arts and cultural services. The plan indicates that activation requires initiatives in policy, funding, direct provision of service and support from the arts sector. The principle ideas of the plan are reflected in the vision statement in which:

“Celebrating the uniqueness of its communities, and united by its cultural heritage, the Grand River and the common future of its communities, Cambridge will be a leading cultural centre in Southern Ontario, known for offering a variety of cultural opportunities and events of interest to a wide range of visitors and residents.”

The definition of culture that is used in the plan is identified as a multi-faceted concept that can mean any of the following:

- It can be the formalized expression of visual, performing, literary, and media arts activity in the community, as expressed in places such as art galleries, theatres, libraries, museums and community arts centres such as the Cambridge Centre for the Arts.
- It can be the customs and heritage (including built heritage) of a community, particularly when represented by a critical mass of people representing a certain cultural tradition or background; and
- It can be the energy and ‘buzz’ created through nightlife, festivals, events, and creative industries in downtowns and other focal points in the community.

The Arts and Culture Master Plan indicates a significant number of recommendations in order to realize the City’s vision. The following are recommendations that the City of Cambridge should consider that directly relate to the Streetscape Plan and Urban Design Guidelines:

- Continue downtown revitalization and beautification activities in all three of the major downtowns (or community core areas) of Cambridge. Overtime, these activities could lead to the development of culture facilities in each of the major downtown areas, and the designation of a cultural precinct in downtown Galt. In support of these efforts, ensure that the overall brand identity for Cambridge reflects the City's culture renaissance.
- Continue to encourage community-based and vernacular public art (e.g., murals, sculptures, possibly graffiti walls, performance art such as busking, etc.) on the part of the private sector and in appropriate public spaces.
- A dedicated marketing plan for arts and culture in Cambridge needs to be developed. This will focus on promoting the various arts and culture opportunities available to the arts and culture community, general residents of the City and Region, and to visitors.

It is essential for the Streetscape Plan to advance these recommendations where possible, especially through the revitalization and beautification of the core areas, and community-based public art. It is also important that the Streetscape Plan supports the key messages that the Arts and Culture Master Plan intends to convey to residents, including:

- The City is taking seriously the improvement of the City's arts and culture infrastructure including the services it provides to organizations through its resource allocations for staff and programmes.
- Arts and culture infrastructure and services are a key decision criterion for new investment.
- The City is taking proactive action to meet the changing needs of the community.
- The City is striving to meet the service, programming, and infrastructure needs identified by arts and culture organizations.

Other Studies

There were several other more specific Master Plans and Guidelines for which the Streetscape Plans consider including:

- City of Cambridge Heritage Master Plan
- City of Cambridge Bikeway Network Master Plan
- City of Cambridge Core Areas Parking Master Plan
- University of Waterloo School of Architecture Urban Design Guidelines
- Galt (Main Street) Streetscaping Study.



Queen Street, Hespeler



2.4 Public Engagement

Engaging the community is critical to obtain a thorough understanding of values and priorities, and one of the keys to successful consultation is to make it meaningful for participants. Various methods of communication with members of the public and stakeholders can be established to fit varying communication styles and desired levels of involvement. Throughout the project, the consultation program was developed to: understand the community's perceptions of their core area; assess residents' current and future needs and aspirations; identify and prioritize opportunities and constraints; identify and confirm options, design directions and strategies, and foster Community engagement in, and ownership of, the emerging Streetscape Plan and Urban Design Guidelines.

2.4 Public Engagement

Community Consultation

To successfully create a Streetscape Plan for a community, consultation with those who will be using the space plays a key role. The following tasks were used to engage the community:

- On-Street Survey
- Public Open House
- Online Survey

As part of the public engagement process the initial step included an on-street survey conducted with random pedestrians in the Study Area. During a sunny weekday in mid-October, IBI took to the streets to obtain opinions/ comments from typical users of the downtown, providing an initial snap shot of people's views, likes and dislikes of the Village Centre. During these interactions the focus was on listening, seeking input, informing and offering examples of how the elements of a streetscape plan could provide the desired atmosphere for the community while also providing functionality to meet a diversity of needs.

A Public Open House was conducted in Hespeler Village. A second, refined public survey, based on the previous survey and continued research was conducted in coordination with the public open houses. The survey was circulated to the public and interested parties and was made available online. The majority of survey respondents in Hespeler were in Hespeler on a daily basis and over 40% were there to use the shops and services in the downtown. The top destinations in the Village were food and beverage providers and the library. Streetscape improvements should focus on providing improved lighting, public art, public benches, a consistent façade design, pedestrian safety at crosswalks, more street vegetation and increasing vehicle parking. To contribute to the success of the urban core, providing everyday amenities, culture and more restaurants were indicated by respondents. The majority of respondents supported the Project Vision Statement and the objectives to help develop streetscape guidelines. Hespeler's best features are the heritage buildings followed by the seasonal/holiday street displays. The majority of respondents believed that few storefronts were well maintained and appealing.

What we were told:

Enhanced connectivity would improve the use of the businesses in the area, Consistent store fronts would look better (similar colour palate) if updated, More events that close off the streets, Add farmers market and live music venues, More free parking, More unique stores would improve the area, Way finding signage or "you are here" map would improve how people move through the core, Add more benches and street seating, Public art, Improved street lighting, Enhanced landscaping (more street trees and planters), Hespeler Village needs higher density, General upkeep (e.g. re-paint garbage cans, fix sidewalks, fill pot holes etc.), A clean river and safe and enhanced trails along waterfront should be incorporated into the final design, More food options, Hespeler is perfect as it is, More store variety, useful stores, more room for driving community events, Heritage buildings are the best features but most could be fixed up



2.5 Complete Streets

Streets are the most abundant and heavily used of public spaces and are the predominant structure that will inform the character of a city over time. They are the public framework upon which land uses are organized, through which utilities are routed and around which people's daily lives revolve.

Complex urban environments have three key requirements for success which are to: create a place through incorporation of unique or special local elements, to provide for traffic safety and the integration of vehicular and pedestrian realms, and finally buildability and the ability for implementation. Complete streets are streets for everyone and they are designed and operated to enable safe access for all users. Pedestrians, cyclists, motorists and transit riders of all ages and abilities must be able to safely move along and across a complete street. All streets can not possibly be all things to all users, but all users must be considered and accommodated in a safe and respectful fashion on all streets. This means more than simply providing a separate facility for each user; it is an exercise that acknowledges the limitations and interactions of each user group with the others and balances the priorities of each group within a hierarchy of street types.

2.5 Complete Streets

Streets as City Structure

Streets represent a significant investment of public funds, both in terms of their surfaces as well as the infrastructure that is buried beneath them. Consequently, it is often the case that the built form and land uses that surround them will change over time whereas the streets will not. It is critical, therefore, to maximize the flexibility of land uses to respond to changing economic conditions by organizing the street pattern to support the evolution of the City. Typically the greatest flexibility is found with a fine grain of small blocks interwoven with many low and moderate volume streets as opposed to large blocks and a few high volume streets, which tend to constrain potential uses to those that are auto oriented.



Streets for All Modes

The street network and individual streets are shared spaces in which the needs of the most vulnerable users are given greater priority over the less vulnerable. Streets must be designed to equitably accommodate a wide variety of users, from pedestrians, cyclists and playing children, to vehicular movement, freight delivery and transit. Because of the role of buses in transporting higher concentrations of people and diverting trips from single occupancy automobiles, it is equally important to find appropriate ways to prioritize transit operational efficiency.



Streets as Public Spaces

Streets are the most visible, heavily used and often the most costly public spaces in neighbourhoods and communities. They require a design approach that acknowledges the role they play in not only enabling circulation and making connections between important destinations, but in encouraging and defining a vibrant neighbourhood or community.

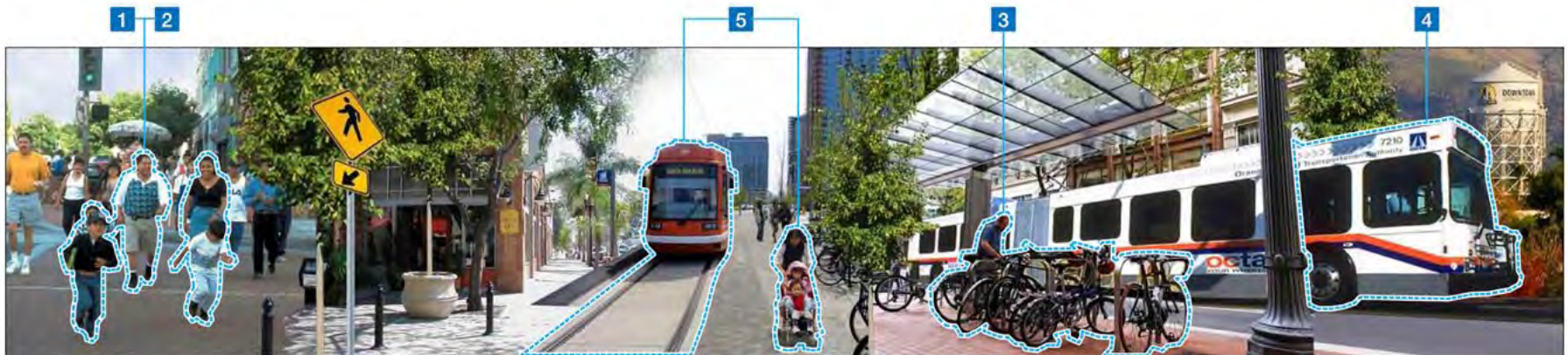




Place Monseigneur Charbonneau, Montreal, Quebec

"Complete Streets are streets for everyone. They are designed and operated to enable safe access for all users. Pedestrians, bicyclists, motorists and transit riders of all ages and abilities must be able to safely move along and across a complete street. Complete Streets make it easy to: 1 cross the street, 2 walk to shops, and 3 bicycle to work. 4 They allow buses to run on time and 5 make it safe for people to walk to and from train stations."

NATIONAL COMPLETE STREETS COALITION www.completestreets.org



Complete Streets

Parking

Parking is an important asset for any vibrant commercial district. Providing appropriate quantities of parking in a format that will support and strengthen the character that makes the place special can be a challenge. Utilizing on-street parking, in most contexts, combines convenience, efficiency and comfort by utilizing vehicle lanes for access to parking while providing a buffer for sidewalk users. When possible, place off-street parking behind street fronting uses and when economically feasible, prioritize structured parking within and below street fronting uses. Finally, ensure that parking “pays its own way” and is appropriately priced as a way to encourage use of other modes of transportation.

Green Streets

Streets represent a significant impervious proportion of the city, and because they convey pollutants, streets can have a detrimental impact on the health of streams and rivers. The impervious nature of streets limits infiltration and groundwater recharge, which contributes to an ever-growing groundwater deficit and receding water table in many communities. It is important to incorporate strategies to limit and mitigate impervious areas, properly handle pollutants carried in rainwater run-off and infiltrate clean water to replenish dwindling ground water supplies.



Milling Road Adaptive Reuse Development, Hespeler



2.6 Summary of Strengths, Weaknesses Opportunities and Threats

Based on the research and the development work to date, a number of strengths, weaknesses, opportunities and constraints have been identified. A summary of the inventory, analysis, background review is provided in this section. This section provides a broad foundation to which the vision and guiding principles are based on.

2.6 Summary of Strengths, Weaknesses Opportunities and Threats

Strengths

- Forbes Park and the connection to the linear park system
- Compact Development between Adam Street and Tannery Street
- Existing and future residential areas adjacent to the core area located within walking distance
- Rich and unique history of the area is largely preserved in the existing architecture and industrial buildings
- The Speed River and connections to adjacent trails and natural areas
- Old Town Hall and Library in close proximity to the core area
- Adequate parking already exists in the core area in publicly owned lots
- Johnson Centre
- Jacob's Landing
- Future trail connection to the Millpond Community

Weaknesses

- Constrained Queen Street right of way makes the integration of parking, vehicular circulation, pedestrian circulation more difficult
- Public infrastructure is poorly maintained
- Little visual continuity throughout the core area including paving, landscape buffers, site furnishings, signage, etc
- Links to the adjacent areas, natural areas, river and linear parks are not well established
- Not easy to navigate as it is not a linear area
- Little sustainable technologies are currently used including lighting and stormwater management
- Lighting is not adequate and of poor quality and design.

Opportunities

Culture and Heritage

The Core Area of Hespeler is already recognized as an area rich in heritage and culture. Prior to amalgamation into the City of Cambridge, Hespeler was its own Town with its own history, flavour and character. Given this unique history, there is opportunity to celebrate and create a unique community. This plan builds on the existing character and the community's desire to preserve its historic treasures while also moving forward in contemporary themes. There is tremendous opportunity for adaptive re-use of industrial buildings in Hespeler Village, particularly on Milling Road. By adaptively reusing these buildings in the creation of destinations and commercial uses they can greatly contribute to the culture and build on the existing heritage of the area.

Community consultation efforts associated with earlier planning and design projects for Hespeler indicated that the community would like to transform the area into a local tourist destination through the development of public amenities and art. Jacob's Landing and the area around the Old Town Hall were considered areas to create general gathering spaces for passive and programmed activities.

Connectivity

There are many different features within the study area and areas adjacent to the study area including the Speed River, Forbes Park, Jacob's Landing, the core business areas, Old Town Hall and the Library. It is important to ensure that these are all well connected. There is opportunity to improve and direct circulation to all the public amenities in and near the study area by creating a quality pedestrian environment through the development of a signage and wayfinding plan. A long term goal of the Region of Waterloo and City of Cambridge is to provide for active transportation systems (biking and walking routes) through the study site. The development of this area will enhance the current connections and provide a significant circulation system to and from the area.

Destination

Hespeler Village serves as an important economic feature for the area and has the opportunity to be a local destination for recreation, entertainment and leisure activities. Many residents visit the area while walking the adjacent trails and stop at a café, ice cream shop, and restaurant or for speciality retail service. There is a need to strengthen the "draw" of the Hespeler Core Area for the surrounding residential communities particularly through the creation of safe and efficient pedestrian and biking connections. There is an opportunity to create a configuration that allows for more 'gathering areas' within the core. There is also an opportunity to use the lands adjacent to the area to animate the street.

In addition to the opportunity for trail connections, the redesigned streetscape can accommodate for an enjoyable pedestrian experience allowing the residents to further use this space for recreational walking, as well as accessing the local amenities on foot or bicycle rather than by car.



Corner of Tammy and Queen Street

Threats

Traffic

As with many core areas, controlling traffic congestion is an issue. Many residents feel that the volume of traffic and the speed at which it travels through the Village makes pedestrian circulation hazardous. This is particularly true during peak hour times in the morning and evening. A better and safer pedestrian circulation system, integrated with and balancing the needs of the users of the vehicular traffic system is required. In the Hespeler Core Area the right of way is constrained which makes the integration of all transportation modes and parking difficult. The Core Area Parking Master Plan suggests that parking be limited to one side of the street to increase the areas needed for pedestrian circulation and transit through the core.

Continuity

There is a strong desire for consistent thematic and landscape treatments throughout the study area. Currently the streetscape does not have uniform or harmonious character or styles in paving, site furnishings, signage, and adjacent connections, lighting and planting. A new plan must ensure that the heritage character is enhanced while providing visual continuity throughout the differing areas. In addition to providing consistency throughout the study area the core area needs to be better demarcated to highlight the importance of the area as a commercial and significant destination for the City of Cambridge. Gateway features signify to visitors and residents that they are entering a distinct area. References to history, community and the City are all important components of any gateways into Hespeler. Gateways should be provided at the intersection of Hespeler Rd. and Queen St. to promote the destination and also within the study area to denote the core area.



Jacob's Landing Hespeler



Jacob's Landing on the Speed River, Hespeler

PART

03 Plan Foundation



3.1 Plan Foundation

In developing any plan, it is vital to have a strong foundation. A foundation is often developed around a number of key elements that are determined through inventory, analysis and public engagement. For the Hespeler Streetscape Plan and Design Guidelines, these elements include: a vision, guiding principles and project goals. With these in place, the development of strategies, priorities and implementation detail can be developed.

3.1 Plan Foundation

Vision

Vision sets a horizon, that is, a direction toward which all stakeholders within an initiative or project can work together corroboratively in order to achieve its goal. It provides a common perspective and driving force in moving the initiative forward. The following key themes represent the vision of Hespeler:

Economic Vitality

Adjacent Connections

Traffic Concerns

Aesthetically Pleasing

Placemaking

Public Amenity Areas

These themes reflect the key components and perspectives that provide a unifying force and directional foundation for the Streetscape Design and Urban Design Guidelines. As a result the following Vision Statement was determined to suit the Hespeler Village context:

Hespeler Village is a place for users to enjoy culture and services. A village is inviting, vibrant, and universally accessible for all individuals and families to gather and experience. A village provides amenities to all who live and work in the area, and is a destination for arts and culture, services and special events.



Parkette on Queen Street, Hespeler

The vision statement also coincides with the City of Cambridge Vision Statement:

“Cambridge celebrates the uniqueness of its founding communities and is united by its heritage, rivers, cultures and common future. Cambridge residents and visitors enjoy the natural environment, safe, clean, caring, sustainable and accessible neighbourhoods, with a wide variety of lifestyle and housing options and ample cultural and recreational opportunities. Cambridge, as a community of opportunity, encourages business growth and transition, entrepreneurial spirit, strong leadership, efficient government and the provision of municipal services, personal growth and civic pride.”

Guiding Principles

To guide Hespeler's Core Area transformation over the long term and align with the Vision Statement, a series of over-arching principles have been created and are presented below. All Guiding Principles and Objectives were initially developed by the project consultant, but were presented to the public for input. The Guiding Principles and Objectives identified in this section have been formulated through site inventory, SWOT Analysis and Public Consultation and have been determined to be the foundation for creating the Streetscape Guidelines and Streetscaping Plans. These principles should be followed to influence the form of all future development in the Hespeler Core Area. The information gathered from background studies, policy review, site visits and best practices inspired the creation of Streetscape Guiding Principles and they have been designed to give a focus to the intent and direction of the Streetscape Plan. The principles identify the desired, long term intentions for the proposed study area. It is the synthesis of these principles that will create a successful platform for the social, economic and environmental goals. The principles guide all strategic and design decisions that exist within the Plan's design, strategies and recommendations and are as follows:

- Create a climate of economic success;
- Embrace art, culture and heritage;
- Create an active and vibrant village destination;
- Inspire high quality and unified design;
- Encourage environmental sustainability.

The following points provide further definition and understanding of the key elements of the Streetscape Plan and outline how each principle is interwoven.



Canolaph in front of former Hespeler City Hall

Create a Climate of Economic Success

A key objective of creating a Streetscape Plan and Urban Design Guidelines is to inject short term vitality and long term sustainability into the core area. The City of Cambridge is embarking on an initiative that will make Hespeler an attractive place for investment and will provide the appropriate conditions for business to flourish. The Provincial, Regional and Municipal policies all indicate the need for community improvement that will eventually increase the economic success of the area. Economic success is inter-dependant on the improvement and success of the other guiding principles as these principles make investment more attractive.

Objective:

- Identify and enhance connections between the streetscape and adjacent or near points of cultural and recreational interest;
- Identify redevelopment options and adaptive reuse opportunities;
- Create areas for public programming opportunities;
- Promotion and marketing of unique Village Character;

Embrace Art, Culture and Heritage

Culture and heritage are important factors in the formulation of place. Placemaking is an approach and philosophy to the planning, design and management of public spaces that capitalizes on a local community's assets, inspiration and potential. Definition and celebration of place has been proven to be a key factor in the success of complex urban environments. To create a successful place it must provide sociability, a variety of activities and uses, access and connection to and from adjacent areas and must present a positive image that is unique to the local context. Where appropriate, key structures and symbols of Hespeler's past should be retained and celebrated through design. Where viable, important heritage features should be retained as a reference to the area's rich past. This will contribute to an authentic and highly marketable identity for the core area.

Objective:

- Create an interactive environment - a place of engagement and community building;
- Promote the integration of public art into the streetscape and public amenity areas;
- Preserve and enhance existing heritage;
- Create gateways to demarcate each distinct neighborhood in the study area;
- Identify and highlight points of interest along the study area.

Create an Active and Vibrant Village Destination

Creating a place that is livable means that it functions and is used effectively. For a street this means that people need to be able to conveniently move through the space in a safe and efficient manner in a variety of different transportation modes. A livable town centre is a place that is universally accessible to all users, all mobility types, all abilities and all ages. It has a pleasurable, safe and inviting pedestrian environment that is safely integrated with other modal types.

Objective:

- Encourage and enhance a safe and quality pedestrian realm;
- Enhance the existing gathering spaces, primarily Jacob's Landing and the area around Old Towne Hall and integrate them into the Streetscape Plan;
- Provide a signage and wayfinding plan that links all areas of the study and adjacent features;
- Integrate barrier-free elements;
- Identify and eliminate pedestrian / cyclist / vehicle conflict points and transition appropriately between them particularly at Queen Street and Guelph Ave;
- Foster and support cultural, commercial, education and recreation opportunities;
- Utilize Crime Prevention Through Environmental Design (CPTED) principles;
- Foster destination
- Provide seating and amenities along the streetscape to promote the use of the space as a place - not just a travel conduit.

Inspire High Quality Design

Quality of the physical environment is one of the most important influences on the image of the area. Physical improvement to the aesthetics of the street, building facades and furnishings in a high quality manner can encourage the use of the space for more than just a travel conduit. Any improvements need to be durable, low maintenance, safe, and sustainable.

Objective:

- Use materials that will perform in four season conditions;
- Ensure all street materials and furnishings are durable and low maintenance; and easy to repair
- Promote architectural variety and building façade standards;
- Integrate common elements along the streetscape to create a unified streetscape while reducing visual clutter (overhead wires, signage, utility poles, etc.);
- Use planting and consistent landscape treatments to visually unify the street; and
- Create a comfortable environment through micro-climate relief (ie. trees, wind screens, etc.).



Hespeler Library

Encourage Environmental Sustainability

Continuing to build on Cambridge's reputation as a leader in environmental initiatives and stewardship, the project will emphasize environmentally friendly design and technology.

Objective:

- Encourage the use of renewable energies such as solar and wind power;
- Effectively manage surface water, encouraging groundwater recharge and preventing erosion;
- Encourage the use of renewable, local and durable materials;
- Minimize light pollution;
- Encourage and support recycling within the waste management system through the use of suitable receptacles and their appropriate location and frequency.
- Strengthening the urban forest
- Promoting active transportation

PART

04 DESIGN AND RECOMMENDATIONS



4.0 Streetscape Concept

From the preceding research, the intent of the Master Plan/Streetscaping Plan is to create a functional and integrated space which draws businesses and residents to the core area to participate in day to day functions and services as well as create an adaptable area to encourage public interaction and programming within the Business Improvement Area.

Included in this section of the report is a description of the following proposed elements to create a functional streetscape for Hespeler Village.

1. To create seamless connectivity to trail systems and parks to draw residents to the core area and make the core area more walkable and accessible.
2. To create a unified streetscape design, including recommendations for durable treatments and hardware.
3. To identify opportunities and guidelines for redevelopments within the study area
4. Enhance the heritage of the existing core area through Architectural Design Guidelines

The large-scale overall Hespeler Master Plan/Streetscaping Plan can be found in full size format in the rear pocket of this report.



LEGEND

	PROPOSED LIGHT STANDARD LOCATION
	PROPOSED TREE SETTING TREE SPOTS ON SIDEWALK
	PROPOSED TREE SETTING TREE SPOTS ON SIDEWALK
	PROPOSED TREE SETTING TREE SPOTS ON SIDEWALK
	PROPOSED BENCH LOCATION
	PROPOSED BOLLARD (ON SIDE PARKING MARKERS)
	PROPOSED BIKE RACK LOCATION
	PROPOSED SEAT WALL LOCATION
	PROPOSED TREE SETTING TREE SPOTS ON SIDEWALK

DATA CHART

EXISTING NUMBER OF PARKING SPACES ON ADJACENT STREET	IN SPACES
PROPOSED NUMBER OF PARKING SPACES ON ADJACENT STREET	1.000
PROPOSED NUMBER OF PARKING SPACES ON ADJACENT STREET	1.000
PROPOSED NUMBER OF PARKING SPACES ON ADJACENT STREET	1.000



HESPELER VILLAGE
STREETSCAPE PLAN
CAMBRIDGE, ONTARIO
SCALE - 1:500
August 27, 2013
File Number: 32689L



4.1 Traffic Circulation

Traffic Circulation within the Hespeler Core has been evaluated through a high level planning context. The proposed changes to the circulation patterns should be evaluated by transportation engineers to ensure intersection movements can handle the proposed traffic movement patterns (specifically at the Queen Street, Adam Street, Guelph Street intersection). Information such as traffic counts at peak hours and collision data should be provided to the traffic engineer/consultant at the time of the study to ensure an accurate analysis is completed.

Queen Street East and West

Traffic patterns moving East and West along Queen Street flow generally well. The existing roadway consists of two lane traffic movement running East and West with turning lanes at the main intersection of Queen Street and Guelph Street/Adam Street. On-street parking varies in location from the North Side to the South side.

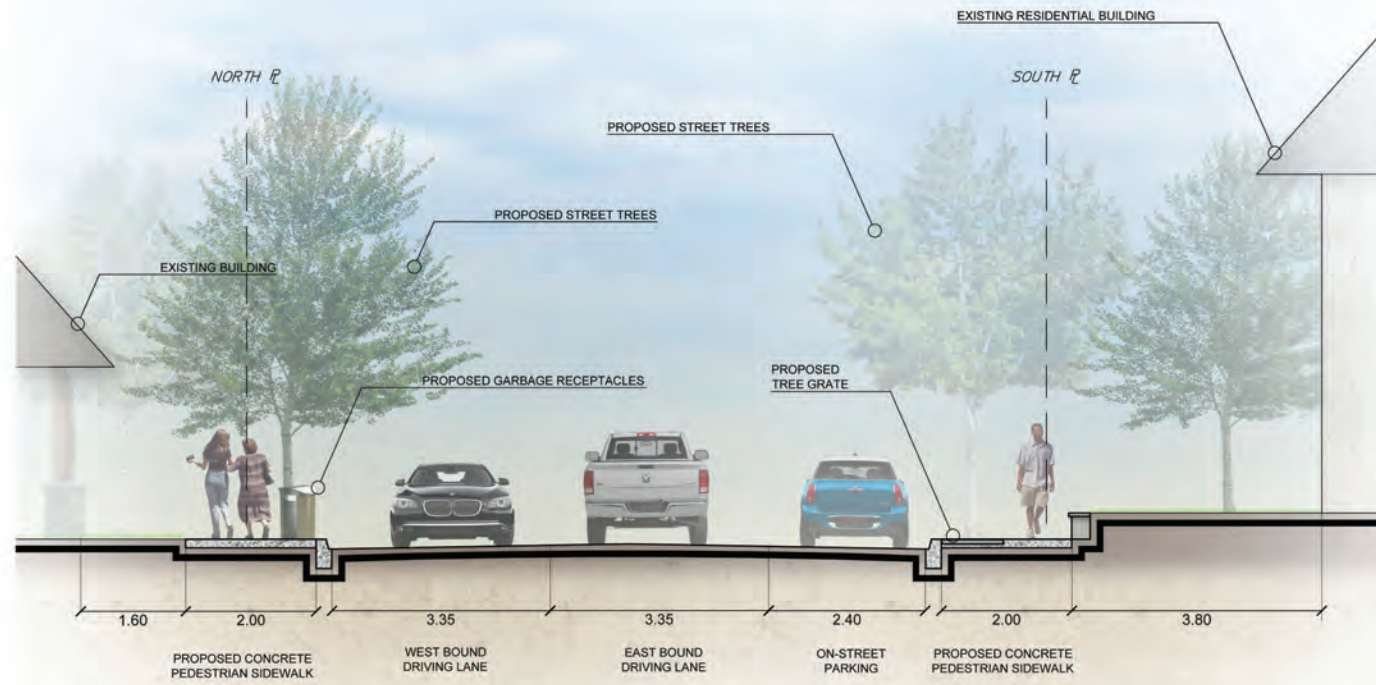
The proposed traffic patterns on Queen Street East and West generally remain consistent with the existing movement. The design recommends creating a more linear roadway, by moving all on-street parking to the south side of Queen Street. This will eliminate the need for traffic to move in and out of the location of parked vehicles and allow for a more consistent pedestrian space/width for the public and business owners to utilize.

The design guidelines for consistent traffic circulation should include:

- Move all on-street parking to the South side of Queen Street East and West
- Design travel lanes to a consistent width of 3.35m
- Design turning lanes to a consistent width of 3.0m
- On-Street Parking to be sized appropriately to 2.4m x 6.5m



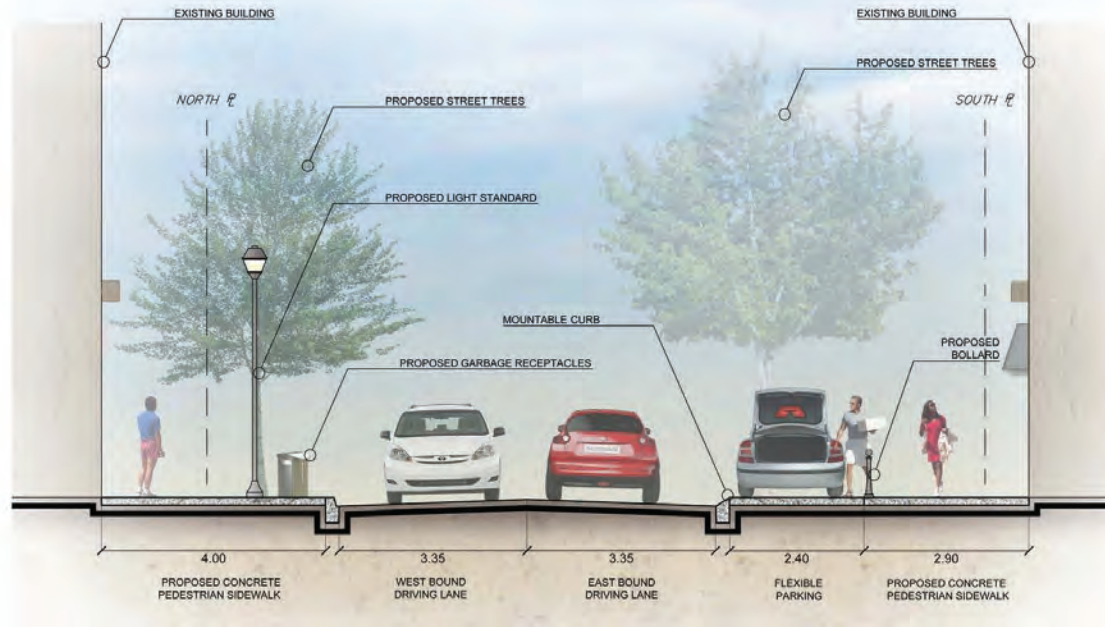
QUEEN STREET AND HARVEY STREET





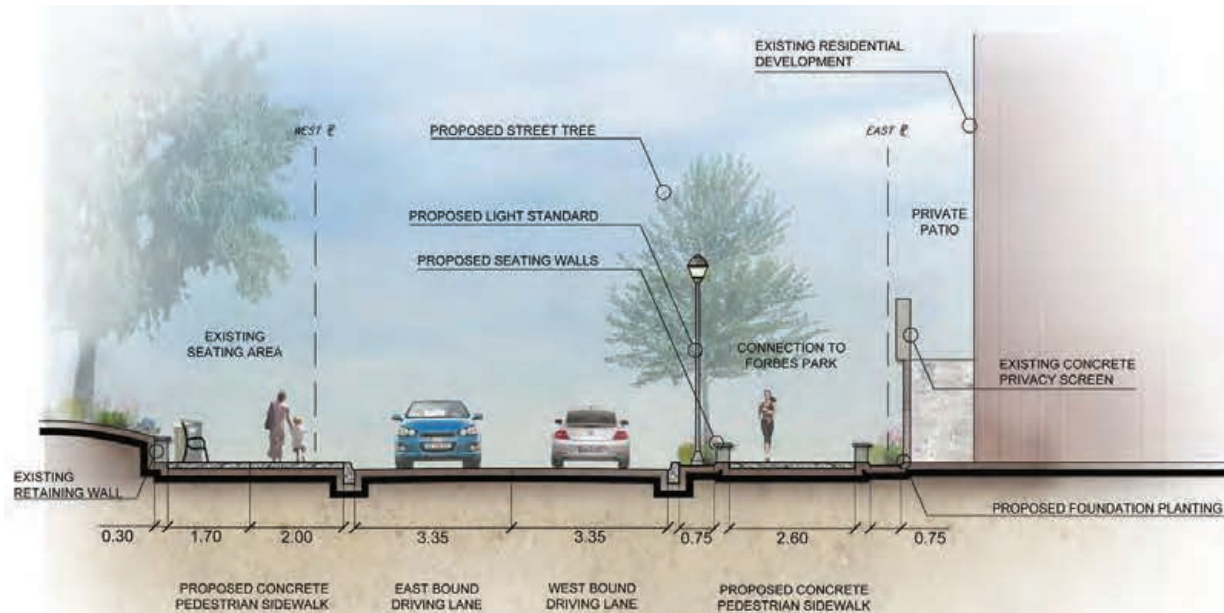
QUEEN STREET

SCALE - N.T.S.



TANNERY STREET AND QUEEN STREET

SCALE - N.T.S.



SPEED RIVER

A SINGLE VEHICULAR DRIVING LANE IN
EACH DIRECTION IS TO BE 3.35m WIDE

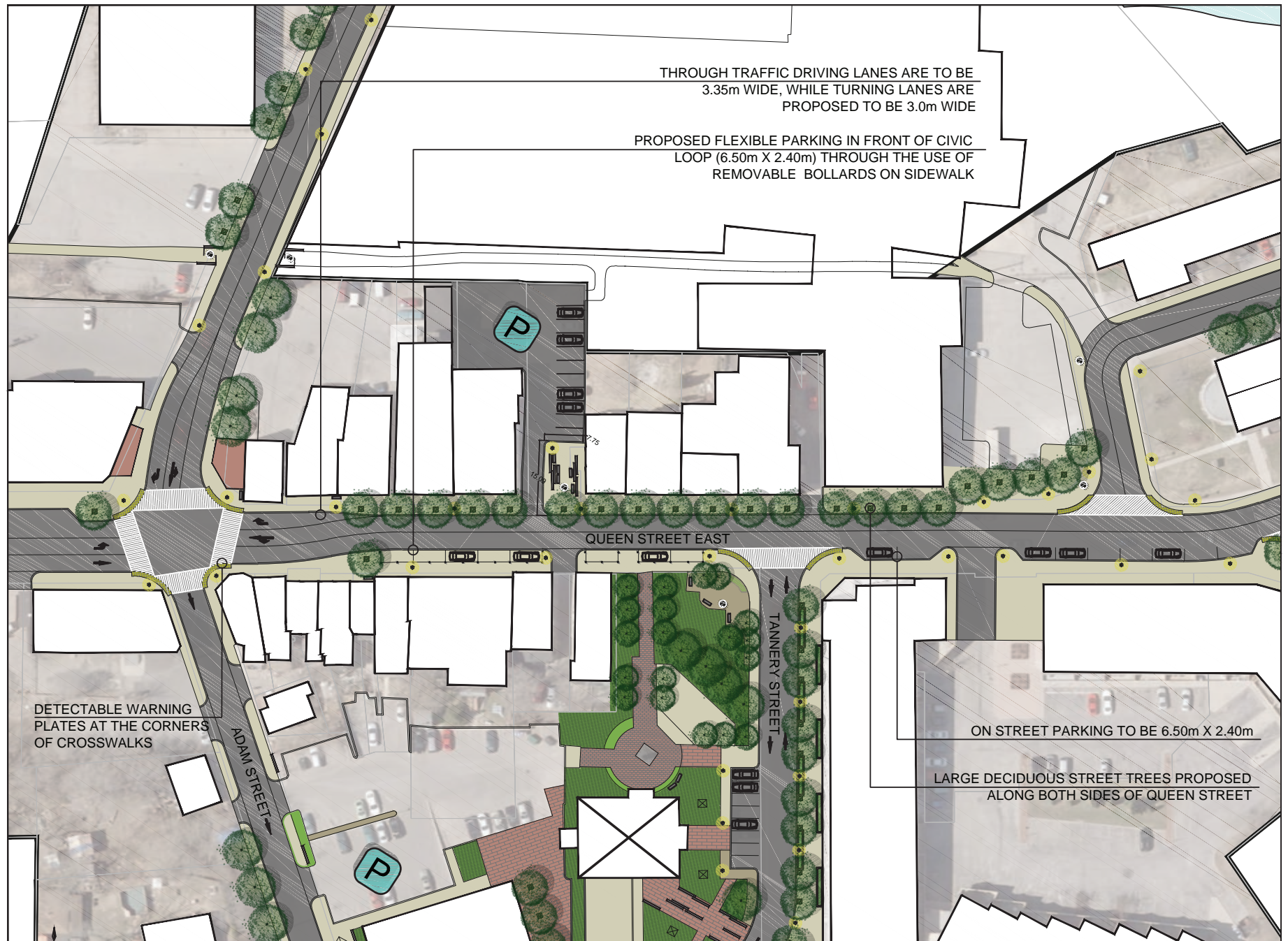
PARKING SPACES TO BE 6.50m X 2.40m
AS PER CITY OF CAMBRIDGE GUIDELINES

QUEEN STREET WEST

QUEEN STREET WEST

PARKING HAS BEEN RELOCATED TO ADJACENT SIDE OF QUEEN
STREET ALLOWING ALIGNMENT OF CORRIDOR TO DOWNTOWN

QUEEN STREET WEST - STREETSCAPING DETAIL AND ALIGNMENT



QUEEN STREET EAST - STREETSCAPING DETAIL AND ALIGNMENT

City/ Civic Loop

Adam to Tannery Street East, Queen Street

The study area has traffic circulation heading south (one way) down Adam Street from Queen St. to the Hespeler Public Library parking lot which then quickly turns into a two-way roadway at the parking lot entrance. Two way traffic is also found on Tannery Street between Adam and Queen Street.

The changes to the proposed “City/Civic Loop” on Adam Street and Tannery Street, will allow for a consistent streetscape design that will create wider pedestrian travel ways to access the civic/institutional buildings and Forbes Park. Please refer to Section 4.2 of the document to review how the pedestrian travel ways will be addressed.

The design guidelines to create the City Loop are as follows:

1. Design one way travel lane at 6.0m in width for the one way portion of Adam Street to the Hespeler Public Library Entrance. Transition the 6.0m lane width at the entrance to the Hespeler Public Library to a 7.5m lane width to accommodate the two way traffic flow into library.
2. Re-design Tannery Street to a consistent width of 7.5m with 1.8m sidewalks on both sides to accommodate proper two way traffic flow and emergency vehicles. Push travel lanes to the South within the right of way to maximize the pedestrian area adjacent to the Hespeler Public Library, Fire Hall and Historical Society.
3. Push the two way travel lane for Tannery Street towards the West of the right of way heading north and south to maximize the pedestrian area for a grand entrance towards Forbes Park. Details of the pedestrian areas along Tannery Street are described in Section 4.2 of this report.
4. Provide proper signage to direct vehicular traffic through “City/Civic Loop”.

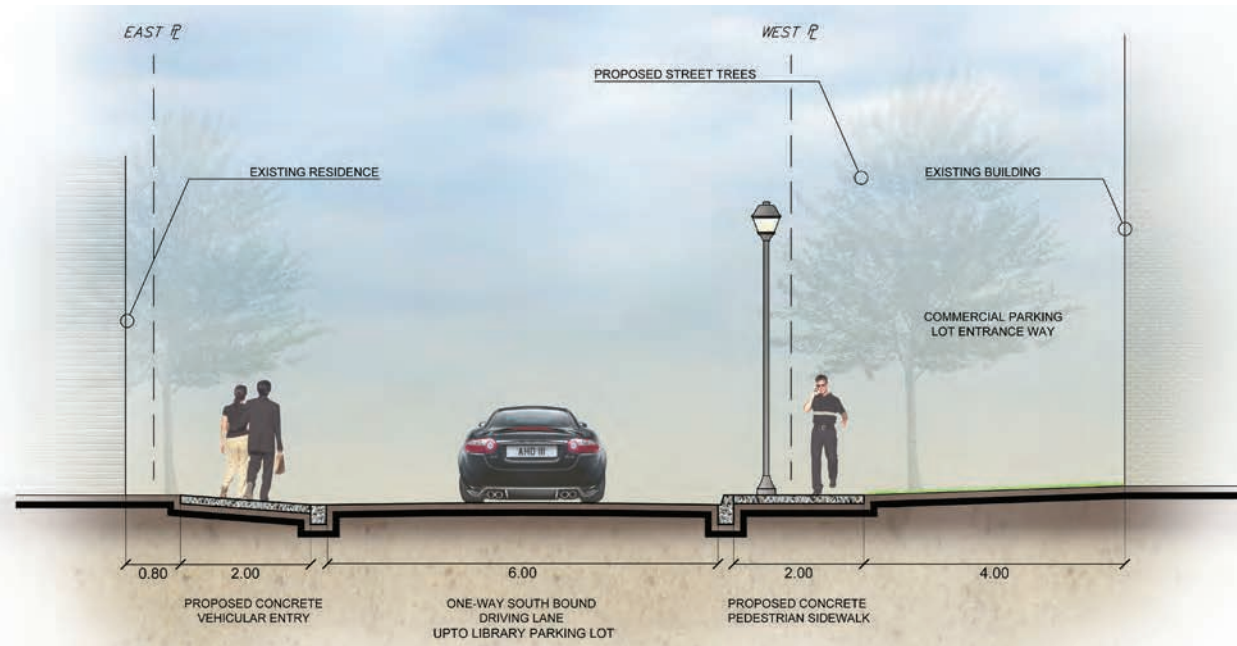


CITY/CIVIC LOOP STREETSCAPING DETAIL AND ALIGNMENT



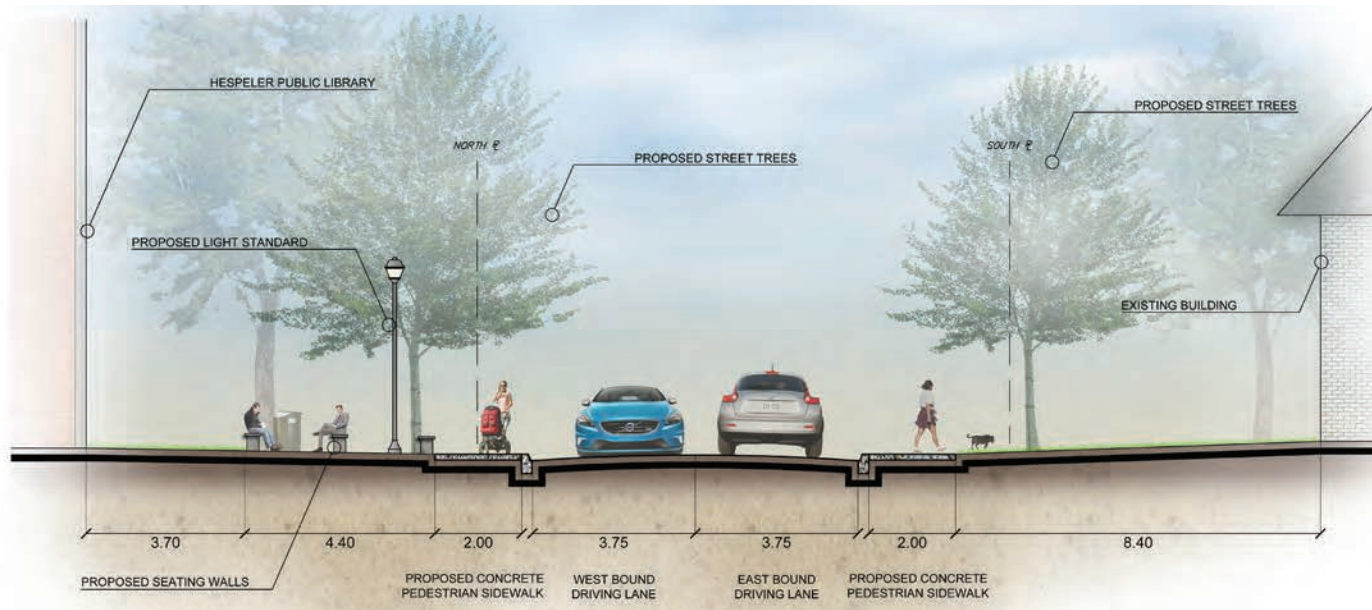
ADAM STREET AND
QUEEN STREET

SCALE - N.T.S.



TANNERY STREET

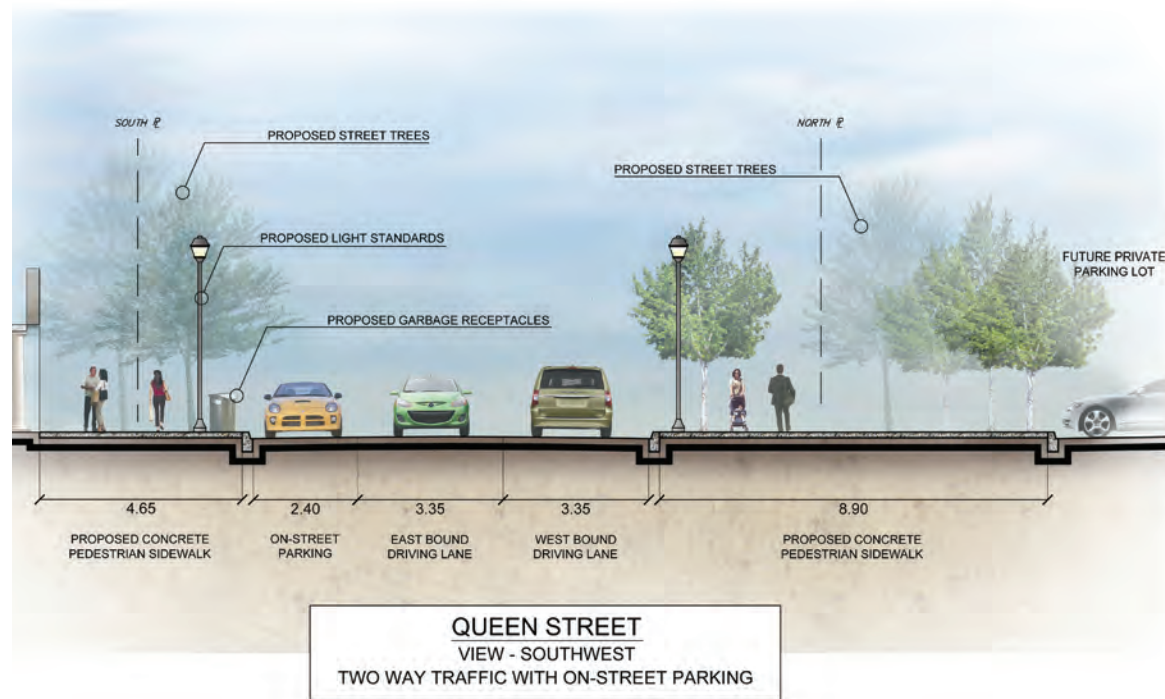
SCALE - N.T.S.





QUEEN STREET AND
CHAPEL STREET

SCALE - N.T.S.



SECTION E - E'

SCALE - 1:50
All Dimensions are
measured in Meters

4.2 Pedestrian Circulation

Existing Conditions:

One of the major issues identified at the stakeholder meeting and public meetings was to identify opportunities for connections from the surrounding neighbourhood communities to the Urban Core/Study Area.

Pedestrian Circulation has been evaluated in two forms for Hespeler Village:

1. Connectivity of the Mill Run Trail to the Urban Core Area;
2. Movement of pedestrians to amenities within or adjacent to the Urban Core Area (all businesses, services, civic/institutional, parks, trails and river.)

The Guiding Principles and objectives to create an active and vibrant village destination will be addressed through the design guidelines for the pedestrian circulation and connections. The proposed guidelines will encourage and enhance a safe and quality pedestrian realm, integrate and enhance the existing public gathering spaces, recommend barrier-free elements and provide signage and wayfinding throughout the study area to adjacent amenities (both public and private).

GOAL: To create a seamless and universally accessible connection for pedestrians to access amenities, public spaces and businesses.

Connection of Queen Street to Forbes Park (Via Tannery Street)

A major issue identified as part of the stakeholder meeting and from the survey respondents was to provide for a better, more prominent connection to Forbes Park from the Core Area. Providing a more identifiable connection to the park will draw more users to the park and to the Core Area.

Forbes Park is located in the south west corner of the Core Area with an entrance onto Tannery Street East. The park is utilized for many programmed activities facilitated by public and privately owned businesses and community groups. To create the connection and utilize existing publicly owned land, plans suggest moving the travel lanes to the west on Tannery Street East to create additional pedestrian space for a grand allée connection from Forbes Park to Queen Street.

In an effort to address the Guiding Principle and Objective to create an Active and Vibrant Village Destination, by encouraging and enhancing a safe and usable public realm and enhanced public gathering spaces, the conceptual design also recommends patterned surface treatments up to the limits of the travelled portion of the road on either extending from the entrance of the park across the pedestrian way and other the other side of the street extending the same surface treatment to provide access directly to the civic buildings (Heritage Centre, Publicly Programmable Space, Fire Hall and Public Library).

To make the connection of the “Grand Allée” between Queen Street and Forbes Park a success, the details of the design should include:

1. Maintaining two way travel lane on Tannery Street East.
2. Widen pedestrian space to 5.0m and provide hardscaped banding along the grand allée mixed with concrete sidewalk from Queen Street, connecting directly to the entrance of Forbes Park.



CONNECTION OF QUEEN ST E. TO FORBES PARK

3. Line the Pedestrian Amenity Area with numerous street trees and pedestrian scale lighting and benches.
4. Provide landscaping treatments adjacent to the existing apartment building to soften the presence of the building facade which exists against the current pedestrian environment from the private higher density development.
5. Provide wayfinding signage to identify the connection between Forbes Park and Queen Street amenities and businesses.
6. Roadway and pedestrian lighting improvements.
7. Ensure proper lane widths of 3.35m for turning movements for Emergency Vehicles from Fire Hall and public transit.

To create the entryway from Forbes Park to the Civic Centre, the following guidelines are recommended:

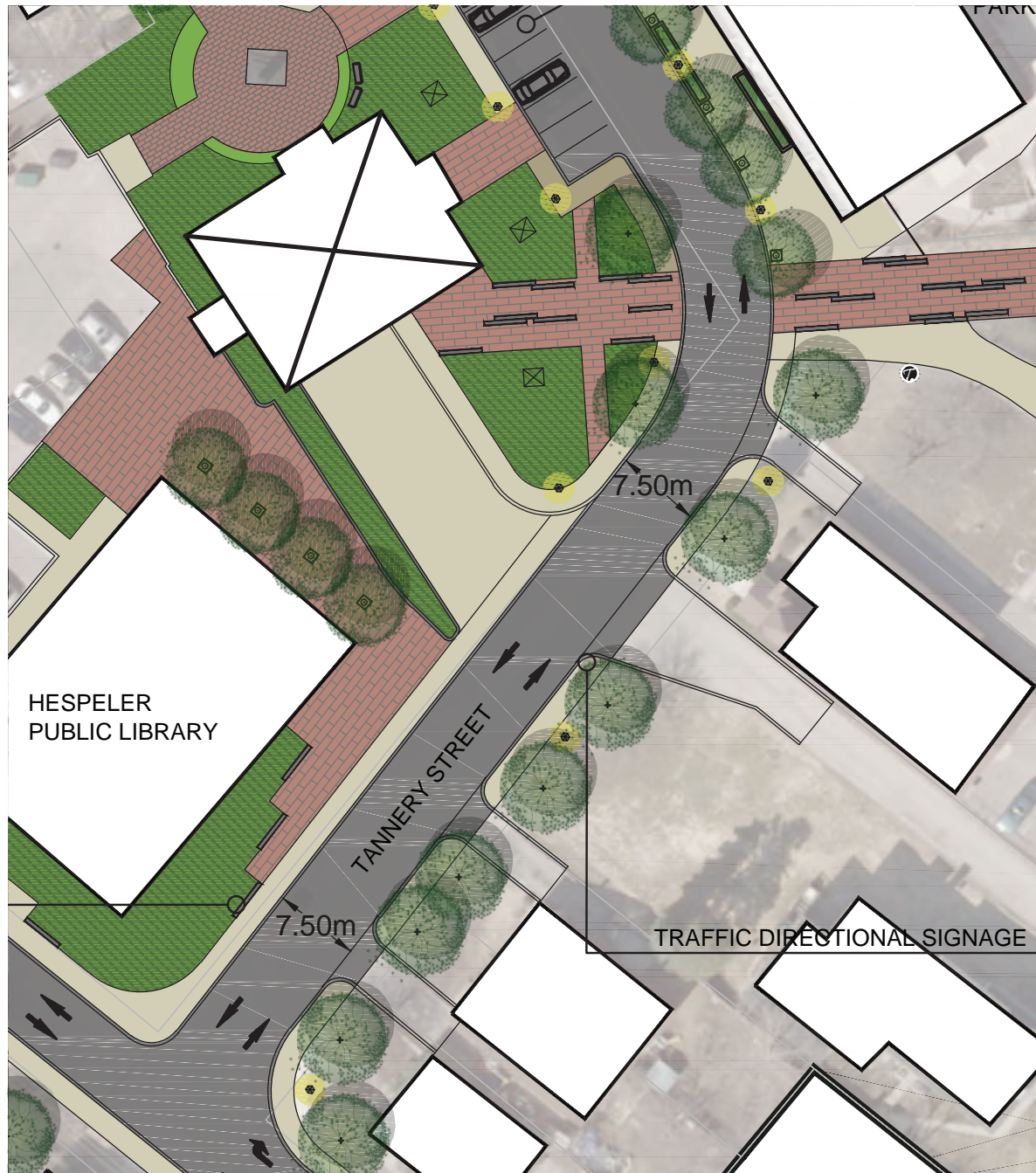
1. Provide hardscaped banding mixed with concrete sidewalk approximately 6.0m wide, along the park entry, and within the pedestrian realm to direct pedestrians to the entry of the Historical Society building. The hardscaped treatments will provide uniformity along either side of Tannery Street, which identifies clear pedestrian routes into the park and city/civic loop. Hardscape treatments shall not cross within the travelled portion of the roadway to eliminate any perceived pedestrian crossing.
2. Add pedestrian lighting at entrance of Forbes Park
3. Provide way-finding signage at park entrance to direct users into the park and into the core area for access to businesses and public programmed spaces. Signage to be consistent with the City of Cambridge Core Area Signage.

Tannery Street (Eastbound - From Adam Street to the Fire Hall)

For the section of Tannery Street East that runs east bound towards the entrance to Forbes Park, it is recommended to keep the laneway as a 2 lane roadway (7.5m in width) and extend the two lanes just past the driveway to the Fire Hall for emergency access, and add an additional 4.0m (approximately) to the north of the street adjacent to the Hespeler Public Library and Fire Hall, which will allow for a large additional public pedestrian realm.

To create the additional public pedestrian realm in front of the Library and Fire Hall, the detailed design should include:

1. Extend the minimalist landscaping design from the frontage of the library towards the public sidewalk, but add some staggered low rise planter beds with seat walls to define a usable public space.
2. Some concrete public amenity area may be planned abutting the Hespeler Library with the area defined by the planter beds and seat walls. Extend the same along the length of the public library on Tannery Street, with staggered seat walls and planter beds towards the parking lot.
3. Maximize the sidewalk area on the north side of Tannery Street for pedestrian area and access to Forbes Park
4. Shade Trees in the public amenity area.
5. Add Waste and Recycling Receptacles.
6. Provide wayfinding signage to public amenities (such as the Hespeler Library, Historical Society and Forbes Park) in accordance with the City of Cambridge Core Area Signage policies..
7. Add proper signage for traffic control.
8. Identify opportunities for local/interactive public art in public amenity area



EASTBOUND TANNERY STREET DETAIL

Adam Street

As a result of public consultation and the advice of City Staff - Adam Street will remain as status quo for traffic operation. The street should be rebuilt to proper width and sidewalks rebuilt for proper accessibility upto the Library.

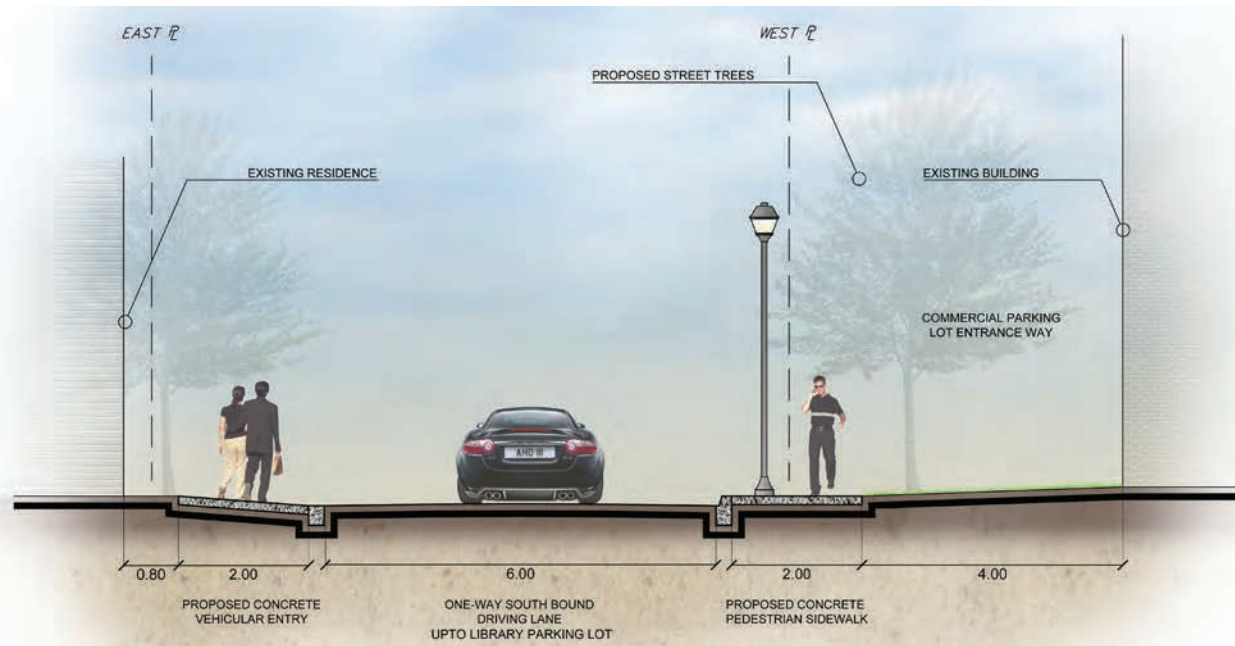
The details of the design of Adam Street should include:

1. Where the road is currently only a one way road, the travelled portion of the road should be narrowed to 6.0m in width which will still provide sufficient width for emergency vehicle use.
2. Widening the boulevard portions of both sides of the road to provide a sidewalk with hardscaped banding and tactile guide strips at the intersection while increasing the amount of landscaped boulevard space. Provide a concrete pedestrian space adjacent to the Public Library with minimalist planting beds with staggered seat walls. Planter beds/seat walls will provide a defined public space with a transition into the public/private parking lot.
3. Provide street trees adjacent to the public parking lot to minimize views from residential neighbours and pedestrians.
4. Add way finding signage (in accordance with the City of Cambridge Core Area Signage Policies) at the intersection of Queen Street and Adam Street to direct pedestrian and vehicle traffic to the public amenity areas.
5. Add way finding signage (in accordance with the City of Cambridge Core Area Signage Policies) at the intersection of Adam Street and Tannery Street to direct users of the civic buildings to the downtown core on Queen Street and to Forbes Park.
6. Parking lot accesses should be designed to be no wider than 6.1m at the property line with appropriate widening/radii out to the curb line for safe egress while maximizing pedestrian space.
7. Screen parking lots with vegetation to create a softer streetscape along Adam Street to focus on the pedestrian realm and remove focus from the automobile.
8. Provide adequate lighting for pedestrian and vehicle movement.



ADAM STREET AND
QUEEN STREET

SCALE - N.T.S.



ADAM STREET - SOUTHBOUND

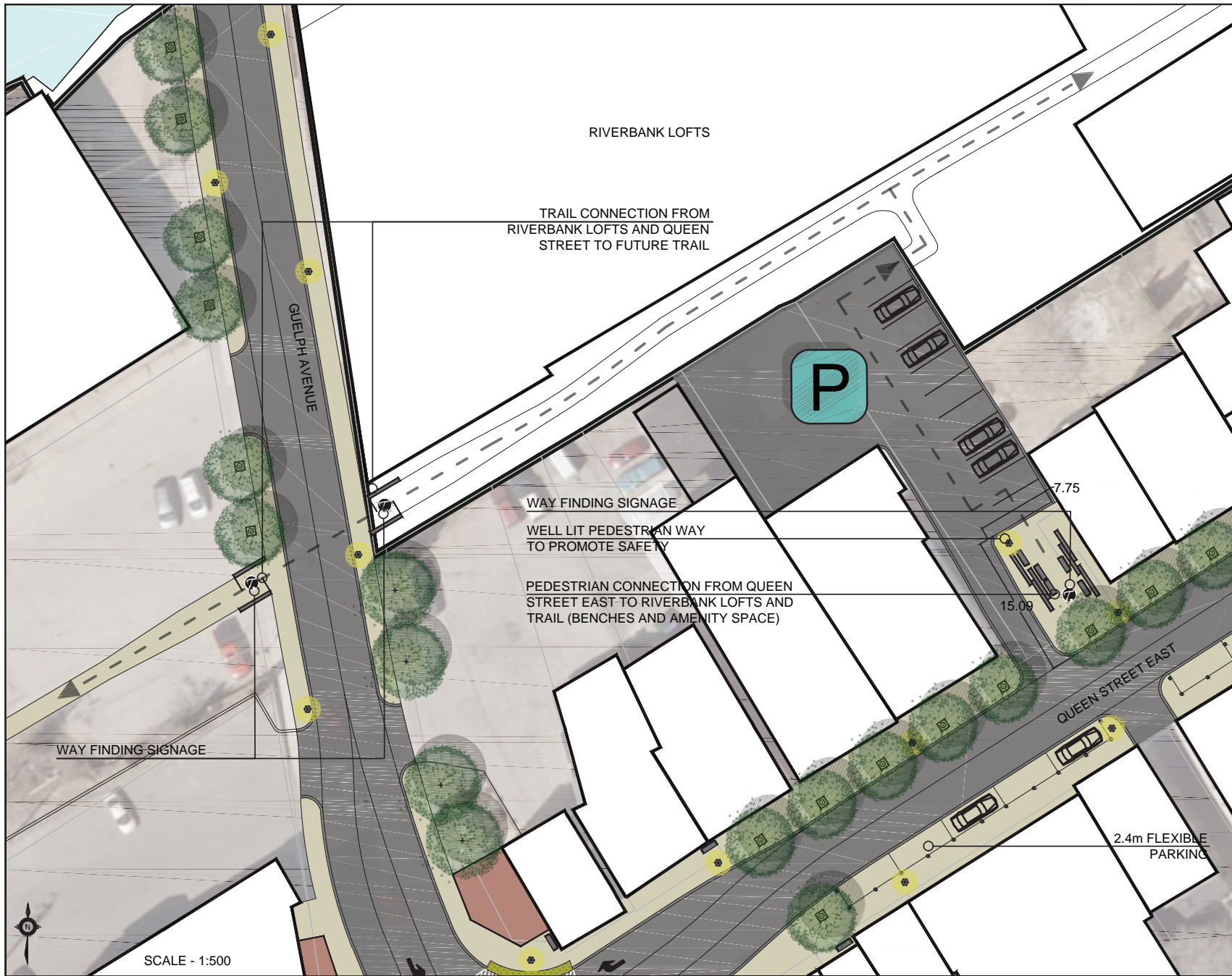
Connection to Riverbank Lofts and Former Rail Line

Approximately 200 residential condominium units are planned to be built at the Riverbank Lofts as part of a Phase I development. The 200 additional units within the core area could bring almost 400 new residents to the area, which will increase the demand for services and amenities in the heart of Hespeler Village. The proposed plans provide for an access connection to the 200 units directly to Queen Street. To ensure the future residents utilize the urban core, the City should provide direct connection through the City owned land, so that the development feels directly linked to the core area, forming an interconnected bond between the high density residential development and the services and amenities offered directly in the core and also providing a connection to the Rail Line trail which runs parallel to the south bank of the Speed River.

The connection that is planned for the Riverbank Lofts to the Urban Core Area, should be designed for universal accessibility with consideration for all modes of transportation, including walking, cycling and for those with barrier free needs.

Details for creating the Riverbank Loft connection to the Core Area include:

1. Well lit pedestrian way for safety through City owned lands to the Riverbank Lofts Development.
2. The pedestrian pathway should be consistent in design with other pedestrian connections in the Village Core.
3. Creating a partnership opportunity with the developer to create a gateway into the Riverbank Loft Community
4. Way-finding signage (in accordance with the City of Cambridge Core Area Signage Policies) in the core area to identify the connection to Riverbank Lofts and to the Core Area from the development.



Connection to Jacob's Landing and Mill Run Trail

The Mill Run Trail runs parallel to the Speed River from Hespeler Road in the west and through Hespeler Village providing a direct trail connection through to Preston. Within the study area, the trail currently runs along the north side of the Speed River (west of Guelph Street) and then moves onto Sheffield Street and then connects back on the East side of Guelph Street to a lookout point across to the dam.

The trail running along Sheffield Street causes a void along one of the most important views within the Hespeler Study Area. The Master Plan design recommends connecting the trail through the Milling Road industrial area onto Milling Road (a future pedestrian only area – discussed in Section 1.3.4 of this Report) and continuing the trail running parallel along the Speed River with a connection to Jacob's Landing and the lookout at the east side of Guelph Street.

To create this connection – the following design guidelines are recommended. Please note that the Grand River Conservation Authority should be consulted through the creation of the connection.

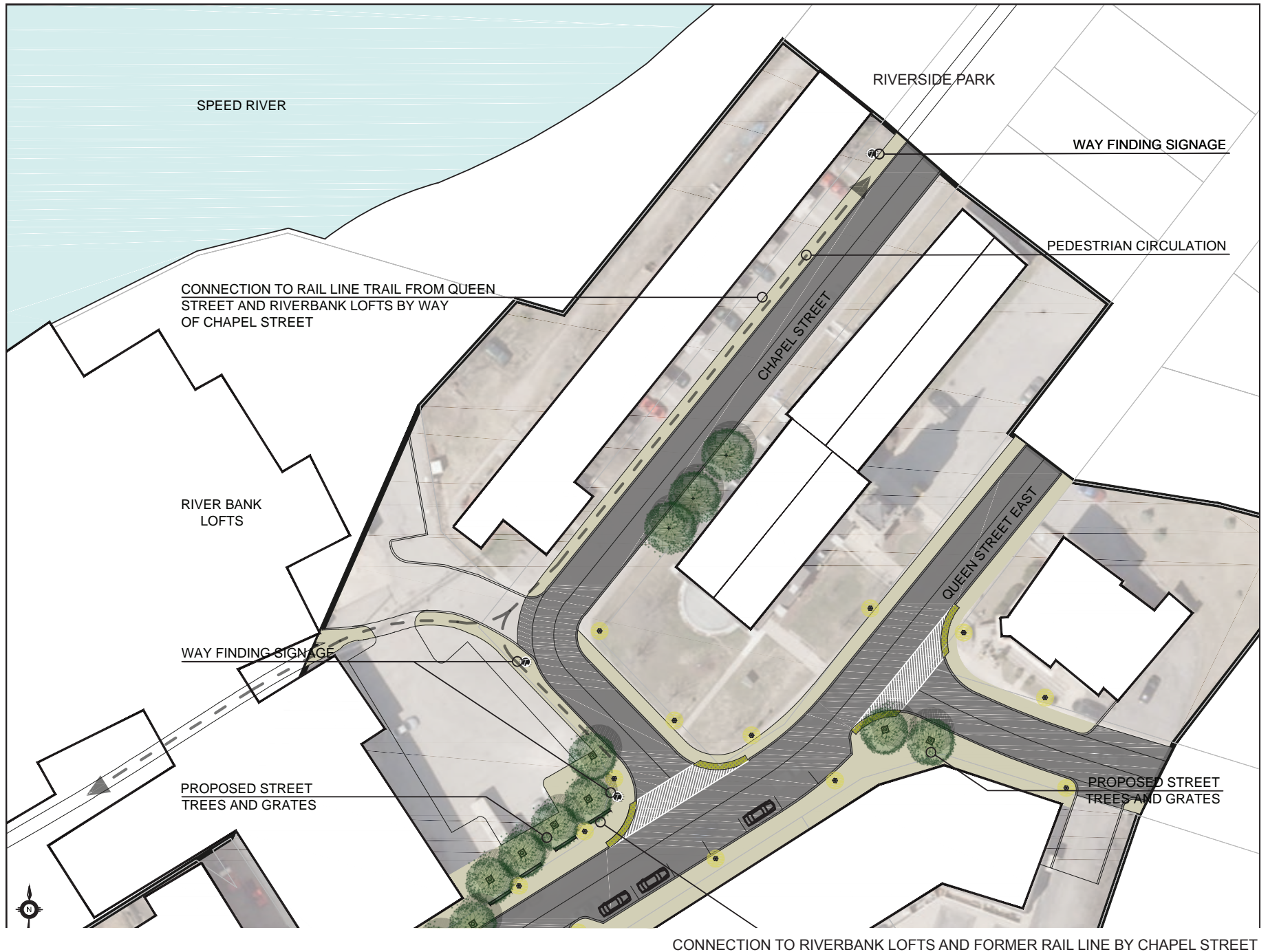
1. Close Milling Road to become a pedestrian only space (as an extension to the Mill Run Trail). Limited vehicle access will be permitted for deliveries (if required) and maintenance vehicles.
2. Upon adaptive reuse of the Milling Road industrial area, encourage the developer to provide a trail connection to the Mill Run Trail and Milling Road
3. Provide way-finding signage at the trail head and entrance of Milling Road to direct users to amenities within the Milling Road development (including Jacobs Landing) and towards the core area and public programming spaces.
4. Continue to work with the rail companies to determine if providing an extension to the Mill Run Trail to the north to connect new subdivisions developed in Hespeler is feasible. Families who live in these neighbourhoods have expressed interest in having a better connection to core amenities.

Connection to Rail Line + Queen Street by way of Chapel Street

There is an existing trail which connects to Spring Street running parallel to the Speed River, connecting into the East Side of the Study Area and lookout point. The conceptual design has identified an opportunity to connect this trail at Spring Street, and provide access to Queen Street and a connection to the former "Spur Line" trail proposed to run through the Riverbank Lofts Development.

To create the connection to the Rail Line Trail to Queen Street and the Riverbank Lofts, the following Guidelines are recommended:

1. Provide identifiable way-finding signage at trail head at the end of Spring Street (Riverside Park, Hespeler)
2. Connect trail directly to north sidewalk on Spring Street
3. Provide a connection to the private development (Riverbank Lofts) with way-finding signage to provide direction to the trail head or core area.
4. Provide way-finding signage at the corner of Chapel Street and Queen Street to provide direction to the trail connection and core area amenities.
5. Provide additional landscape treatments to connect to the trail head on Chapel Street and Queen Street.



4.3 Redevelopment Opportunities

The majority of properties within the Urban Core Character Area (which are not civic or institutional uses) are zoned for Mixed Use Development (with Commercial and Residential Uses) to be built at a minimum of 6.0m in height. Any redevelopments in this character area are not subject to any parking requirements or loading requirements to create compact urban development. (Please refer to Zoning Figure Z-6 from the City of Cambridge Zoning By-law, 150-85, as amended).

The Urban Core currently has a generally compact form of mixed uses varying from two to three storey buildings. Heritage Designated buildings and Properties of Interest have been identified within the character area, with a list described within the Architectural Guidelines (Section 2) of this report.

The buildings in the Urban Core (between Guelph Avenue/Adam Street to Chapel Street), are generally stable and no parcels of land have been identified as an opportunity for redevelopment at this time




Although no major redevelopment is recommended, the conceptual design provides a recommendation to City Staff and the Hespeler Business Improvement Area to promote the Façade Improvement Program to all business owners and follow the recommendations provided by the Architectural Design Guidelines within this report to improve the facades of the privately owned buildings. Findings through the Public Consultation process have identified that many business and building owners are not aware of the program, and if the core area needs improvement, it will take investment by the business owners to utilize the Façade Improvement Program and Design Guidelines to improve the Urban Core Area.

The Grand River Conservation Authority (GRCA) should be contacted for any properties planned for redevelopment or site improvements as they may be located within the Grand River Conservation Authority Regulated Area and Flood Plain, however, may require a development permit or a limitation on permitted uses



Map Z6 - City of Cambridge Zoning By-law

Legend

-  Area where no off-street parking or loading are required (See Sections 2.2.2.9 and 2.3.1)
-  Area where required non-residential parking may be reduced by 25% and residential visitor parking does not have to be provided. (See Sections 2.2.1.1(b) & (d) & Section 2.2.2.10)
-  Area where Commercial may be substituted for Industrial site development specifications. (See Section 3.4.1.6)

Queen Street Redevelopment Opportunity

The existing development along Queen Street to the West of Guelph Avenue and Adam Street consists of generally low density residential development, some converted dwellings and a mix of commercial units.

South Side of Queen Street

On the South side of Queen Street closer to Adam Street are newer three storey mixed use developments that are stable. There are some areas of opportunity for redevelopment (16 Queen Street West). The City of Cambridge should consider the following design guidelines for future development along the South side of Queen Street (West of Adam Street).

1. Continue with the minimum two storey building height as the Zoning Requirement in the Core Area.
2. To create continuity along the south side frontage, it is recommended that all development be consistent with the building line of the anchor building located on the south side of Queen Street (West of Adam Street). This will create a unified street massing and a consistent/even pedestrian realm.
3. Encourage shared driveways between abutting properties to rear lot parking areas to maximize on street parking along Queen Street and eliminate multiple entrances for safety.
4. Parking to be located at rear of development.
5. Respect the massing of adjacent new developments and anchor buildings.
6. Universally accessible entrances from Queen Street.
7. Follow the Architectural Design Guidelines included in Section 2.0 of this report for details on suggested building design, scale, signage detail, orientation, etc.
8. Buildings of architectural or heritage value should be preserved and integrated into the streetscape. Refer to the Section 2.0 for Architectural Design Guidelines

North Side of Queen Street

The North Side of Queen Street West (west of Guelph Avenue) has many redevelopment opportunities. With Ernie's Restaurant being an anchor building on the north side of the street, it is easy to vision the remainder of the study area down Queen Street to respect the scale, massing and architectural treatments as established on the existing anchor building.

The North Side of Queen Street also offers future developments a waterfront view which makes the parcels of land a premium for development and businesses.

The following design guidelines are recommended for the development along the north side of Queen Street (west of Guelph Street).

1. Continue with the minimum two storey building height as the Zoning Requirement in the Core Area.
2. To create continuity along the north side of Queen Street West, it is recommended that all development be consistent with the building line of the corner anchor building (Ernie's) located on the north side of Queen Street (West of Guelph Street).
3. Design buildings to face onto Queen Street, but a consideration to views towards the Speed River for both residential and commercial uses is encouraged. This could include: patios, balconies, amenity areas or providing connections directly to the unsanctioned pedestrian trail to the North (running adjacent to the Speed River).
4. Encourage shared driveways between abutting properties to maximize on street parking along Queen Street and eliminate multiple entrances for safety.
5. Parking to be located at rear of development, but buffered with native vegetation from the view of trail users utilizing the old rail line pathway or the Mill Run Trail, to create an organic transition towards the waterfront.
6. Universally accessible entrances from Queen Street.
7. Respect the massing of adjacent new developments and anchor buildings.
8. Follow the Architectural Design Guidelines included in Section 2.0 of this report for details on suggested building design, scale, signage detail, orientation, etc.
9. Buildings of architectural or heritage value should be preserved and integrated into the streetscape. Refer to Section 2.0 for Architectural Design Guidelines



Future Development by Riverbank Lofts Developer (West of Guelph Street and South of Speed River)

On the west side of Guelph Avenue is a parcel of land currently owned by the developer of the American Standard (Riverbank Lofts) development. It is recommended that any redevelopment on this site take into consideration the views and vistas of the surrounding area (with the Speed River directly adjacent to the site) and that the developer also work with City Staff to provide a connection to the unsanctioned trail through the subject property. Any parking lot which is designed as part of the redevelopment should be screened with landscaping treatments from Guelph Avenue to remove the dominance of the automobile and encourage an organic connection to the bridge/waterfront and to Queen Street.

1. Consult with Grand River Conservation Authority with respect to any future plans for the property.
2. If the building is to remain on site, consider providing window treatments and landscape treatments which provide views up and down the Speed River and to the neighbouring revitalized Riverbank Lofts.
3. Consider adding bio-swales with seat walls to help filter runoff prior to draining directly into the Speed River.
4. Provide additional landscaping treatments to shield the view of any parking lot from those travelling on Guelph Road.
5. Provide connection and way finding signage to connect the rail line trail through the development to the existing trail connection running parallel to the Speed River.



Former American Standard - Future Riverbank Lofts

Milling Road/River Front Revitalization

Existing Conditions:

The Milling Road Industrial lands are an area of great opportunity for adaptive reuse. The site is currently provides services such as yoga, registered massage therapists, studios, etc. The site currently fronts onto Milling Road and has views directly onto the Speed River and the beautifully landscaped, Jacob's Landing. The site has direct access to Guelph Street. Parking is located on street, with some available on site or behind the industrial buildings.

Across Milling Road and sitting on the bank of the Speed River is an industrial building currently owned by the City of Cambridge. It is utilized for storage for maintenance vehicles and replacement parts for City infrastructure. Through discussion with City Staff, future uses are intended to remain the same.

Opportunities for Revitalization:

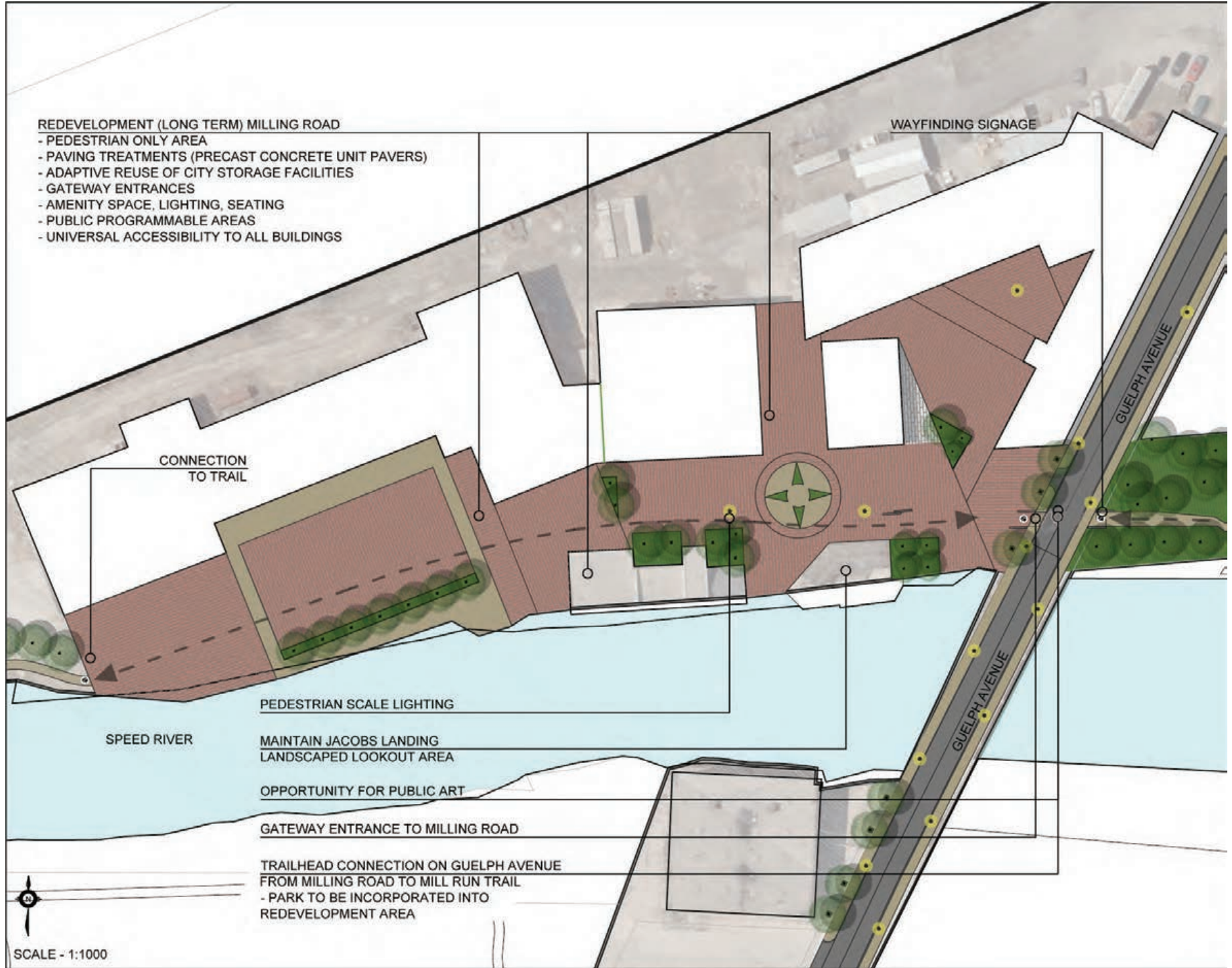
This area of Hespeler has incredible potential as an adaptive reuse revitalization project, which could create a similar atmosphere to the Distillery District in Toronto. City Staff should work closely with the landowner of the Milling Road Industrial lands and the GRCA (as the property is located in the flood way) to make a successful urban space for all residents and visitors to enjoy.



Distillery District, Toronto - Night Life (Google Images)

Design Guidelines for City Owned Land (Milling Rd)

1. Maintain Jacob's Landing
2. Close Milling Road to vehicular traffic between the buildings and the Speed River and create a pedestrian travel zone only
3. Interlocking brick pavers should project from the industrial building fronts across the entire new pedestrian court to connect to Jacob's Landing and the City owned industrial building
4. Reroute the Mill Run Trail which runs down Sheffield Street to run through the privately owned lands and connect to Milling Road.
5. Way-finding signage should be located at the entrance to the Milling Road Industrial Site
6. Consider an Adaptive Reuse to the City Storage building to rent to a local business owner for a coffee shop, ice cream shop, restaurant with patio, canoe rentals etc. Patio space can be provided along the frontage of the Speed River, or looking inward to the pedestrian only plaza.
7. Provide a gateway entrance to Milling Road through landscape or hardscaping treatments.
8. Provide for pedestrian scale lighting and partner with private owner to provide seasonal lighting to create a public space which is operational year round.
9. Identify locations for public art.
10. Provide hardscaped areas for public programming opportunities and festivals.
11. Consider revisiting the Zoning By-law to open up permitted uses from Light Industrial to a mix of uses such as commercial, light industrial and some residential. This will require consultations with the Grand River Conservation Authority as part of the lands are located within the Flood Plain.



Design Guidelines for Privately Owned Milling Road Revitalization

1. Partner with the City of Cambridge to provide consistent hardscaping treatments to create a consistent barrier free plaza across Milling Road. The hardscaped treatment around the entire site, will allow for programming spaces for business owners and for City-run events.
2. Provide a connection to the Mill Run Trail through the site to provide additional access to industrial, commercial and potential residential uses.
3. All parking and deliveries should be provided at the rear of the industrial facility with access off of Guelph Street. When deliveries cannot be made to the rear of the facility, a plan for deliveries could be created to best suit businesses and pedestrian activities. (ex. Sparks Street in Ottawa). The plaza area should remain accessible for emergency vehicles.
4. Provide shade trees, seat walls, benches, bike racks, pedestrian scale lighting, waste and recycling receptacles and way-finding signage throughout the development.
5. Partner with the City of Cambridge to provide seasonal lighting throughout the entire pedestrian zone.



6. Provide locations for public art.
7. Provide outdoor amenity area for businesses to spill out into the plaza (ex. Shops, patio spaces, market areas).
8. Partner with City of Cambridge to create a gateway feature into development.
9. Consider revisiting the Zoning By-law to open up permitted uses from Light Industrial to a mix of uses such as commercial, light industrial and some residential. Must consult with the Grand River Conservation Authority to determine appropriateness of changing the zoning on site.
10. Attempt to keep all access into the former industrial space barrier free.
11. Consult with the Grand River Conservation Authority prior to any redevelopment on site.
12. Provide up-lighting for the facades of the former industrial buildings
13. Follow the Architectural Design Guidelines included in this report to aid in the adaptive reuse opportunity.



Distillery District, Toronto (Google Images)



Distillery District, Toronto (K. Wills Images)



Distillery District, Toronto (K. Wills Images)

Improvements to City Owned Land

Within the “City Loop” is a 0.8ha parcel of land which is intended for use as a public space at the corner of Tannery Street and Queen Street. The Public Amenity Space is under utilized and exhibits issues of topography and shifting soils during rainfall events. The site falls approximately 3.0m to 4.0m over a short period from the Fire Hall/ Historical Society to Queen Street. The ultimate/long term build out plan as described in this document will be completed as funds become available. City Staff to determine short, medium and long term goals for the ultimate design.

GOAL: To improve usage of public space for publicly planned/programmed events and for every day usage and clearer sight lines.

To improve the quality of the public space, the City should consider the following guidelines:

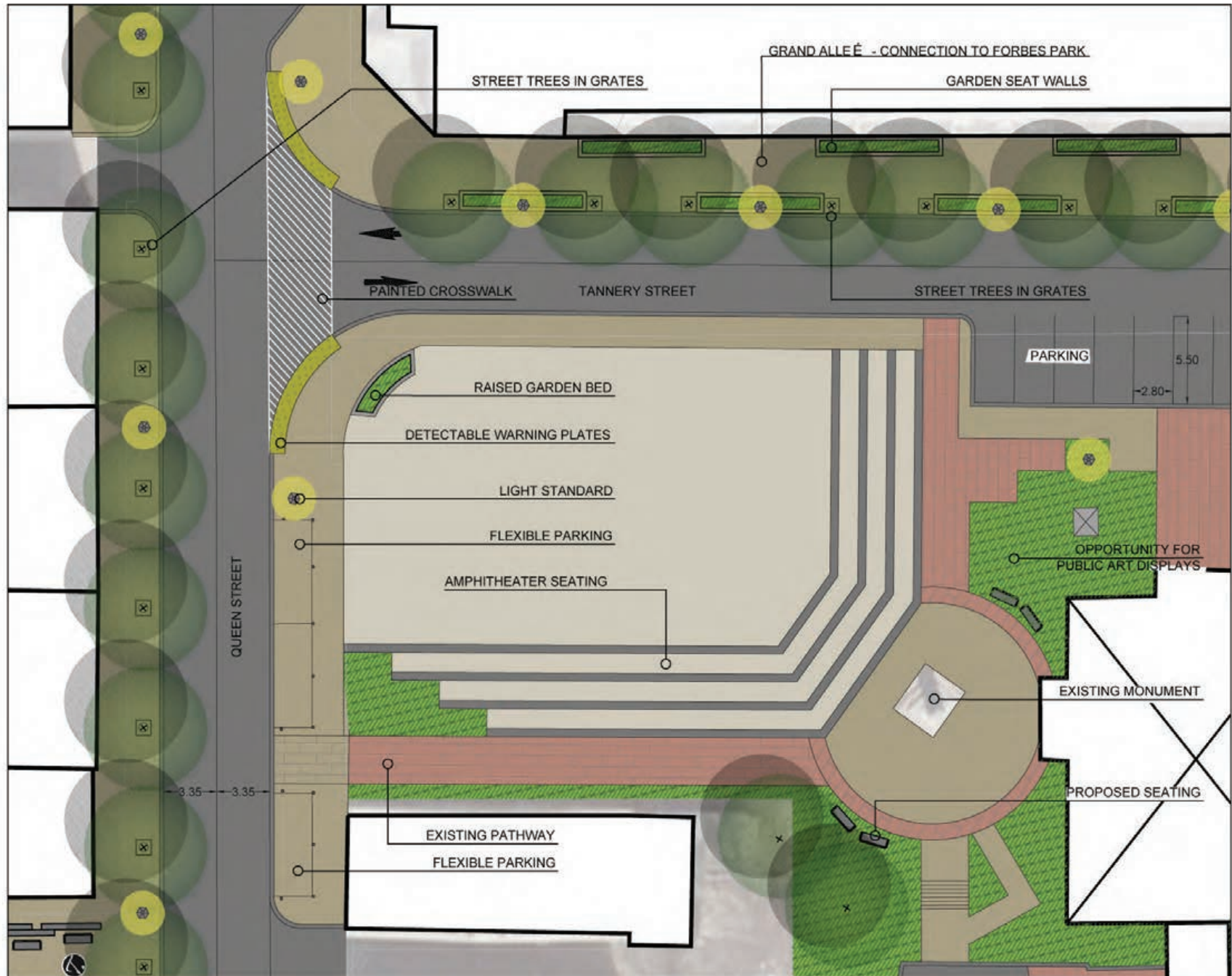
1. Create accessible public space directly adjacent to street right of way at grade.
2. Design with Crime Prevention Through Environmental Design (CPTED) Principles for safety in public space.
3. Consider “hardscaping” treatments (stamped concrete, granite or alternative), with stepping from Queen Street up towards the Historical Society. Also, provide a universally accessible route for users to access public amenity space. Hardscaping the park can provide for more programmable events, including concerts and public gathering spaces, Remembrance Day events, skating rinks, public art, local market area for different vendors, etc. Residents can use Forbes Park as a greener park setting for other events and enjoyment.
4. Provide elements for shade and seating arrangements for public enjoyment.
5. Provide/ identify opportunities for public art.
6. Provide bike racks.
7. Provide waste and recycling receptacles.
8. Way-finding signage to public amenities (including Forbes Park, Civic

buildings and businesses on Queen Street)

9. Provide rolled curbs and alternative surface treatments onto Queen Street to extend programmable space for larger events. The alternative surface treatments will also define the pedestrian crossing from Tannery Street, across Queen Street.
10. Provide lighting for pedestrian area and safety.
11. Provide pedestrian access to each civic building and Forbes Park connection from the programmable area.



Programmable Public Space - Markets, Music, Arts, etc.



FUTURE DEVELOPMENT OPTION - PROGRAMMABLE SPACE (TANNERY AND QUEEN STREETS)

4.4 Streetscape Design

The proposed streetscaping design should be inspired by the guiding principle of High Quality Design. The design guidelines set out in this report provide direction for using materials which will perform in four season conditions, ensure that all street materials will be durable and low maintenance, integrate common elements of the streetscape to reduce clutter, increase sense of place and identify opportunities for planting and consistent landscape treatments to visually unify the street.

On Street Parking

Along the Queen St. entrance into the Hespeler Core Area, it is recommended to move all on-street parking onto the south side of the corridor. This change will create a unified view shed down the Queen Street Corridor towards the core area. It will also allow for a straighter travelled lane to reduce weaving for the automobile around the on-street parking.

The goal of the streetscaping design concept is to meet the same or close to the same target of parking space numbers that exist currently.

Flexible Parking in Core Area

During the Spring/Summer/Fall Seasons, the City and businesses may express interest in encroaching out into the public right of way with patios, sidewalk sales or public events that require a larger programmable space. In the winter time, there is no need for the extra space and the space can be converted into parking. Flexible parking has been added along Queen Street West between Adam Street and Tannery Street. The City should work with business owners to determine under what occasions the flexible parking will be permitted or removed. To accommodate the needs during the different seasons and to assist in creating an Active and Vibrant Village Destination, the conceptual design proposes flexible parking within the core area of Hespeler.

The following guidelines are recommended for the detail design of the flexible parking:

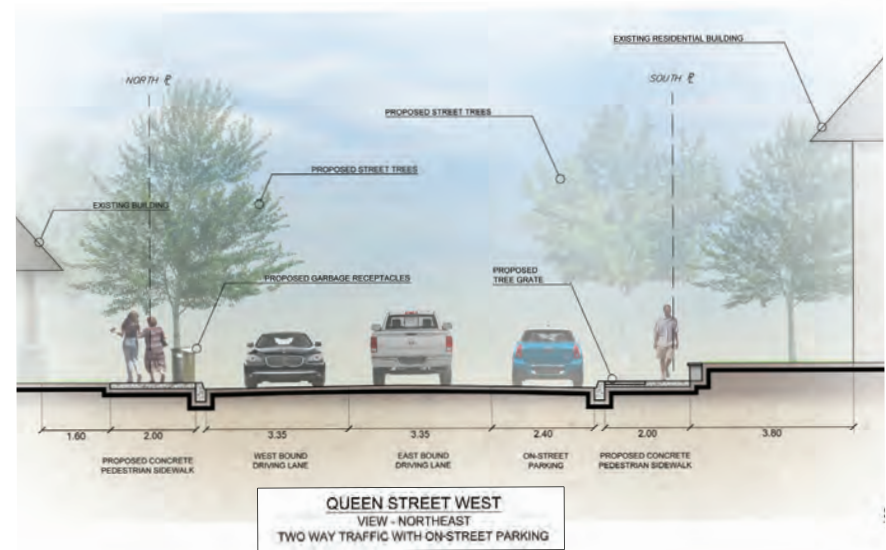
1. Parking spaces planned at same grade as sidewalk
2. Rolled curbs from street level for access to parking
3. Consider moveable bollards for flexible parking
4. Parking to be concrete slab



EXAMPLE: FLEXIBLE PARKING- KITCHENER, ON

Geometry:

Lane Widths (2 Lane Street)	3.35m
Lane Width (1 way Street)	6.0m
Turning Lane	3.0m
Average Sidewalk Width	3.0m – 4.0m dependant on location of existing buildings
On Street Parking Width	2.4m
On Street Parking Length	6.5m
Standard Curb Width (curb and gutter)	0.25m
Rolled Curb Width (curb and gutter)	0.25m



TYPICAL CROSS SECTION

Pedestrian Crossings at Intersections:

3.0 metre wide crosswalks have been designed with distinct thermoplastic impressed asphalt to denote a pedestrian and vehicular conflict. The installation of crosswalks can be completed prior to the reorganization of the street, as they can be installed on existing asphalt. The City of Cambridge logo, Hespeler logo, or public art could be incorporated into the crosswalks to further create a sense of place.



EXAMPLE: THERMOPLASTIC PEDESTRIAN CROSSING



EXAMPLE: STRATA CELLS

Landscaping and Trees

Street trees are an important feature to any streetscape for their health benefits for the community. They have numerous benefits such as: reduce the heat island effect; provide shade and refuge; increase the aesthetic quality of the street; purify toxins in the air; and create more enjoyable walking environments. In order to create a strong, visually unified and healthy streetscape that encourages Environmental Sustainability:

1. Street trees are planted consistently throughout the study area. They are placed in curb extension areas and are generally spaced 6.0 to 10.0 metres on-centre (dependant on species). They are repeated rhythmically and consistently throughout the area in a well-planned scheme.
2. Salt tolerant, urban tolerant and native species are encouraged throughout the area. For locations under hydro wires, smaller plant species should be specified as to not interfere with the functioning of the wires. Street tree species should be as per City of Cambridge and Cambridge North Dumfries Hydro Standards.
3. The use of strata cells (structured soil cell) is proposed throughout the study area. Urban trees require a large volume of soil in order to survive and establish into healthy specimens, however, often urban environments do not allow for adequate space. Soil structure systems allow for adequate soil volumes and also allow the structural support required to engineer roadways.
4. Planters are incorporated into the ROW and will be landscaped using native plant material, including grasses, wildflowers, trees and shrubs. The planting scheme should reflect a four-season approach for year-round interest.
5. Landscape should incorporate a wide range of strategies to minimize water consumption, including the use of native and adapted species, use of mulches and compost, alternatives to lawn and rainwater collection systems.
6. There is opportunity for the local horticultural society to be involved and take a leadership role in the maintenance of the beds.

Street Furniture

To promote the downtown core as a people oriented and desirable destination, a unified and improved palette of streetscape features has been developed. Guidelines for each of these features provide the necessary information and direction to guide future implementation of the streetscape improvements. Street furnishings and streetscape elements have been selected based on suitability, durability as well as form. These guidelines emulate many of the features installed in the Galt Core in order to present a consistent approach to streetscape furnishings in the City of Cambridge and to ensure maintenance costs are minimized. The specified elements are appropriately scaled for a comfortable and quality pedestrian experience. The objective for street furnishings is to create a high quality and unified streetscape.

General Guidelines:

1. The design of street furnishings specified reflects the heritage and unique character of the downtown area. They represent a consistent style and a high aesthetic quality. Street furnishings are to be durable, have low maintenance requirements, and are ideally produced in a sustainable manner.
2. Furnishings have been located throughout the study area in support of gathering places, adjacent to planting beds, in proximity to transit nodes and in appropriate areas for rest and socializing. Seating is placed for effective views of key areas, public gathering and recreation.
3. Streetscape furnishings are generally located in line with public street tree planting near the curb edge as to maintain a clear and unobstructed sidewalk.
4. The roadway, parking, curbs and sidewalks should be replaced as a whole and phased according to infrastructure improvements. Localized safety hazards should be corrected in the short term. This is further addressed in the short and long term phasing portion of the streetscape plan.

Sidewalks

Poured in place concrete has been specified for primary pedestrian walkways because of its affordability, durability and its accessibility. The use of alternative paving (concrete pavers, impressed concrete, textured asphalt, or coloured concrete) is limited to points of emphasis to highlight potential pedestrian vehicular conflicts and to clearly define pedestrian areas. All paving should be built to City of Cambridge standards.

Drainage:

To encourage the Guiding Principle of Environmental Sustainability proper drainage of the sidewalks is encouraged by sloping toward the curb and gutter along the street - but must maintain an appropriate cross slope. Streets should include surfaces or areas that absorb run-off and encourage natural percolation where possible. An infiltration system should be incorporated to increase soil moisture for street trees and reduce stress on stormwater management systems.

Lighting

A major issue identified as part of the public meeting and from the survey respondents was the lighting within the study area. The main concern identified with the lighting is that the light standards are too tall in height on Queen Street, that the light is polluting the residential units in the mixed use buildings during the night time. With repairs being costly to a variety of lighting and hardscaping treatments throughout the City, the design calls for low maintenance durable fixtures.

An analysis of all lighting infrastructure was undertaken by Rombald Inc. Rombald identified that there were two types of lighting which currently exist in Hespeler. Located on Queen Street between Adam Street and Harvey Street are the pole mounted cobra head style High Intensity Discharge (HID) fixtures which are dedicated to vehicular traffic only. This section of the study area does not have any lighting which is dedicated to pedestrian traffic. Any illumination that currently exists along the sidewalk is from spillage of the cobra head fixtures or from the retail stores.

The street lighting that is currently on Queen Street between Adam Street and Cooper Street, as well as Tannery Street is pole top mounted lantern style fixtures mounted on a pre-cast concrete base. Street lighting on Guelph Avenue is primarily cobra head fixtures with some post top mounted

fixtures,

The following recommendations were developed from the study to address lighting concerns

1. Review existing overhead servicing of fixtures and consider consulting with Cambridge North Dumfries Hydro Inc. to review possibility of providing new underground infrastructure for new fixtures
2. Review the existing condition of underground infrastructure and consider replacing/reworking to allow additional downtown requirements such as seasonal lighting, event power, etc.
3. Replace and retrofit existing lighting fixtures with Energy Efficient technologies.
4. Review fixture selection to reduce glare and unwanted light trespass.
5. Replace existing pole-top fixtures throughout the area with Energy Efficient technologies. In the situation where the pole is required to be replaced, the City will be required to provide a base adapter.
6. Replace existing lighting along the bridge with a similar Energy Efficient style of fixture but leave the infrastructure in place.
7. Photometric study is required to do an accurate assessment of light pollution on private properties and to ensure adequate lighting for vehicular and pedestrian traffic.



PROPOSED LIGHTING: CONSISTENT WITH MAIN STREET IN GALT



EXAMPLE: MOLOK GARBAGE CONTAINER

Benches

The bench specified is the Scarborough Bench manufactured by Landscape Forms. The bench features a woven seat and back style with black powder coating. This bench has been specified in the 72" length.



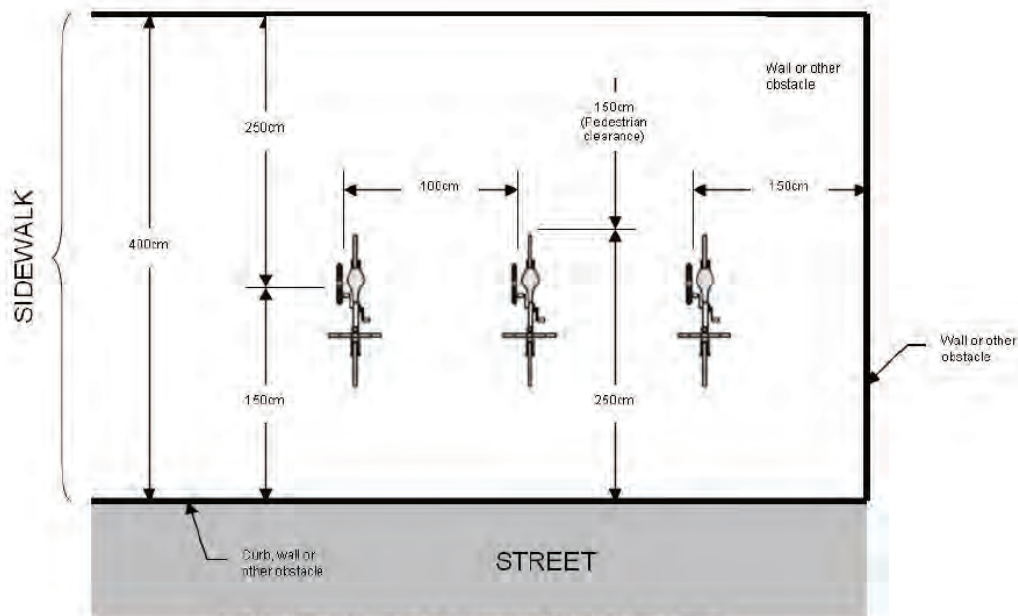
PROPOSED STREET BENCH

Waste and Recycling Receptacles

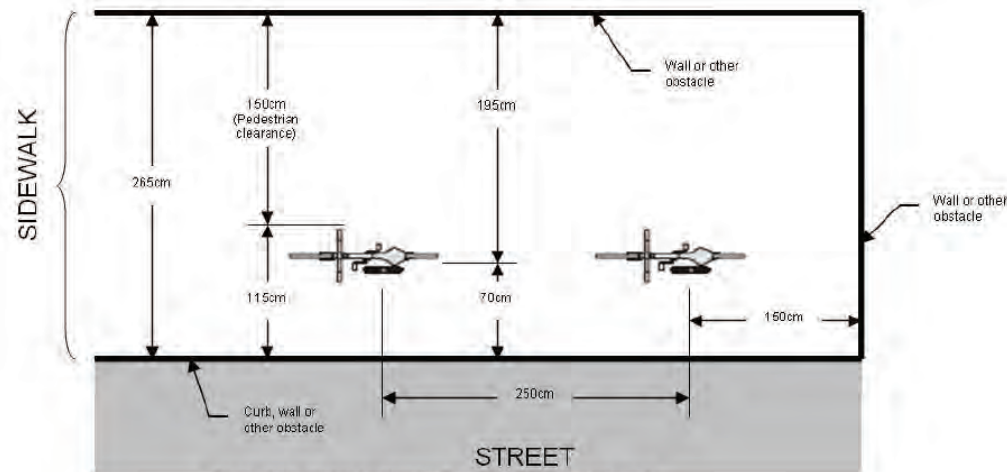
To reduce waste within the study limits the CityScape Molok Garbage and Recycling containers have been identified as the preferred collection type. These containers hold 500L of garbage and can be serviced by Waste Management at a third of the collection rate as a regular waste collection bin.



EXAMPLE: MOLOK GARBAGE/RECYCLING CONTAINER



– Single bicycle racks installed perpendicular to the sidewalk



– Single bicycle racks installed parallel to the sidewalk

Bicycle Parking

The bike ring specified is the MBR 100 manufactured by Maglin in black powder coated finish. This bike ring should be installed as direct burial, where possible, and spaced at 32-36" apart on centre when facing each other and 72" apart on centre when facing outwards Bicycle Parking must also conform with the City of Cambridge Bicycle Parking Guide. Examples shown below for spacing.



EXAMPLE: MBR 100

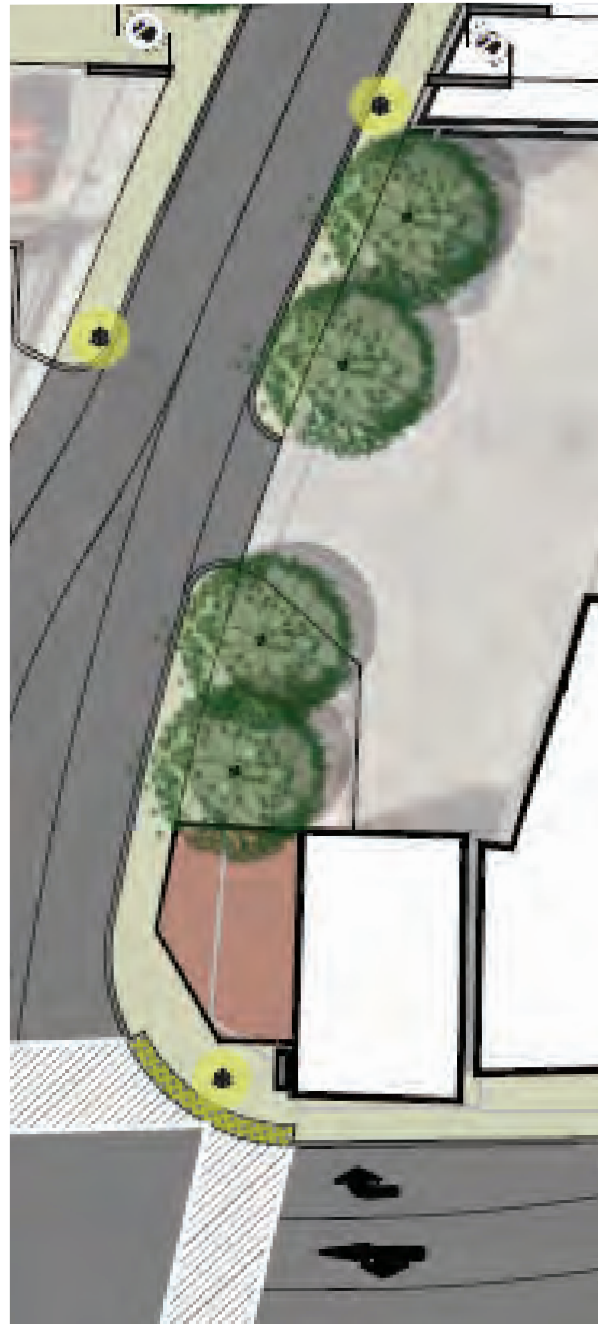
Wayfinding Signage

Signage and wayfinding is an important circulation component to a successful downtown core as it provides direction to pedestrians. To improve wayfinding and awareness of core area amenities/features, new signage is designed and located at strategic points along the streetscape. The objective of a signage and wayfinding strategy is to provide attractive and functional signage that clearly directs visitors to amenities within the core area. In order to achieve this objective:

1. Signage and wayfinding is designed to be consistent with the newly developed signage policy across the City of Cambridge. It will, at a minimum, include the following destinations: civic buildings, green space, trails, parking, farmer's market, and other amenities unique to each study area.
2. Signage must also meet the Engineering Standards



EXAMPLE: WAY FINDING SIGNAGE



VEGETATIVE BUFFER WITHIN PUBLIC RIGHT OF WAY TO REDUCE PRECENCE OF PARKING AREA

Parking Lot and Private Development treatments in Public Right of Way

Parking lots along Queen Street, Adam Street and Guelph Avenue should all be screened with landscaping treatments to create uniformity along the street frontages and encourage a pedestrian oriented route



PUBLIC ART BUFFERING PARKING AREA

Source: Google Images

Opportunities for Public Art

To address the Guiding Principle of Embracing Art, Culture and Heritage, Key areas have been identified as strategic places where public art should be incorporated. Public art in urban areas enhances the pedestrian environment and creates a sense of place that is unique to the core area. Public art is used in the core to further define and articulate the unique history of the area. Public art should be installed through new capital projects and phased with infrastructure improvements. There is opportunity to involve the local community through design competitions and commissioned work by local artists.

Through site redevelopment the Site Plan Review Committee should encourage private developers to incorporate a public art feature to enhance the site. Public art can be located on a building facade or as a stand alone feature and should be consistent with the Community Service Departments Public Art Policy.

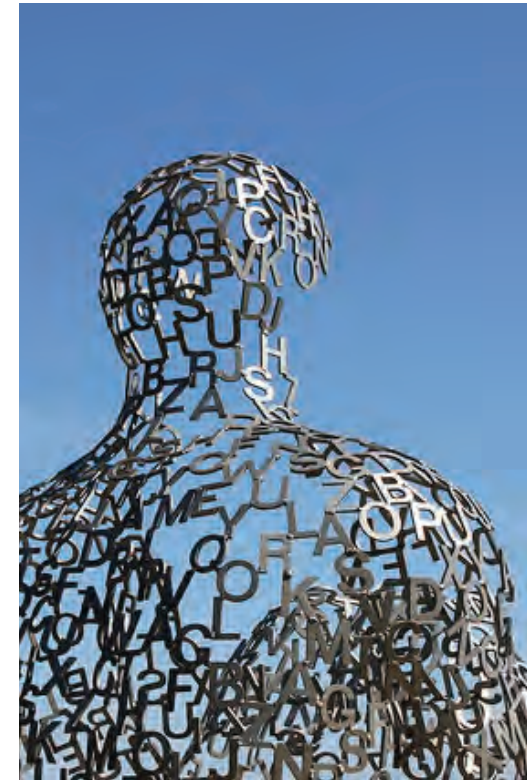
All public art should be durable, and touchable to enhance public interaction and culture.



Source: Google Images - Aero Agosenis, Boise, Idaho



Source: Google Images- Cupertino, California



Source: Google Images, Milwaukee, WI

Accessibility:

To meet the objective of integrating barrier free elements, the design guidelines call for dropped curbs and ramps are specified at intersections along curb face the extent of the crosswalk width for accessibility. Textures or relief patterning is used at the corners of sidewalks through truncated domes surface to denote intersections. This patterning should not create trip hazards for pedestrians.

Decorative band paving is used at all intersections to demarcate their importance and to define pedestrian and vehicular conflict. Truncated dome surface, textured paving, is also used on the dropped curbs at intersections to also define a potential conflict.

Sidewalks are designed to allow a minimum 2.0 metre wide clear unimpeded path of travel; where possible wider sidewalks have been designed. Sidewalks should be continuous across any driveway entrance and should be designed to provide flush entrances to each doorway.



EXAMPLE: GRAND RIVER TRANSIT BUS SHELTERS

Bus Shelters

The bus shelters in Hespeler should coordinate with the current design of the Grand River Transit bus Shelters, as shown above.



EXAMPLE: TRUNCATED DOMES - ALTERNATIVE COLOURS DETERMINED AT DETAILED DESIGN



PART

05 Architectural Guidelines

5.1 Existing Heritage and Architectural Features of the Study Area

The concentration of cultural heritage resources within the Village of Hespeler form a significant resource within the community. The preservation of these resources ensure Hespeler remains connected with its past, contributes to its current character and helps shape and influence its form, function and aesthetic quality in the future. The purpose of the Architectural Guidelines is to ensure heritage and architecturally significant characteristics are preserved maintained and respected through future development patterns. Also, for the protection of the unique and distinct character these attributes create throughout the Village of Hespeler.

Heritage Property Identification

The City of Cambridge includes cultural heritage resources that are designated under Part IV and V of the *Ontario Heritage Act, R.S.O. 1990* and those identified as properties of interest by the Cambridge Municipal Heritage Advisory Committee (MHAC) for their cultural heritage significance. Heritage resources existing within the Hespeler Village study area, which include both properties designated under Part IV of the Ontario Heritage Act and properties of interest. No Heritage Conservation Districts exist within the Hespeler Village study area.

The properties within the Village of Hespeler designated under Part IV of the Ontario Heritage Act and identified as properties of interest, are shown in green and yellow respectively, in Figure 1.



Source: City of Cambridge GIS

Established Heritage Policy and Municipal Advisory Committee

Currently, policies and the Municipal Heritage Advisory Committee (MHAC) exist at a local level, which aim to protect cultural heritage resources.

Cambridge Official Plan

The City of Cambridge Official Plan (Regionally Approved November 21, 2012), contains many policies which aim to protect and enhance cultural and natural heritage resources within the Hespeler Village. Specifically, an objective of the growth management policies is to encourage the continued development of the Hespeler Village as a Community Core Area of high intensity and mix of land uses, promoting elements such as heritage conservation and the adaptive re-use of existing buildings. Another objective of the growth management policies is, to promote the principles of conservation and sustainability, the intensification of development in the built-up area and the protection, enhancement or restoration of the natural environmental and cultural heritage resources, where feasible and appropriate.

Cambridge Municipal Heritage Advisory Committee

The Cambridge Municipal Heritage Advisory Committee (MHAC) is a committee established in 1976, made up of Council appointed volunteers to assist council and property owners in designating culturally significant properties under the Ontario Heritage Act R.S.O. 1990 (Heritage Act). Specifically, the mandate of the Committee is to provide advice on cultural heritage resources, and assistance to City Council and landowners in designating properties and districts under Part IV or Part V of the Ontario Heritage Act. In addition to designations under Part IV of the Heritage Act, MHAC helps to identify possible properties of interest according to their cultural value, and urban design contribution, structural condition, and potential for adaptive re-use. MHAC has established a sub-committee to review applications for alterations ('Application to Alter a Designated Heritage Property'), submitted by land owners for heritage properties. The application is typically heard by MHAC, who also provide suggestions and guidance through the applications process.

MHAC administers a Heritage Grant Programme that provides matching grants to property owners for the restoration and conservation of such properties.

Existing Architectural and Heritage Features of Hespeler Village

A heritage designated property or building may be seen to be significant as a result of various features. These features could include a particular building type, architectural feature or element, the building style or period, could be the work of a local and/or significant architect or builder, or could have an important role in the community's history. A designation can be structured to include the entire building, the façade or a specific architectural feature. Designation protects these features from alterations or modifications that would decrease the overall cultural value, and may protect the heritage feature from demolition.

The architectural features within Hespeler Village form part of its character and historical significance. The alignment of architectural features and elements, from one building to the next, creates visual continuity and establishes a coherent visual context throughout. It is therefore essential to ensure the preservation maintenance, and rehabilitation of exteriors and storefronts with significant and appropriate historic and architectural features as well as buildings whose form, proportions and materials contribute to the character of the street. Many buildings in Hespeler consist of architectural features which define the character, overall historical theme and historical significance of the area. It is essential that these buildings and features continue to make a contribution to the historic character of the downtown.

The character defining elements of Hespeler and its cultural assets are outlined below.

Character Defining Elements of Hespeler

The character defining elements of Hespeler, as discussed in the Heritage Master Plan are as follows:

- Commercial Centre of the Village;
- A large collection of heritage buildings – stores, inns, industries, town services;
- Two to three storey retail buildings are typical, predominately built of stone (local limestone construction) and brick;



HISTORICAL SOCIETY - HESPELER VILLAGE



HESPELER PUBLIC LIBRARY

- The Stamped and Enamel Ware Ltd. building (later known as American Standard);
- Furniture factories located on the north bank of the river, across the bridge from the commercial core, currently used for light industrial and service uses;
- Late 19th century worker cottages and row houses, maintained on Spring Street and the south side of Queen Street East;
- Reduced to no setbacks with broad sidewalks; and
- A tradition of fine public buildings, continuing with the architectural updates to the Hespeler Public Library and also including the historic Fire Hall and Company of Neighbours building.

Cultural Assets of Hespeler

The Cultural Assets of Hespeler, as discussed in the Heritage Master Plan are as follows:

- Three churches, fire hall, public library, former post office building;
- Speed River, Hespeler Mill Pond, canoe launch (Jacob's Landing); and
- Forbes Park;
- Intersection of Guelph Ave, Queen Street/Adam Street

Guidelines for Heritage Properties & Properties of Interest

Many buildings along the main corridor (Queen Street) have been altered over the years resulting in the loss of significant heritage resources, loss of original architectural details or changes to façades, rendering them beyond recognition. While it is acknowledged that changes to structures in the Village will occur over time, it is also a concern that these changes do not adversely impact the historic building fabric and character of the streetscape. These historical or architecturally significant buildings and characteristics must be maintained in an attempt to showcase the local heritage and character of the area.

GOAL: Any building renovation or alteration to a heritage designated building or property of interest, regardless of the planned use, must retain the overall design integrity of the historic building by restoring the original features and materials and respecting the traditional design elements. Historical photographs and original architectural drawings (if available) should be used to determine the appropriate design solution.

As such, the following Guidelines apply to the preservation, rehabilitation, restoration, renovation or improvements of designated heritage properties and properties of interest.

Facades

Preservation of traditional façade elements found on existing buildings creates patterns along the face of the block that contributes to the overall character of the area. These building and architectural elements include:

- Kick plates as the base to building fronts;
- First floor display windows;
- Recessed central entrance areas or angled entrances on corners;
- Transoms above entrance doors;
- Clerestory portions of display windows;
- Sign bands;

- Parapet walls with caps or cornices;
- Vertical window patterns, shapes, window sills on 2nd floor;
- Pilasters and decorative brick or stone

The sum of the façade elements defines a building's visual qualities and character. Even when building uses have changed, it is still important to retain and/or interpret traditional façade elements.

Guidelines

1. The original design and materials of the building should be respected.
2. Rehabilitate and restore architectural details rather than replace, using proper heritage methods. Archival images should be used when available to determine historical design, when repairing a façade; where no reliable clues to the original condition exist the design should reflect the style, rhythm and scale of other traditional examples on the street.
3. Both vertical and horizontal divisions shall be consistent from one building to the next. For example, horizontal lines shall be in line with neighbouring buildings, while vertical lines should reflect the rhythm and spacing of existing buildings within the overall streetscape.
4. Special care should be given to brick and masonry repair. Gently clean brick and masonry – never sandblast. Mortar mix must match existing – modern 'hard' mortars will damage the existing brick.
5. Existing architectural details, such as ground floor cornice and signboard, must be retained and rehabilitated. Where no original details exist, archival research should be undertaken to establish design parameters.
6. Preservation or restoration of ornamental cornices is particularly encouraged. Other important façade elements to be respected include belt courses, pilasters, windows, window arches and frames.

- 7 Adding more elaborate or extensive ornamentation than was originally found on the building façade is not generally appropriate.
8. It is not the intention of this guideline to recreate the past if the original building façade does not exist. However, if documentary evidence such as photographs of the original does exist, then one recommended alternative is to restore the façade based on this documentation. Where exact reconstruction is not practical, new simplified contemporary interpretations of the original details are possible as long as the intent, scale and character of the original detail are retained and in context to the streetscape.



Roofs

Visible roof material should be treated in the same manner as the building façade, reflecting the existing conditions and character of the area.

Guidelines

1. Visible roof material should be treated in the same manner as the building façade, and repaired or replaced with like materials, such as copper or zinc.
2. Non visible roofs should be repaired or replaced using modern materials and where possible or appropriate green roofs should be encouraged.
3. Use low-profile mechanical units and elevator shafts on rooftops to reduce visibility from the street. If this is not possible, setback or screen rooftop equipment from view. Also be sensitive to views from the upper floors of neighbouring buildings.
4. Skylights or solar panels should have low profiles and not be visible from public right of ways. These features should be installed in a manner that minimizes damage to historic materials.

Building Materials

Building materials are an important element in the foundation of architecture, as the chosen material helps to define a specific character and style. It is important to maintain these character defining elements, by ensuring original building materials are maintained and preserved, to protect the heritage value.

Guidelines

1. If the replacement or refurbishment of original building materials is required by a building owner, the form, material and detailing of the new elements must be compatible with the overall historic character.
2. Use sympathetic and compatible materials, in keeping with texture, colour and composition.
3. Clean masonry using gentlest means necessary; never sandblast. It is often better not to clean and to leave a weathered patina to maintain historical character.
4. Never paint unpainted masonry.
5. Traditional materials are preferred, modern materials may be used if sympathetic.
6. Retain original materials wherever possible through repair and restoration.
7. Avoid concealing original façade materials. If the original material has been covered, uncover it if feasible. If portions of the original material must be replaced, use a material similar to the original. If facade cannot be recovered, owner must report to city and a consensus shall be made for building materials/replacement of facade.
8. Stone and brick were the predominant building materials used in the downtown. Avoid the use of materials that are not visually compatible with these materials and the original façade, such as shiny metals, mirror glass, plastic panels, and vinyl windows or doors.

Windows and Doors

The alignment of architectural features from one building to the next such as window locations, create visual continuity and establishes a coherent visual context throughout the downtown. For the commercial buildings they create unity along the face of the block that contribute to the overall character of the area.

Guidelines

1. Restore original spacing, proportion and rhythm of openings, if altered from the original.
2. Recessed entries identify the entrance and provide shelter. The rhythm of these recessed entrances on the street clearly contributes to visual continuity and the traditional character.
3. Use doors with a large area of glass above a solid panel at the base surrounded by a painted frame. Avoid unfinished anodized metal, bright aluminum, or stainless steel frames.
4. For most historic buildings, large panes of glass at the display windows level with solid kick-plates below are appropriate.
5. Multi-pane designs that divide the storefront window into small components should only be used if they replicate historic elements and original openings that can be documented.
6. Finished frames may be metal with black anodized or painted finish; however, painted or varnished wood is preferable. Residential type doors are not acceptable. If documentation of the original entry is available, the recommended alternative is restoration or replication.
7. The kick-plate, generally the area of the street façade below the storefront window and above the sidewalk, should be preserved wherever possible. For buildings with historic significance, the original kick-plate should be restored from documentary evidence. If original information is not available, develop a new simplified design that retains the original character and dimension of a kick-plate that would most likely have been on the building. For renovations where there is no documentary evidence, appropriate kick-plate materials are: brick, wood panels, stone, and glazed tile or painted metal in muted tones.
8. The use of a clear glass transom over doors, or clerestory features within the upper part of the display window area, is generally appropriate. The design of the entire storefront, clerestory, display window, transom and entry door was generally considered as one element with a consistent framing detail.
9. Rehabilitation should consider using the original materials and proportions of the opening to re-establish this character. Align transom or clerestory window and framing with other adjacent buildings to maintain a clear line along the block face.
10. Maintain the original spacing patterns and proportions of the windows. Re-open/ reveal upper storey windows if they are presently blocked.
11. If lowered ceilings are necessary, step the dropped ceiling back from the window. If the window must be covered to accommodate the dropped ceiling, recreate the original style windows from historical documentation.
12. Repair rather than replace original windows. If repair is not feasible, replace with windows that match the existing windows as closely as possible.
13. Preserve the window frame, sash and surrounds. Size, frame and trim material, method of operations, size of sash members, window frame elements, and the pattern of divided lights are important features to replicate if unable to preserve.
14. A historic material such as wood is most appropriate. If moulded plastic, vinyl or aluminum replacements must be used they should replicate original materials, finishes, and dimensions as closely as possible.



STOREFRONT REVITALIZATION (BEFORE)



STOREFRONT REVITALIZATION (DURING)



STOREFRONT REVITALIZATION (AFTER)

Cornices and Parapets

Cornices and parapets are often subject to decay due to weather exposure and material deterioration and can cause a hazard to the street below. The appeal of heritage buildings is often found in the wood details, cornice, brackets and trim. These essential details should be repaired or replaced to their original configuration.

Guidelines

1. Repair rather than replace cornices and parapets. If replacing, replace with copy of original.
2. Establish consistent demarcation lines throughout.
3. Upper and lower cornice and signboard heights should generally be the same height and width.
4. The height of the parapets should be limited to less than 1.8 metres.
5. False parapets should be discouraged
6. Aluminum or modern pre finished materials are not acceptable unless architecturally appropriate and sympathetic to the building.



EXAMPLE: CORNICE

Setbacks and Step-backs

Guidelines

1. Occasionally, the line at the sidewalk is retained by the use of other elements such as planters, columns or railings, and the storefront is recessed. Where buildings are built to an alley edge, consider alley display windows and secondary customer entries if original materials and features are not damaged.
2. Preserve storefront display windows at the sidewalk edge. Maintain historic recesses and entry ways where they exist.
3. If approval is given for a roof top addition, any portion of the structure built above the existing roof line should step back from the storey beneath it in order to limit its visual impact on the streetscape.

Signage

Guidelines

1. Signage should be consistent in location, and the primary signage should be located on the ground floor entablature (between the storefront windows and the upper façade).
2. Signs may be painted directly onto storefront windows, but should promote the heritage character of the area and allow for unobstructed views into the storefront display.
3. Signage should be incorporated into the

design of the building façade.

4. Signage must never be backlit, only lit externally.
5. Signage must conform with the provisions of the City's Sign By-law, as amended.

Signage for Heritage Designated Buildings

1. Signage on heritage designated buildings shall follow Section 26 of Sign By-law, as amended. Whereas all signage proposed on heritage buildings shall be forwarded to the Heritage Planner of the Municipal Heritage Advisory Committee for approval.
2. As described in Section 26.2
 - A wall sign shall have a total sign area not greater than 0.3 sq.m. for each 1.0m of linear frontage of the building wall upon which the sign is located and, in any event, not greater than 1.25 sq.m. for each sign.
 - No sign installed or erected in Heritage Conservation Districts and on Designated Properties shall be internally illuminated.
 - No business establishment shall have more than one sign per storey for each building face of such establishment.

Lighting

Guidelines

1. Lighting affixed to buildings should be in keeping with the style of the building and character of the area. This would include styles appropriate with the architecture and heritage features of the area, or contemporary

designs that are compatible with the historic context.

2. In no circumstance should building lighting be seen as a replacement for street lighting, or vice versa.
3. Lighting for the purpose of illuminating signage should be of an appropriate style, size and light level, and integrate well with the building and sign that they serve. Where possible, lighting should project directly on the signboard.
4. Encouragement should be given to energy efficient lighting to promote sustainability.
5. Special attention should be given to reducing levels of light pollution.

Accessibility

The Accessibility for Ontarians with Disability Act (AODA) provides the framework for the development of mandatory standards of accessibility. These development standards must be reviewed and implemented through the application of the AODA.

Guidelines

1. Accessibility issues should be explored and, if possible, addressed during any restoration work.
2. All new buildings and major renovations/ additions to existing buildings will need to conform to existing legislation regarding universal design principles.

5.2 Area Specific Guidelines

Milling Road

The Milling Road Area is formed of industrial buildings owned by both private ownership and by the City of Cambridge. As described earlier in Section 4.3 of this report, the Milling Road Area has a great opportunity for creating a unique district through adaptively reusing the existing buildings. As such, it is recommended that all buildings in the Milling Road Area be maintained, and adaptively reused to accommodate restaurants, retail/service and office based type of development.

Any redevelopment proposals should be in accordance with the Architectural Design Guidelines listed in Section 5.3 of this report and the Urban Design Guidelines (listed in Section 4.3 of this report). Notwithstanding the above, the following guidelines will also apply to the Milling Road Area.

Guidelines

1. The existing industrial character of the façade of all buildings should be maintained.
2. Any replacement or renovation to existing window should be in keeping with the industrial character to reflect the former uses and character of the Milling Road Area.
3. Brick facades should never be covered with any architectural façade treatment. Maintain the existing character and weathering of existing brick wherever possible.
4. Keep original signage paint on building to preserve heritage character.
5. New signage should be complementary to industrial character. Canopy and awnings may also be used for signage.
6. Provide façade lighting to highlight character and historic elements during evening hours to assist in creating a lively evening atmosphere.



MILLING ROAD: EXISTING CONDITIONS

5.3 Guidelines for Future Infill

The purpose of this section is to provide guidance for the design of new construction and the renovation of non-contributing buildings in Hespeler Village, in order to retain the historic context of the area while providing new opportunities. Non-contributing buildings are those that have little or no cultural heritage, significance, but are in sufficiently good condition to warrant remodeling. While new building design is expected to reflect the character of its own time, thereby making the downtown a living district, it is important that it also respect the traditional quality that makes the downtown unique. These qualities include massing, scale, and consistency with adjoining buildings, storefront detailing and choice of materials.

Modern architecture should not be discouraged but rather should be approved at the discretion of City staff on a site specific basis, and should follow the general guidelines for building materials identified in Section 5.1. Any buildings of this style must remain sympathetic and complementary to the overall character of the area and surrounding buildings.

GOAL: To ensure that infill development retain the overall design integrity of the historic area by protecting the original character of the area, through the use of features and materials and respecting traditional design elements, and to provide specific guidelines for areas such as the core area along Queen Street, redevelopment along Queen Street (west of Guelph Avenue and Adam Street) and potential redevelopment of the multiple block at Tannery Street and Queen Street.

Height

Appropriate building heights sensitive to other buildings on the block, assist in restoring or recreating the historic alignment of architectural features, which unify the street visually.



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Guidelines

1. In general, the building should appear similar in height, mass, and scale to other buildings in the historic area to maintain the area's visual integrity and unique character.
2. Notwithstanding the above, it is important to maintain a variety of heights to create visual interest.
3. While the actual heights of buildings are of concern, the perceived heights of buildings are equally important. New infill buildings with a proposed height taller than existing shall step back at height of existing buildings, to give the appearance of continuity at street/pedestrian level and to reduce the overall massing and appearance of the building.
4. Two and three storey buildings make up the primary architectural fabric of the downtown; taller buildings should be located at key intersections or prominate locations.

Setbacks and Step-backs

Guidelines – Front Property Setbacks

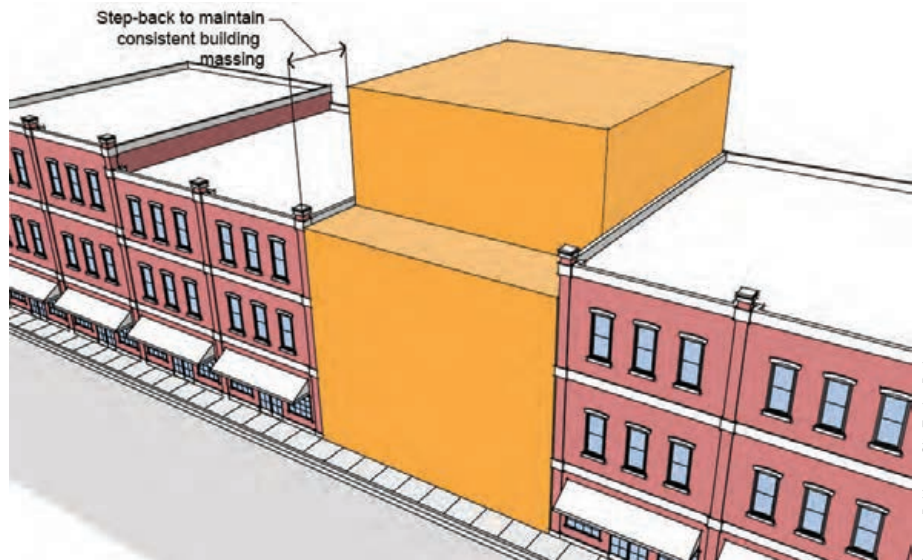
1. In many cases, the building's placement on the site is an important defining characteristic. To maintain the original setback of historic buildings the footprint of a new building, where possible, should be in line with the footprint of at least one adjacent existing building or the anchor buildings at the corner of Queen Street and Guelph Avenue/ Adam Streets.
2. For commercial style buildings, if a portion of the building wall is proposed to be setback from the sidewalk, careful consideration should be given to maintaining the front line of the building at the sidewalk edge through the use of planters, railings, columns or similar features up to an overhanging second floor.
3. A portion of a new building may be set back to accommodate a ground floor colonnade or porch, so long as the upper storey remains in line with surrounding and adjacent buildings.
4. A new building or addition on the same lot as an historic building should be set back to reveal the historic building.

Guidelines – Front Property Step-backs

5. Additional storeys should be substantially setback (approximately 1.5m -3.0m from the primary façade so that the original building heights and façade are clearly distinguishable from the new upper floor as seen from the street.
6. If approval is given for a roof top addition, any portion of the structure built above the existing roof line should step back from the storey beneath it in order to limit its visual impact on the streetscape.

Guidelines – Side Property Setbacks

7. Position additions set back from the main façade, and create the difference between the original façade and the addition with a subtle change in colour, texture or materials



EXAMPLE: BUILDING STEP-BACKS



EXAMPLE: BUILDING STEP-BACKS

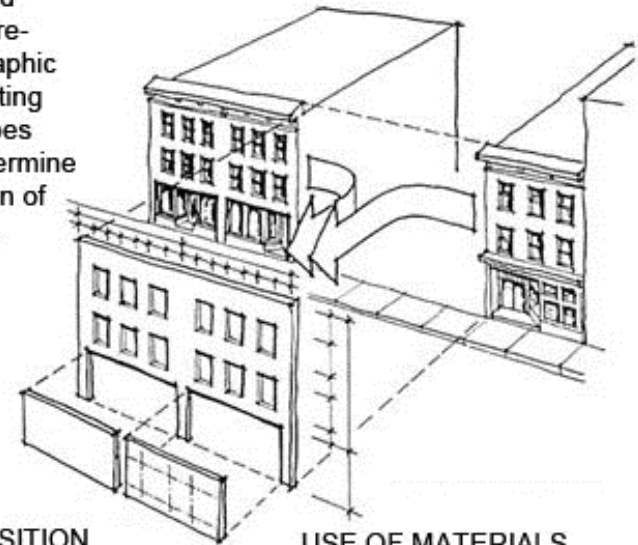
Facade Articulation and Massing

Repetition of traditional façade features creates patterns and visual alignment that contribute to the overall character of the area. While these features may be interpreted in new and contemporary ways through infill and intensification, the following guidelines will assist in ensuring uniformity and appropriate façade design, in keeping with the general character of the area. Architectural treatments should conform to adjacent buildings however, contemporary design can be a welcomed addition to the streetscape but the design should be respectful to the surrounding buildings. It is important that the façade pattern of the block be maintained. This is especially important for projects facing key pedestrian streets. The commercial building facades create patterns which form contributing factors to the overall character of the area. Future building facades should be designed to reinforce these patterns and support the area's established visual character.

Guidelines

1. Both vertical and horizontal divisions should be consistent from one building to the next. Large buildings can be sub-divided into smaller units with subtle façade treatments that will create consistency within the larger building yet provide additional units, promoting intensification. For example, horizontal lines should be in line with neighbouring buildings, while vertical lines should reflect the rhythm and spacing of existing buildings along the streetscape. Breaking up the façade of larger buildings into smaller bays will aid in reflecting the character and style of smaller buildings along the streetscape.
2. Massing of new infill buildings should reflect the scale, massing and size of buildings within the immediate context.
3. Where possible, new infill buildings should be of similar width and height to existing buildings within the immediate context.
4. Where possible, a kick plate should be provided as a base to the storefront. Align the height with others in the same block.
5. Incorporate clerestory elements in display windows and transoms. Align transoms above entrance doors with others, when others are appropriately placed.

The concepts and characteristics presented in this graphic are those of existing Architectural Types which would determine the "detail" design of the Infill Facade.

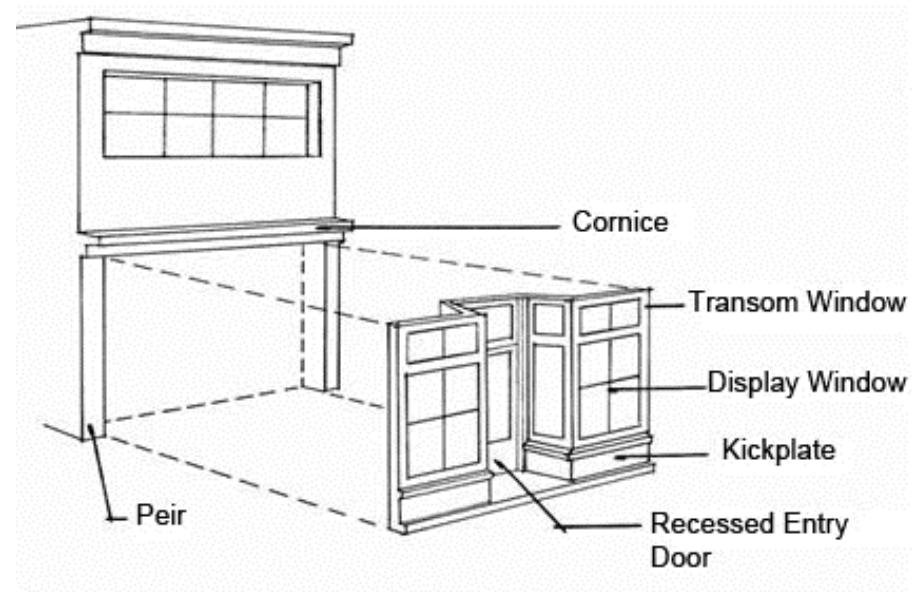


VISUAL COMPOSITION

The Visual Composition of the infill facade (that is, the organization of its

USE OF MATERIALS

An infill facade should be composed of materials which



STOREFRONT WITH TRADITIONAL MATERIALS

A Cornice can be constructed with wood framing, plywood and moldings with a sloping sheet metal cap to shed water. The Cornice spans the top of the storefront, often covering a structural beam unfinished brick

Transoms are optional design elements that help to break up the massive effect of very large sheets of glass. Transom windows can be clear, tinted or stained glass.

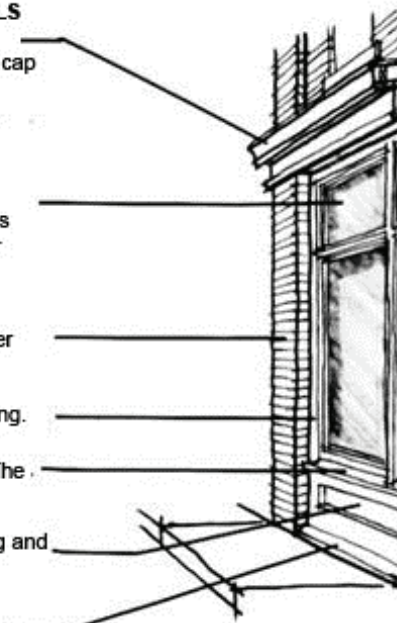
Masonry Piers are uncovered and match the upper facade.

The storefront is recessed 6 inches into the opening.

The storefront and windows are framed in wood. The sill slopes forward for drainage.

The kick plates are constructed with wood framing and a plywood back with trim applied to it.

The storefront rests on a masonry or concrete base to prevent water damage.



STOREFRONT WITH CONTEMPORARY MATERIALS

A Cornice is made with sheet metal over a wooden frame.

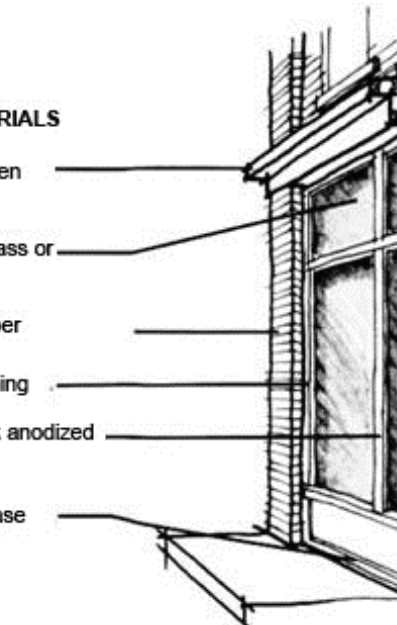
Optional transoms can be stained glass, clear glass or opaque.

Masonry piers are uncovered and match the upper facade.

The storefront is recessed 6 inches into the opening

The storefront and windows are framed with dark anodized aluminum or painted aluminum

The storefront rests on a masonry or concrete base



6. First floor display windows should be aligned with the height of others in the block when others are appropriately placed.
7. Storefront windows should be provided, even for restaurant venues.
8. Upper storey window openings, vertical patterns, shapes and styles should be consistent with others in the block, including window sills on upper floors.
9. Sign band should be placed above the street level.
10. Parapets should be included to provide a distinctive upper edge to a building's façade, constructed with materials characteristic of the area, preferably with a raised parapet on the main façade.
11. Cornices should be included to provide distinctive edges and linear detail to a building's façade, constructed of materials characteristic of the area with a strong projecting cornice shape.
12. Angle building entrances on corner lots.
13. Central entrances should be recessed.
14. Designs should be sympathetic to and incorporate similar roof lines and proportions to that of other buildings in the block.
15. The first floor of downtown commercial buildings should be primarily transparent, oriented towards the street and inviting storefronts.
16. Upper floors should incorporate traditional vertically proportioned window openings within a more solid façade treatment.

Roofs

Guidelines

1. Roofs of new buildings are recommended to be similar to other surrounding buildings on the block.
2. Use low-profile mechanical units and elevator shafts on rooftops to reduce visibility from the street. If not possible, provide adequate setback to screen rooftop equipment from view. Also be sensitive to views from the upper floors of neighbouring buildings.
3. Rooftop amenity areas, including open space, green roofs, balconies or terraces are encouraged to promote sustainability and green energy practices.
4. Skylights or solar panels should have low profiles and not be visible from public right-of- ways. These features should be installed in a manner that minimizes damage to historic materials.

Materials

Guidelines

1. Use building materials that have a texture, pattern and scale similar to those in the central core area.
2. The use of brick as the primary building material is encouraged to reflect historic building patterns in the commercial downtown. Choose accent materials similar in texture and scale to others in the downtown. These include:
 - Brick and stone masonry
 - Wood details surrounding windows
 - Finished lumber, applied to achieve traditional patterns (i.e., horizontal siding rather than diagonal)
 - Finished painted metal and sheet metal

- Clear or lightly tinted glass
- Ceramic tiles
- Brick, clay and ceramic pavers
- Slate, finished metal, glazed ceramic and tile roofs
- Concrete and stone as lintels and wood or concrete columns
- Embossed metal

The following materials are generally inappropriate:

- Coarsely finished, “rustic” materials, such as wood shakes, shingles, barn board or stained fir plywood.
- Poorly crafted or “rustic” woodworking and finishing techniques
- Corrugated metal and fiberglass
- Most rock or fieldstone
- “Antique” or old brick with partial paint, mottled light variegated brick, oversized brick and white brick mortar
- Stucco surfaces that are highly textured (i.e., associated with a “hacienda” or “Mediterranean” style)
- Expanded metal
- Silver or clear anodized aluminum sheets
- Silver or clear aluminum extrusions for windows and doorways
- Residential type sliding glass doors
- Imitation wood siding or stone
- Flat or moulded plastic sheeting in quantities exceeding 0.5 square meters when used as primary façade materials

- Imitation metal “rock work”
- Plastic moulded imitation of any conventional building material
- Mirror or metallized reflective glass
- Glass block

Awnings and Canopies

Traditionally, most storefronts at the turn of the last century were covered by fabric awnings to offer some protection from the sun and rain. The use of retractable fabric awnings is encouraged only of the side of the street where sun is an issue to limit “visual clutter”.

Guidelines

1. The fabric should be durable and made of traditional colours
2. Awnings should integrate well into the overall design of the storefront.
3. Only one style of awning should be used on a building.
4. Awnings must be retractable in order to be in keeping with the heritage character of the street and to allow for sidewalk maintenance.
5. All proposed awnings must be in accordance with the City of Cambridge Sign By-law 191-03 (Section 5 – Awning sign)



EXAMPLE: STOREFRONT SIGNAGE

Signage

Guidelines

1. Signage should be sympathetic to the existing area and should unify store front signage in the area.
2. Signage should be incorporated into the design of the building façade.
3. Signage should be consistent in location, and the primary signage should be located on the ground floor entablature (between the storefront windows and the upper façade).
4. Signage should never be backlit, only lit externally.
5. Signs may be painted directly onto storefront windows, but should promote the heritage character of the area and allow for unobstructed views into the storefront display.
6. All signage should be designed in accordance with the City of Cambridge Sign By-law 191-03



EXAMPLE: STOREFRONT SIGNAGE

Lighting

Guidelines

1. Lighting affixed to new buildings should be in keeping with the style of the building and character of the area. This would include styles appropriate with the architecture and heritage features of the area, or contemporary designs that are compatible with the historic context.
2. In no circumstance should building lighting be seen as a replacement for street lighting, or vice versa.
3. Lighting for the purpose of illuminating signage should be of an appropriate style, size and light level. Where possible, lighting should project directly on the signboard.
4. Encouragement should be given to energy efficient lighting to promote sustainability.
5. Special attention should be given to reducing levels of light pollution.
6. Lighting should be installed in such a wattage that it does not cause damage to the building fabric.



EXAMPLE: SIGNAGE AND LIGHTING

Accessibility

The Accessibility for Ontarians with Disability Act (AODA) provides the framework for the development of mandatory standards of accessibility. These development standards must be reviewed and implemented through the application of the AODA.

Guidelines

1. Accessibility issues should be explored and, if possible, addressed during any restoration work.
2. All new buildings and major renovations/additions to existing buildings will need to conform to existing legislation regarding universal design principles.

5.4 Area Specific Guidelines

In addition to the guidelines noted above for Infill and Redevelopment opportunities, area specific guidelines have been formed to add additional direction for development within specific areas of Hespeler Village.

Core Area/Queen Street

Guidelines

1. Pedestrians shall be the focus of new development in the Core Area through scale, massing, etc.
2. Buildings in the Core Area shall be oriented to the street to create a dynamic urban experience.
3. Street orientation will promote a sense of character, by encouraging large expanses of storefront windows, sheltered covered entrances and traditional architectural design characteristics.
4. Reduced setbacks are preferred in the downtown core.
5. New buildings shall be setback to align with anchor buildings at the corner of Queen Street and Adam Street/Guelph Avenue.
6. Arcades and covered colonnades

are not normally promoted, notwithstanding, Hespeler's recently restored example, as shown in the photo to the right.

7. Building materials typical of the Core Area/Queen Street area shall be incorporated in new building designs and infill. The typical building materials used in the Core Area/Queen Street are local limestone and brick.

8. Some examples of original architectural details exist and should be retained. Most buildings in the core would have had largely glass storefronts, with transom windows in decorative wooden frames. The store entrance would have been inset from the street to provide shelter from the elements and to provide increased viewing area for the display window. Many of the buildings in Hespeler have been altered from the original design, but fortunately many do not require major renovation work to restore them to their former glory.



Queen Street West of Guelph Street and Adam

Guidelines

1. Buildings shall be of similar height and width. The natural slope of the lots backing on to the Speed River should allow for a walk-out basement in the rear. Buildings with views should consider offering raised patios as a feature to overlook views.

2. Buildings in this area will be oriented towards Queen Street at the front and the Speed River at the back to allow residents and commercial tenants to take advantage of the City Streetscape and views of the adjacent Speed River/Jacob's Landing.
3. Mixed use or residential buildings should be grouped into compact form to promote intensification.
4. Building fronts shall be at a similar line as neighbours using the existing setback of the anchor building on the North West Corner of Queen Street and Guelph Street (Ernie's Restaurant) as the guide.
5. Opportunities exist for mixed used development with businesses fronting onto Queen Street and having access to shared parking in the rear.

5.5 Recommendations for Implementation of Architectural Design Guidelines

What can City Staff Do?

Amend Zoning By-law

The City of Cambridge Zoning By-law currently reflects the redevelopment of the core area, but should be further detailed to include the addition of site specific zoning provisions. This will provide an opportunity for the implementation of the Urban Design Guidelines and Architectural Design Guidelines.

Include Municipal Heritage Advisory Committee (MHAC)

Any proposed alterations to heritage buildings or properties of interest must go through the Municipal Heritage Advisory Committee. The Heritage Committee should be equipped with the Urban Design Guidelines and Architectural Design Guidelines to help formulate opinion and recommendations for approval for updates to any designated heritage properties or properties of interest.

Provide Opportunities for Funding

- Cost sharing and/or joint ventures with private developers should be explored.
- Promotion of Heritage Grant Programme
- Promotion and implementation of Façade Improvement Program(s)

What Can the Business Improvement Area (BIA) Do?

The Hespeler Business Improvement Area (BIA) should be encouraged to create awareness within their respective membership to educate all affiliates of funding opportunities and programming available to help better the Business Improvement Area. The awareness can be completed through:

- Information sessions at BIA meetings (include a member of City Staff to describe details of Façade Improvement Program).
- Email reminders to membership regarding programs and funding available for improvements.
- Providing contact information (City Staff contacts, Architectural Contacts) for assistance of the façade improvement program to all members.

Implementation Goals



The implementation goals of the Urban Design Guidelines and Architectural Design Guidelines are as follows:

- Continue to protect heritage resources within Core Areas.
- Utilize the described guidelines in this document for any development, adaptive reuse, or façade improvement projects.
- Ensure all City Staff (including Planning, Building, Economic Development) are familiar with and utilize the proposed guidelines Urban Design and Architectural Design Guidelines.
- Ensure the Municipal Heritage Advisory Committee is familiar with and implements Guidelines for all improvement projects.
- Continue to dedicate the Reurbanization Planner (Senior Policy Planner) of City Staff to promote improvement program and encourage redevelopment within core areas to follow the guidelines.
- Create new Official Plan policies for redevelopment in core areas.
- Re-evaluate Zoning in core areas (specifically Milling Road Area) to allow a variety of uses and development forms. This may require consultation with the Grand River Conservation Authority.
- Create awareness of Façade Improvement Program through BIA Marketing to all members.
- Secure Funding for the Façade Improvement Program.



PART

06 Costing Estimates and Phasing

06

6.1 Breakdown of Phasing and Cost Estimates

A general cost estimate for the proposed design of the subject study area has been prepared based on the advice of Transportation Public Works Staff for a potential phasing breakdown.

The following phasing and cost estimates have been broken down on a logical section basis. The purpose is to assist City Staff determine the funding required to enhance the streetscaping elements.

Miscellaneous Items

Provisional Items

Phase 1: Queen Street (Harvey Street to Cooper Street)

Phase 2: Adam Street (Queen Street to Project Limits)

Phase 3: Guelph Avenue (Queen Street to Project Limits)

Phase 4: Tannery Street East (Adam Street to Queen Street East)

Phase 5A: Public Square- Short Term Option

Phase 5B: Public Square - Long Term Option

Phase 6: Milling Road - City Owned Land

Phase 9: Milling Road - Potential Future Development Option

6.2 Assumptions

A set of assumptions has been developed for the upgrading to the streetscaping features in Hespeler Village. It is important to note that all cost estimates are based on the high level conceptual plan and are subject to change as the detailed design is undertaken.

- Based on Preliminary Design.
- All existing asphalt, curb, gutter and sidewalk within the right of way of all roads within the project limits are priced to be removed based on assumed road cross- sections in the absence of as-built drawings.
- New roadway construction includes 80mm HL4 base and 40mm HL3 surface asphalt (to be confirmed by geotech consultant).
- Standard curb and gutter for all roads.
- Sidewalk widths vary as shown on streetscape drawings.
- No provision for removal/replacement of existing road base granular materials.
- No provision for removal/replacement of sewers and watermains at all roads within project limits.
- No provision for hydro poles or traffic light relocation/replacement.
- No provisions for grade changes, surplus removal based on thickness of new roadway.

6.3 Miscellaneous Items

Any updates to the streetscaping elements within Hespeler Village will be funded and completed by the City of Cambridge. For every phase of construction that is completed separately from one another, it is advised to factor in the following Miscellaneous Items, for general construction set up items and undertakings that will run throughout the life of the project.

The cost estimates for the Miscellaneous Items that are required for each phase have been developed using the following criteria:

Criteria:

- Mobilization and Demobilization
- Provide bonding and insurance
- Provide field office
- Construction layout
- Traffic control and maintenance
- Project sign boards
- Temporary construction fencing
- Tree protection
- Pre-construction Survey
- Garbage Pick up
- Vacuum excavation and disposal of earth (provisional)

Item	Fee Estimate (approximate)
TOTAL	\$110,000.00

All draft costing subject to change based on detailed design.
These are high level estimates.

6.4 Provisional Items per Phase

The following Provisional items should be considered for budgetary purposes for each phase of construction for the upgrades to the Hespeler Village:

Criteria:

- Water/Calcium for compaction/dust control
- Removal of unsuitable subgrade material
- Air Quality downtime
- Supply, place and maintain Granular ‘A’ for temporary pedestrian or vehicle access
- Cold asphalt mix
- Erosion and sediment control
- Contaminated soil/materials

Item	Fee Estimate (approximate)
TOTAL	\$70,000.00

All draft costing subject to change based on detailed design.
These are high level estimates.

6.5 Phase 1: Queen Street (Harvey Street to Cooper Street)





Evaluation Criteria:

Removals:

- Remove existing light standards
- Remove existing roadway surface – including disposal off site (curb and gutter, sidewalk, asphalt)
- Contingency = 50%

Streetscaping:

- Supply and install metal detectable warning plates/truncated domes
- Supply and plant deciduous trees – including grates, guards and strata cells
- Supply and install benches
- Supply and install seatwalls
- Supply and install bollards
- Supply and install flexible parking
- Contingency = 20%

New Roadway Surface:

- Scarify, reshape and compact base
- Base course asphalt
- Surface asphalt
- Adjusting manholes and catch basin frames, etc.
- Concrete curb and gutter
- Concrete sidewalk
- Pavement markings
- Contingency = 25%

Streetlighting:

- Supply and install new light standards
- Supply and install buried PVC conduit
- Supply and install electrical wires
- Supply and install handwells
- Cast in place concrete light standard foundations
- Contingency = 20%

Item	Fee Estimate (approximate)
Part 'A' – Removals	\$165,000.00
Part 'B' – Streetscaping	\$640,000.00
Part 'C' – New Roadway Surface	\$600,000.00
Part 'D' – Street lighting	\$280,000.00
Sub Total	\$1,685,000.00
HST	\$219,000.00
TOTAL	\$1,904,000.00

All draft costing subject to change based on detailed design. These are high level estimates.

Total Length: 575.0m

Average Cost per Linear Metre (excluding HST): \$3000.00

6.6 Phase 2: Adam Street (Queen Street to Project Limits)



Evaluation Criteria:

Removals:

- Remove existing light standards
- Remove existing roadway surface – including disposal off site (curb and gutter, sidewalk, asphalt)
- Contingency = 50%

Streetscaping:

- Supply and install metal detectable warning plates/ truncated domes
- Supply and plant deciduous trees – including grates, guards and strata cells
- Supply and install benches
- Supply and install seatwalls
- Supply and install bollards
- Supply and install flexible parking
- Contingency = 20%

New Roadway Surface:

- Scarify, reshape and compact base
- Base course asphalt
- Surface asphalt
- Adjusting manholes and catch basin frames, etc.
- Concrete curb and gutter
- Concrete sidewalk
- Pavement markings
- Contingency = 25%

Streetlighting:

- Supply and install new light standards
- Supply and install buried PVC conduit
- Supply and install electrical wires
- Supply and install handwells
- Construct cast in place concrete light standard foundations
- Contingency = 20%

Total Length: 129m

Average Cost per Linear Metre (excluding HST) = \$1300.00



Item	Fee Estimate (approximate)
Part 'A' – Removals	\$22,000.00
Part 'B' - Streetscaping	\$45,000.00
Part 'C' – New Roadway Surface	\$82,000.00
Sub Total	\$149,000.00
HST	\$19,370.00
TOTAL	\$168,370.00

All draft costing subject to change based on detailed design. These are high level estimates.

06



Evaluation Criteria:

Removals:

- Remove existing light standards
- Remove existing roadway surface – including disposal off site (curb and gutter, sidewalk, asphalt)
- Contingency = 50%

Streetscaping:

- Supply and install metal detectable warning plates/truncated domes
- Supply and plant deciduous trees – including grates, guards and strata cells
- Supply and install benches
- Supply and install seatwalls
- Supply and install bollards
- Supply and install flexible parking
- Contingency = 20%

Total Length: 235m

Average Cost per Linear Metre (excluding HST) = \$2100.00

All draft costing subject to change based on detailed design. These are high level estimates.

New Roadway Surface:

- Scarify, reshape and compact base
- Base course asphalt
- Surface asphalt
- Adjusting manholes and catch basin frames, etc.
- Concrete curb and gutter
- Concrete sidewalk
- Pavement markings
- Contingency = 25%

Streetlighting:

- Supply and install new light standards
- Supply and install buried PVC conduit
- Supply and install electrical wires
- Supply and install handwells
- Construct cast in place concrete light standard foundations
- Contingency = 20%



Item	Fee Estimate (approximate)
Part 'A' – Removals	\$52,000.00
Part 'B' - Streetscaping	\$170,000.00
Part 'C' – New Roadway Surface	\$150,000.00
Part 'D' – Street lighting	\$121,000.00
Sub Total	\$493,000.00
HST	\$64,090.00
TOTAL	\$557,090.00

6.8 Phase 4: Tannery Street East (Adam Street to Queen Street East)



Evaluation Criteria:

Removals:

- Remove existing light standards
- Remove existing roadway surface – including disposal off site (curb and gutter, sidewalk, asphalt)
- Contingency = 50%

Streetscaping:

- Supply and install metal detectable warning plates/truncated domes
- Supply and plant deciduous trees – including grates, guards and strata cells
- Supply and install benches
- Supply and install seatwalls
- Supply and install bollards
- Supply and install flexible parking
- Contingency = 20%

Total Length: 155m

Average Cost per Linear Metre (excluding HST): \$4,300.00

New Roadway Surface:

- Scarify, reshape and compact base
- Base course asphalt
- Surface asphalt
- Adjusting manholes and catch basin frames, etc.
- Concrete curb and gutter
- Concrete sidewalk
- Pavement markings
- Contingency = 25%

Streetlighting:

- Supply and install new light standards
- Supply and install buried PVC conduit
- Supply and install electrical wires
- Supply and install handwells
- Construct cast in place concrete light standard foundations
- Contingency = 20%

Item	Fee Estimate (approximate)
Part 'A' – Removals	\$40,000.00
Part 'B' – Streetscaping	\$410,000.00
Part 'C' – New Roadway Surface	\$132,000.00
Part 'D' – Street lighting	\$85,000.00
Sub Total	\$667,000.00
HST	\$86,710.00
TOTAL	\$753,710.00

All draft costing subject to change based on detailed design. These are high level estimates.



6.9 Phase 5A: Public Square (Short Term Option)

06

HESPELER VILLAGE



Evaluation Criteria:

Removals:

- Remove existing structures off site
- Contingency 20%

Streetscaping:

- Supply and install deciduous trees and strata cells
- Supply and install benches
- Prepare raised garden bed
- Supply and install interlocking pavers
- Supply and install pre-cast concrete unit pavers
- Public art
- Way finding signage
- Top soil and sod
- Contingency 20%

Item	Fee Estimate (approximate)
Part 'A' – Removals	\$16,000.00
Part 'B' - Streetscaping	\$110,000.00
Sub Total	\$126,000.00
HST	\$16,380.00
TOTAL	\$142,380.00

All draft costing subject to change based on detailed design. These are high level estimates.

6.10 Phase 5B: Public Square (LongTerm Option)



Evaluation Criteria:

Removals:

- Remove existing structures including off-site disposal
- Remove existing concrete surface, steps, etc (includes disposal)
- Remove existing concrete raised planter including off-site disposal
- Earth excavation, hauling and disposal off-site
- Contingency 50%

Streetscaping:

- Supply and install deciduous trees and strata cells
- Supply and install benches
- Prepare raised garden bed
- Supply and install interlocking pavers
- Supply and install pre-cast concrete unit pavers
- Public art
- Way finding signage
- Concrete slab
- Concrete amphitheater
- Top soil and sod
- Contingency 20%

Item	Fee Estimate (approximate)
Part 'A' – Removals	\$76,000.00
Part 'B' - Streetscaping	\$330,000.00
Sub Total	\$406,000.00
HST	\$52,780.00
TOTAL	\$458,780.00

All draft costing subject to change based on detailed design. These are high level estimates.

6.11 Phase 6A: Milling Road City Owned Land

06



Evaluation Criteria:

Removals:

- Remove existing light standards
- Remove existing roadway surface – including disposal off site (curb and gutter, sidewalk, asphalt)
- Contingency = 50%

Streetscaping:

- Supply and install metal detectable warning plates/truncated domes
- Supply and plant deciduous trees – including grates, guards and strata cells
- Supply and install benches
- Supply and install seatwalls
- Supply and install bollards
- Supply and install flexible parking
- Contingency = 20%

New Roadway Surface:

- Scarify, reshape and compact base
- Base course asphalt
- Surface asphalt
- Adjusting manholes and catch basin frames, etc.
- Concrete curb and gutter
- Concrete sidewalk
- Pavement markings
- Contingency = 25%

Streetlighting:

- Supply and install new light standards
- Supply and install buried PVC conduit
- Supply and install electrical wires
- Supply and install handwells
- Construct cast in place concrete light standard foundations
- Contingency = 20%

Item	Fee Estimate (approximate)
Part 'A' – Removals	\$25,000.00
Part 'B' – New Roadway Surface	\$50,000.00
Sub Total	\$75,000.00
HST	\$9,750.00
TOTAL	\$84,750.00

All draft costing subject to change based on detailed design.
These are high level estimates.

6.12 Phase 6B: Milling Road Potential Future Development Option with Private Sector

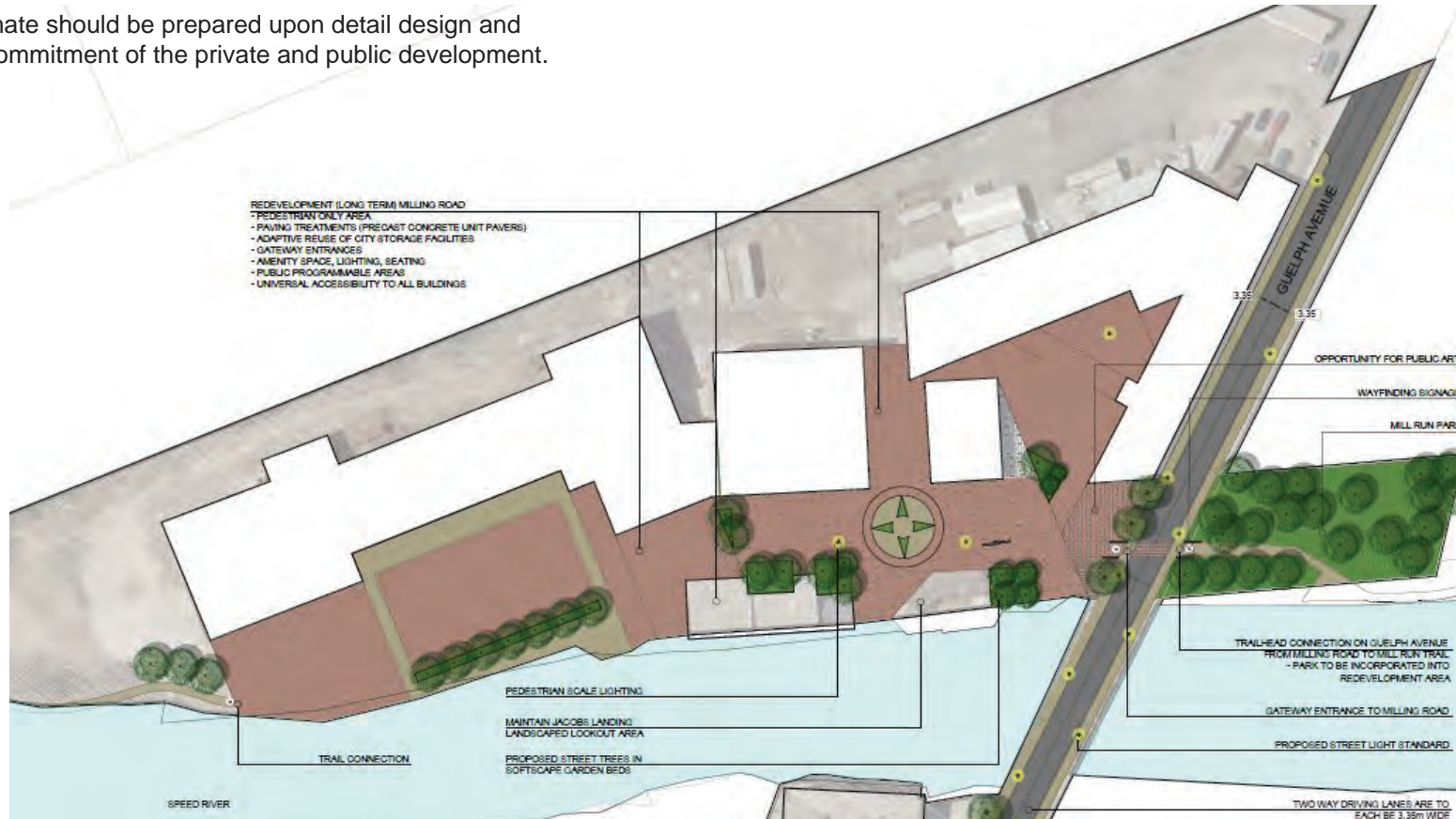
This report has identified a future development option for the Milling Road area that could be undertaken by a partnership between the City of Cambridge and the Private Sector owner of the Milling Road Site.

The design is only a high level conceptual document to identify a future development option. The actual future design could vary as it is dependant on the vision of the private property owner for the future uses and operations of the subject site.

With so many variables, no cost estimate has been developed for the purposes of this report.

A detailed cost estimate should be prepared upon detail design and understanding the commitment of the private and public development.

Item	Fee Estimate (approximate)
Costing to be determined upon detailed Public/Private Development Design	N/A



6.13 Full Streetscape Cost Estimate - Hespeler Village

The total estimated costs for the upgrade of streetscaping features as per the conceptual design for Hespeler Village are:

Item – King Street (from/to)	Fee Estimate (approximate)
Phase 1 – Queen Street (Harvey St to Cooper St)	\$1,685,000.00
Phase 2 – Adam Street (Queen St. to Project Limits)	\$149,000.00
Phase 3 – Guelph Avenue (Queen St to Project Limits)	\$493,000.00
Phase 4 – Tannery Street East (Adam St to Queen St. E)	\$667,000.00
Miscellaneous	\$110,000.00
Provisional Items	\$70,000.00
Sub- total	\$3,174,000.00
HST	\$412,620.00
TOTAL	\$3,586,620.00

All draft costing subject to change based on detailed design. These are high level estimates.

6.14 Full Streetscape Cost Estimate plus Public Space - Hespeler Village

Item	Fee Estimate (approximate)
Full Streetscaping of Study Area (including HST)	\$3,586,620.00
Public Square – Phase 5A (including HST)	\$142,380.00
Milling Road – City owned land upgrades (including HST)	\$84,750.00
TOTAL	\$3,813,750.00

Item	Fee Estimate (approximate)
Full Streetscaping of Study Area (including HST)	\$3,586,620.00
Public Square – Phase 5B (including HST)	\$458,780.00
Milling Road – City owned land upgrades (including HST)	\$84,750.00
TOTAL	\$4,130,150.00

PART

07 Recommendations for Implementation

07

7.1 Recommendations for Implementation of the Hespeler Streetscaping Plans

The build out of the proposed conceptual design is dependent on funding available for the various phases of construction. IBI Group staff has consulted with the Transportation Public Works Department to review what time schedules they had already set in place for City road reconstruction projects. Transportation Public Works noted that the reconstruction of Queen Street was the first (and only) project in their schedule slated for reconstruction in the upcoming future.

1 At the advice of City Staff and the priorities determined as part of the public consultation process the City of Cambridge should continue to move forward with the reconstruction of Queen Street (for the entire length of the study area) and incorporate the proposed design guidelines and streetscaping upgrades as proposed as part of this report. If funding is not available to accommodate all streetscaping elements proposed as part of the conceptual design along Queen Street, it is strongly recommended that the replacement of light standards be the number one priority in terms of streetscaping upgrades.

2 The second phase recommended for reconstruction should be the detailed design and construction of Tannery Street. Another significant priority identified through the public process was to create a strong connection between Forbes Park and Queen Street. The redevelopment of this street to include the Grand Alee, upgraded entrance to the park and way finding signage will create the connection needed for publicly programmed events in the park and access to the main street (Queen Street) to enjoy the upgraded streetscaping design and to access the businesses.

3 It is recommended that City Staff review the proposed conceptual design for the public park space on Queen Street and determine if funding is available to create the short term or long term vision. The preferred redevelopment is the long term option (if funding is available) to create a large open plaza that can be publically programmed to host weekly farmers markets, public and privately programmed events and a meeting space for visitors, residents and business owners.

4 The remainder of the phasing should be assessed based on the need for infrastructure upgrades or road improvements.

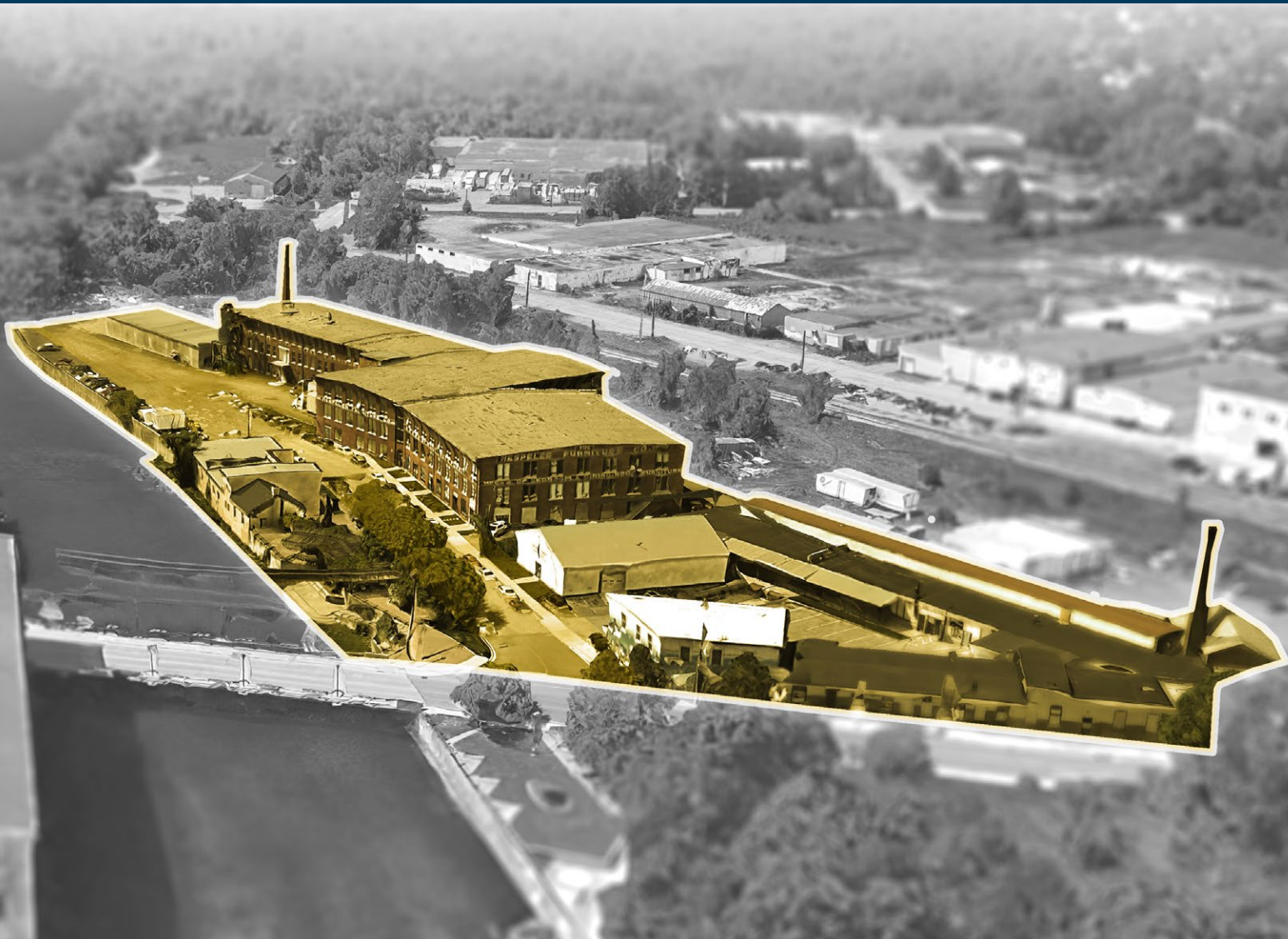
5 If the owner of the Milling Road privately owned lands comes forth with an option for site improvements, City Staff should use the opportunity during that time to prepare a detailed design to the City owned right of way and buildings, so the site can fully operate as a unique destination. Development in this area is dependent on the vision of the owner of the private land, but the City could potentially move ahead with the redevelopment of the industrial building currently used for storage and set a precedent for the future development of the Milling Road site.

When funding is secured for the proposed redevelopment of each phase, it is recommended that the City of Cambridge undertake the detailed design process involved to create an accurate streetscape plan that follows the design guidelines proposed within this report. The detailed design program will include an accurate survey of the existing conditions, a detailed design, detailed cost estimates that reflect the market of the time period and a team to tender and administer the contract for construction.

Although the ultimate build out of the conceptual streetscape design is a long term vision, the City of Cambridge should move forward with the proposed phasing to make small changes over a period time to reach the ultimate goal and needs of the Hespeler Village Community.



MARTIN **SIMMONS**
ARCHITECTS



Milling Road

STREETSCAPE PLAN & URBAN DESIGN GUIDELINES

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Mandate

In January 2020, the City of Cambridge sought urban design services to build on the 2013 Hespeler Streetscape Plan & Urban Design Guidelines (SUDG) to develop a specific streetscape plan and guidelines for the Milling Road area in Hespeler Village. The 2013 Hespeler SUDG outline a Phase 6A option and a Phase 6B option for Milling Road. Phase 6A involves basic streetscape improvements on City owned land. Phase 6B outlines a potential future option for partnership based around development of the two non-City owned parcels along Milling Road.

In developing these Guidelines, three stakeholder consultation sessions and one Public Information Centre (PIC) were held throughout process. The intent of these sessions was to elicit discussions from key stakeholders on the potential for revitalization of Milling Road. The sessions began as high level brainstorming and listening sessions, followed by a detailed review of logistics and barriers to allow for revitalization to occur.

These guidelines are intended to be read in conjunction with the policies of the Official Plan and all applicable City policies and guidelines including but not limited to:

- Hespeler Village Streetscape Plan & Urban Design Guidelines (October 2013)
- Cambridge Heritage Master Plan (June 2008)
- Arts & Culture Master Plan (March 2009)
- Hespeler Village River Activation Master Plan (February 2003)
- Cambridge Cycling Master Plan (2020)

Part One

STUDY & CONSULTATION

Background Study

Purpose & Objectives

In January 2020, the City of Cambridge sought urban design services to build on the 2013 Hespeler Streetscape Plan & Urban Design Guidelines (SUDG) to develop a specific streetscape plan and guidelines for the Milling Road area in Hespeler Village. The 2013 Hespeler SUDG outline a Phase 6A option and a Phase 6B option for Milling Road. Phase 6A involves basic streetscape improvements on City owned land. Phase 6B outlines a potential future option for partnership based around development of the two non-City owned parcels along Milling Road.

These Guidelines aim to encapsulate the feedback received from stakeholder input, public engagement and technical review with City staff.

Vision

Milling Road is ideally located on the Speed River with proximity to residential, commercial and industrial properties. The local architecture and historic significance provide a compelling story for visitors, and a unique opportunity to create an alluring sense of place. Precedents such as Toronto's Distillery District and Cambridge's own Gaslight District offer successful models that Milling Road can draw upon. Riverfront real estate is highly sought after, and Milling Road has the opportunity to become it's own sustainable model for Hespeler Village.

With these guidelines Milling Road will become a safe, local and pedestrian friendly destination with historical character, creating a unique sense of place within Hespeler Village.



Figure 1: Hespeler Village Streetscape Concept from the Hespeler Village Streetscape Plan & Urban Design Guidelines

Background Documents

Hespeler Village Streetscape Plan & Urban Design Guidelines

The following points have been excerpted from the Hespeler Village SUDG. For full text and further context, refer to the original document:

- Partner with the City of Cambridge to provide consistent hardscaping treatments to create a consistent barrier free plaza across Milling Road. The hardscaped treatment around the entire site, will allow for programming spaces for business owners and for City-run events.
- Provide a connection to the Mill Run Trail through the site to provide additional access to industrial, commercial and potential residential uses.
- All parking and deliveries should be provided at the rear of the industrial facility with access off of Guelph Avenue. When deliveries cannot be made to the rear of the facility, a plan for deliveries could be created to best suit businesses and pedestrian activities. (ex. Sparks Street in Ottawa). The plaza area should remain accessible for emergency vehicles.
- Provide shade trees, seat walls, benches, bike racks, pedestrian scale lighting, waste and recycling receptacles and way-finding signage throughout the development.
- All parking and deliveries should be provided at the rear of the industrial facility with access off of Guelph Avenue. When deliveries cannot be made to the rear of the facility, a

plan for deliveries could be created to best suit businesses and pedestrian activities. (ex. Sparks Street in Ottawa). The plaza area should remain accessible for emergency vehicles.

- Partner with the City of Cambridge to provide seasonal lighting throughout the entire pedestrian zone.
- Provide up-lighting for the facades of the former industrial buildings.
- Follow the Architectural Design Guidelines included in this report to aid in the adaptive reuse opportunity.

Study Area

30 Milling Road

In 2016 and 2018 the City of Cambridge hired external consultants to complete condition assessments for the City-owned building at 30 Milling Road. The latest assessment in 2018 noted architectural, mechanical, and electrical components of the building are all in poor condition. The building is currently being rented to a short-term tenant to operate business and store supplies. Although the building does not pose an immediate risk, major renovations will be required to allow long-term use of the building. The building foundation is structurally part of the flood wall along the riverbank and was noted in the 2018 report as having visible erosion to the stone masonry. The risk for contamination will need to be assessed in greater detail to understand if any is present on site, and if removals are necessary to meet environmental standards.

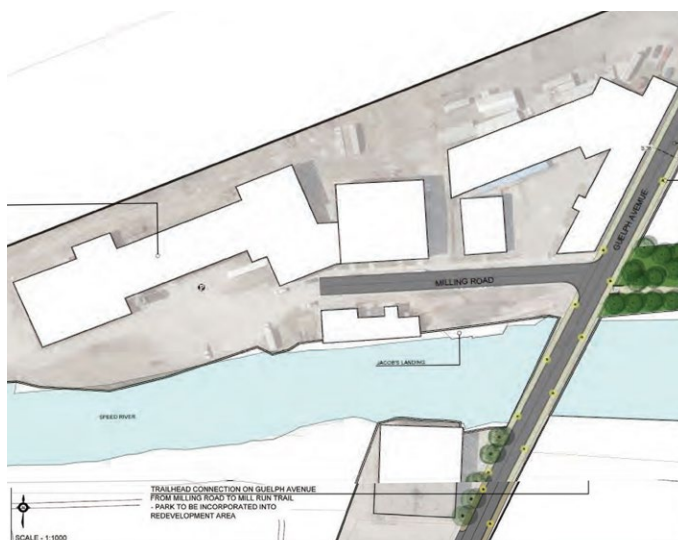


Figure 2: Milling Road existing conditions diagram from the Hespeler Village Streetscape Plan & Urban Design Guidelines



Figure 3: Milling Road revitalization concept from the Hespeler Village Streetscape Plan & Urban Design Guidelines

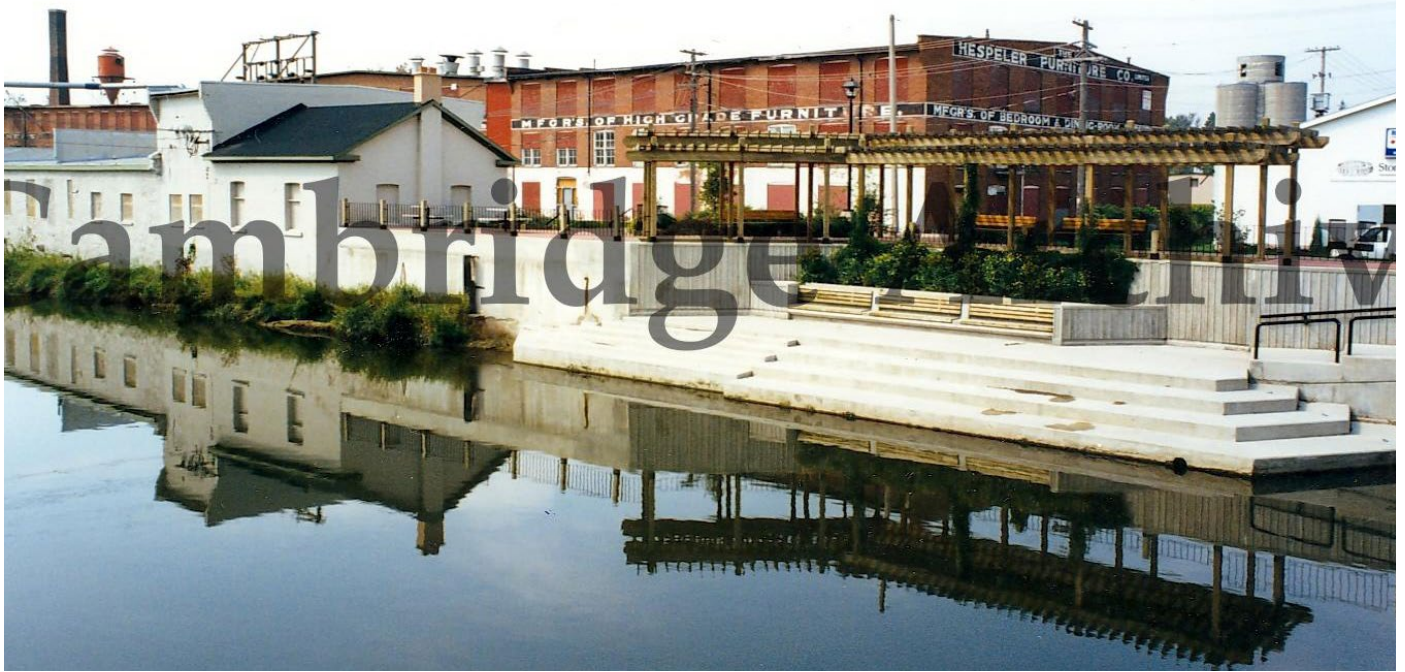


Figure 4: Photo of Jacob's Landing Park from the City of Cambridge Archives

Mill Run Trail

A 6.5km woodland trail that connects Hespeler to Preston along the north and south shores of the Speed River. The trail borders Riverside Park, Chilligo Conservation Area, Jacob's Landing and Hespeler Mill Pond. It sits on the original 1895 rail bed of the "Galt, Preston, and Hespeler Street Electric Railway". The Mill Run is a channel for a flow of water that powers the water wheel in a watermill. Water powered mills grinding flour and sawing timber were established along the riverbanks in the 1840's. Three mill runs are located along trail.

Jacob's Landing

Jacob's Landing is a 2.7ha Park divided by Guelph Avenue. The portion of Park included in these Guidelines is west of Guelph Avenue and on the south side of Milling Road. With riverfront access, Jacob's Landing features a canoe/boat launch, fishing opportunities and views across the river. Through stakeholder consultation it was expressed that Jacob's Landing is currently underutilized by the public, and these guidelines will explore opportunities to increase use of the site.

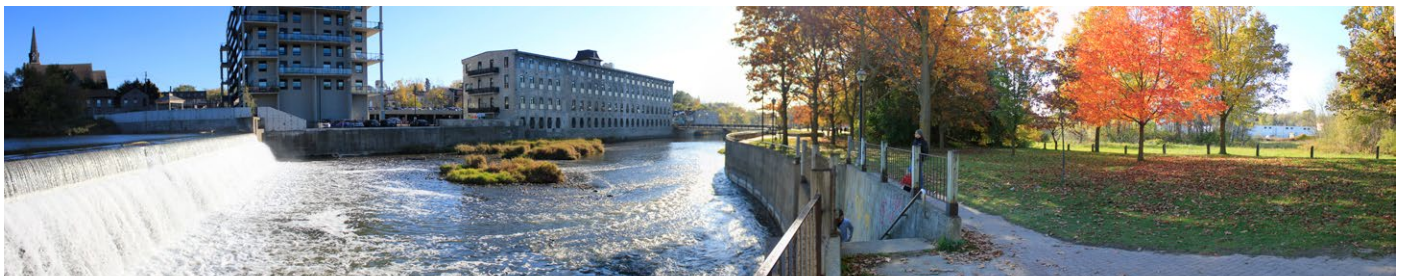


Figure 5: Panoramic photo of Hespeler Falls Speed River Dam and Riverbank lofts in the background (historically the American Standard industrial complex)



Figure 6: Panoramic photo of Mill Run Trail nearby Milling Road

Neighbouring Properties

There are two private properties on the north side of Milling Road: 25 Milling Road and 54 Guelph Avenue. These properties are both zoned for industrial use and are currently rented to a variety of businesses in the commercial and industrial sector. The opportunity to allow additional zoning types may be considered to diversify business and increase traffic to Jacob's Landing and Milling Road.

25 Milling Road has several entrances fronting on to Milling Road, with primary parking on private property at the terminus of Milling Road. The primary parking lot for 54 Guelph Avenue is accessed from Milling Road and is adjacent to Jacob's Landing Park. Pickup and deliveries are an important logistical consideration for the owners and tenants. Both properties have several transport trucks accessing the loading docks each day. The atypical parcel shape of 25 Milling Road makes it difficult for large vehicles to maneuver the site, with only one way in and out.

Local Heritage

The Hespeler Furniture Company

The Hespeler Furniture Company was established in 1901 by Mr. George A. Gruetzner (originally from Buffalo NY). Former salesman and manager of The Simpson Co. of Berlin (now Kitchener). He served on the Parks Board, Town Council, and was elected as mayor of Hespeler 1925-1930.

American Standard

Built between 1847 and 1901 for Jacob Hespeler as an industrial complex, the original portion of the building was used as a saw mill harnessing the power of the Speed River. It later evolved into a grist mill before being converted for milling cloth. In 1913 the property was purchased by Stamped and Enameled Ware, and in 1969 it was taken over by American Standard. In 2016 the complex was converted to condominium buildings - The Standard (10 storey, 152 units apartment building) and Riverbank Lofts (riverside mill

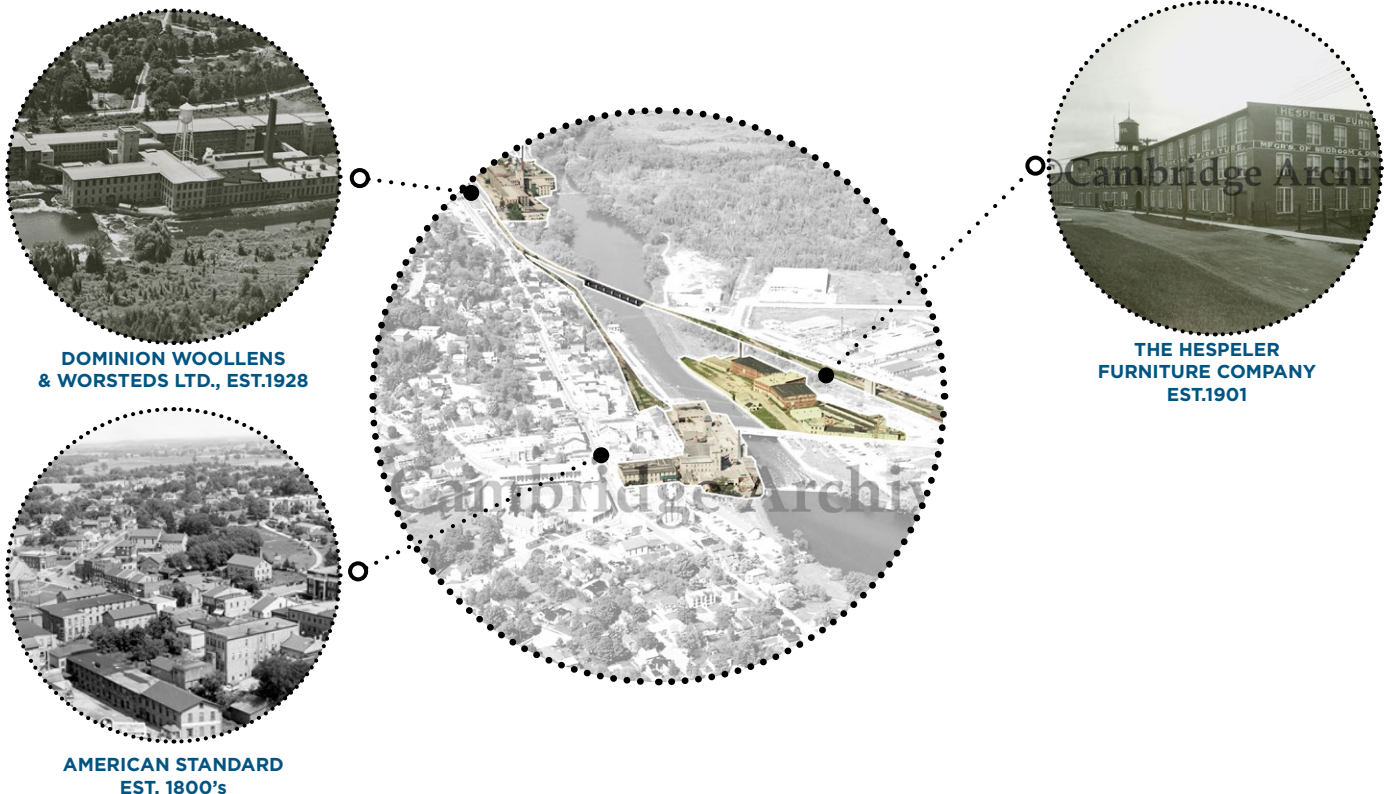


Figure 7: Diagram showing the location of Dominion Woollens & Worsteds, American Standard and The Hespeler Furniture Company within Hespeler Village.

building converted to 45 condominium units). Various cornerstones of existing buildings were incorporated into signage and landscape features throughout the site.

Dominion Woollens & Worsteds Ltd.

In 1880 Robert Forbes took over J.Schofield & Co. and established R.Forbes & Co. It ran until 1928 when the company renamed to Dominion Woollens and Worsteds Co.Ltd. and was in operation until 1959. At this time it was purchased by Silknit, the largest woollens and worsteds mill in the British colonies at the time - employing almost one third of all Hespeler village citizens. During WWII the company employed hundreds of 'mil girls' from across Canada to make Canada's wool for military uniforms. Production ceased permanently in 1984, and shortly after one third of the building was destroyed in a fire. Currently the building is rented for retail stores that front on Queen Street East at the intersection of Guelph & Queen E.

Precedent Research

Streetscape Revitalization

Larch Street, Waterloo

Larch Street is a one-block street in Waterloo's Northdale Neighbourhood and was identified in the Northdale Neighbourhood Streetscape Master Plan to be reconstructed using a Woonerf Street typology (a first for the City of Waterloo). A Woonerf Street is one that is a 'shared Street' where the users of a right-of way are accounted for in a setting that has enhanced landscape attributes. Users (automobiles, cyclists and pedestrians) share the same right-of-way in a curb-less street design. This curb-less design allows for increased streetscaping, including trees and seating areas, while accommodating all users. As part of the design, attention is given to prioritizing pedestrians over vehicles, which encourages active transportation, naturally reduces vehicle operating speeds and increases safety.

Saint Lawrence Market Street, Toronto

Located in Toronto's historic St. Lawrence neighbourhood, Market Street is a mixed use, multi-purpose and flexible public street. The street is framed by historic commercial buildings, including St. Lawrence Market, restaurants and boutiques.

The design incorporates built-in flexibility for seasonal adaptation. During the summer months, the sidewalk lends itself to patio space for restaurant, while in the winter months it allows for on-street parking. Removable bollards and the absence of traditional stepped curbs allow for flexible programming of the streetscape.

The streetscape cross-section includes precast concrete unit pavers on a concrete base and bollards in lieu of raised curbs. Pavers were selected to respect the heritage context and clearly delineate pedestrian and vehicular zones through tonal contrast for AODA compliance.

Berczy Park, Toronto

Berczy Park is a small downtown park nestled amongst the Financial District, St. Lawrence the Old Town neighbourhoods. The park is focused around a plaza space with a large fountain and detailed granite paving motif. Gentle berms frame the plaza for passive activities and screening from adjacent roadways. Pathways run diagonally through the space to lengthen sight lines and increase the perceived size of the park.

The continuing surge in the number of dog owners has been manifest into a signature and playful fountain that merges a classic Victorian two-tiered fountain with a menagerie of spouting life-sized dog figures. Inspired by the park's past and present, it will become a destination in the city appealing to children and attracting visitors. A discrete and playful dog-watering trough around the perimeter of the granite fountain basin accommodates the thousands of dog visits expected in the park on a weekly basis.

Philadelphia Green Streets Design Manual

Offering a detailed design template for green streets, Philadelphia's Green Streets Design Manual outlines appropriate green stormwater infrastructure appropriate for various street typologies. The Manual provides standardized procedures and design details that can be adapted to our local context.

Riverfront Revitalization

Erie Street Plaza, Milwaukee

Erie Street Plaza is an ecopark that manages stormwater by collecting runoff to support a designed wetland that's part of the site. The Plaza aims to re-charge groundwater and utilizes river

water for irrigation.

The plaza was designed to be flexible for programmed events, such as art festivals, gatherings, concerts, movies, weddings, festivals, farmer's markets, and winter carnivals. Passive activities include boat-watching, fishing and relaxing on the waterfront.

Industrial Adaptive Reuse

Distillery District, Toronto

The Distillery District is a commercial and residential district located east of downtown Toronto. The heritage buildings of Gooderham & Worts Distillery were re-purposed to house cafés, restaurants, and artisan shops. The district comprises more than forty heritage buildings and ten streets, and is the largest collection of Victorian-era industrial architecture in North America.

The internal brick-paved streets and lanes are restricted to pedestrians and cyclists, with vehicular traffic restricted to streets and parking areas outside of the district's historic centre. Several large sculptures installed along the lanes enliven its streetscapes. There are informal public spaces on the pedestrianized streets with chairs and tables for general use, as well as formal patios for some of its coffee houses and restaurants. Trinity Street is the widest street in the district and often functions as a public square for events such as market days.

Artscape Wychwood Barns, Toronto

Wychwood Barns is a former industrial complex of five buildings that has been converted into a community centre through adaptive reuse of the original structures. The community was engaged to identify the missing activities that Wychwood Barns could provide. The exterior envelope was largely retained during restoration. Barn 1 is a private live-work studio and housing for community artists, while Barn 2 is a community gathering space. Barns 3 and 4 are private-public spaces where non-profit organizations can operate. A greenhouse and community gardens are located in Barn 4. Barn 5 was stripped of its roof and south wall, leaving the steel structure exposed to form an arcade.

Scales of Intervention

Speed River

The site is perched on the edge of the Speed River and provides unique opportunities for users to engage and interact with the riverfront. It also provides an opportunity to leave the site better than we found it by enhancing the riverfront ecological value. Currently the interface between the subject site and the Speed River is dominated by concrete.

Looking upstream of the site, the Grand River is provided the space to widen and meander before being channelized at Hespeler Falls. By the time water reaches Jacob's Landing and 30 Milling Road the River follows a straight channelized path with concrete on both sides. Looking downstream of the site, the Grand River remains channelized for approximately 850m before meandering again.

Considerations should be made to soften the landscape between the river's edge and Jacob's Landing while respecting the requirements set out by the Grand River Conservation Authority. This can be achieved through simply incorporating naturalized plantings along banks and limiting the amount of hard surface at the river's edge.

Industrial Heritage

The industrial heritage of Hespeler Village and Milling Road is an important part of the local history and should be prioritized in the design narrative. Refer to the Built Form Section for additional information on specific buildings, including their local, regional and national significance.

Village Core

Significant effort has been made in the preparation and implementation of Hespeler Village Streetscape Plan & Urban Design Guidelines. Refer to the Guidelines for detailed information for landscape and architectural character to be considered along Milling Road.

Inventory & Analysis

Land Use

Milling Road Right-Of-Way

Milling Road is a 16m wide right-of-way with a 1.5m wide sidewalk and 9m roadway inclusive of on-street parking on the north and south sides of the road. Large shade trees along the south side of the road straddle Jacob's Landing.

City-Owned Easement

Between 25 Milling Road and 54 Guelph Avenue there is a narrow utility easement that is currently being utilized by 25 Milling Road as a drive aisle to the rear of the property.

30 Milling Road

This parcel is currently occupied by a City-owned building and Jacob's Landing Park. The building was previously occupied by City departments, and is currently being rented for short-term lease. Jacob's Landing features a large pergola with bench seating that overlooks the Grand River and is ideal for group photography. River access provides visitors with opportunities to fish and launch a canoe or kayak.

25 Milling Road & 54 Guelph Avenue

Both parcels are currently zoned industrial with large buildings for industrial and commercial business. 25 Milling Road has multiple commercial entrances that front onto Milling Road, and 54 Guelph Avenue has 1 commercial entrance.

Both parcels have access from Milling Road, with large lots for vehicular parking and loading access for delivery and pickup.

Landscape Character

54 Guelph Avenue

54 Guelph Avenue Unit #1 at the intersection of Guelph & Milling is on the property line. A concrete sidewalk occupies the boulevard and terminates at the building face. A traditional light standard at the intersection provides historic character and ties in with the design language of Hespeler Village.

54 Guelph Avenue Unit #10 has a slight setback from the right-of-way and is planted with mown grass.

25 Milling Road

A 1.5m wide concrete sidewalk with an asphalt kill strip extends the length of 25 Milling Road. Doorway connections consist of varying hardscape treatments, including: concrete, asphalt and unit pavers. Mown lawn occupies all space between doorway connections.

Jacob's Landing

A wood pergola extends along the Grand River and anchors the park. A mature vine covers a majority of the pergola and contributes to the historic character and feel of Jacob's Landing.

The large retaining wall that separates the upper and lower levels of Jacob's Landing provides character with board formed concrete. The elongated steps at the lower level provide river access, while contributing to the unique character of this site.

30 Milling Road

The City-owned building is utilitarian with asphalt parking and aged concrete sidewalk. The interface with Jacob's Landing incorporates unit pavers that extend into the Park. Jacob's Landing has large mature shade trees and shrubs with low plantings of juniper, rose, hostas and daylilies.

Architectural Character

Milling Road is not a long street and contains relatively few buildings. Of the buildings on the street, most are of pre-WWII vintage. The design of these buildings sets the tone for the street as well as for this part of town. The effect is traditionally pleasant and speaks of the industrial heritage of the community.

Roof slopes are low, providing an almost flat roof look along Milling Road. The deep roof eaves and corbelled masonry projections at the ends of 25 Milling Road impart the suggestion of a deep cornice running the full length of the building.

54 Guelph Avenue

54 Guelph Avenue Unit #1 is a two-storey brick building that has been painted many times over the years. Located prominently on the corner of Guelph & Milling, this building has an opportunity to stand out and anchor the entrance to Milling Road.

54 Guelph Avenue Unit #10 is a large industrial shed with stucco and siding. The building is modest in appearance and unique in its divergence from the rest of the buildings on the street. As a very plain, modern-looking, single storey structure, with the gable end facing the street, it interrupts the otherwise uniform architectural character of Milling Road.

25 Milling Road

25 Milling Road is a three-storey red brick building established in the 1800's as a factory. The industrial history, red brick and painted signage lettering all contribute to the sense of place and character that is sought after for spaces of industrial adaptive reuse. The building makes up over half of the street's length and dominates the look and feel of the space. It should be used as a baseline model for a redevelopment template. It is a somewhat neglected but attractive structure. The long, three storey facade sets the tone for the street and provides a comforting degree of enclosure along this relatively narrow roadway.

Except for the window above the main entrance to 25 Milling Road, openings in the masonry walls are punched. These openings are small enough to be spanned by unit masonry lintels. These punched openings accommodate traditional window sizes and configurations, typically of fixed wood framing with smaller operable sashes. The original windows have likely been replaced with units that have flat tops and arched wood infill pieces. The original doors would likely have been solid wood and have been updated to fully glazed aluminum leaves.

30 Milling Road

The City-owned building is a one-storey red brick building that has been visibly painted once. The architectural style is modest in construction in appearance. The building appears modular with walls extending and inseting along the length of Milling Road. Major renovations will be required to allow long-term use of the building, with architectural, mechanical, and electrical components all in poor condition. The building

foundation is stone masonry and concrete, and is structurally part of the riverbank flood wall. It was noted in the 2018 building condition assessment report as having visible erosion to the stone masonry.

The building foundation appears to have water damage where the foundation meets the Jacob's Landing flood wall. As noted in the condition assessment reports, the east end room is unsafe for use, and access has been blocked off. The central area contains two offices with a kitchen and lunch area. Utility panels, boxes and ductwork are located and accessible from the central area. This portion of the building has been drywalled and painted, however it is still apparent major renovations will be required for long-term tenancy. The eastern drive shed is a large open space with four supporting beams. The concrete floor appears to be in good shape, with additional footings poured for the supporting beams. Pink fiberglass insulation has been added to the ceiling, with clear plastic and strapping to hold it in place. The roof shows visible signs of water damage, as the pink insulation has blackened over time.

Wayfinding & Circulation

The primary mode of transportation to access Milling Road is by vehicle. Cyclists and pedestrians can access Milling Road via Guelph Avenue and Mill Run Trail; however these modes are used less prominently than commercial and personal vehicles. Wayfinding and connectivity to Milling Road can be improved through economic incentive such as retail opportunities and commercial services. A cohesive design language can also improve the atmosphere and usability through signage, furnishings, and programming along the street. Incentive can also be provided beyond the limits of Milling Road through marketing and signage in Hespeler Village, along major highways, and City tourism documents.

Pedestrian Circulation

Pedestrian connectivity to Milling Road is tied to internal and adjacent vehicular parking, as well as access from Mill Run Trail. Foot traffic across the Guelph Avenue bridge should provide a stronger connection to Hespeler Village south of the Speed River. Improving this connectivity may create economic opportunities in both directions and reinforce Milling Road as part of the Village core. Increased focus on the pedestrian experience along Milling Road was noted as a priority during stakeholder consultation.

Cyclist Circulation

Prior to the mid-1980's Mill Run Trail continued along Milling Road and connected through 25 Milling Road. During this time, the owners of 25 Milling Road expressed concerns of vandalism and liability, and the trail connection was rerouted. Mill Run Trail currently terminates at Jacob's Landing, east of Guelph Avenue. Cyclists and pedestrians are required to walk or bike across Guelph Avenue and reconnect north of 54 Guelph Avenue along the Goderich-Exeter Railway. It was expressed that improving the connectivity of multi-modal transit through Milling Road was an important priority and will be considered as part of the long-term phasing of the Guidelines.

The Cambridge Cycling Master Plan (2020) has highlighted the need for an All Ages and Abilities (AAA) cycling and pedestrian route across the Speed River at Guelph Ave. This Guelph Ave bridge crossing is currently not AAA. The entire corridor is labelled as a 'constrained corridor', meaning that it has been identified as a desired and an important cycling connection; however, further review is required to consider how cycling facilities can be accommodated, given the other competing priorities for vehicles, goods movement, and/or transit. This bridge was identified as a 'long-term' improvement.

Vehicular Circulation

There are currently several locations where visitors can access parking, with various ownership and proximity to the site. Parking for the public is on-street parking along both sides of Milling Road. Parking is currently unmetered by the City, and space is limited to approximately 30 unmarked stalls. Private parking occurs on 25 Milling Road and 54 Guelph Avenue, with large commercial vehicles accessing both sites. 30 Milling Road currently has 3-4 parking spots in front of the building. Additional parking further from the site is located along Sheffield Street, with 63 angled and marked stalls. Access to Milling Road requires people to walk along Guelph Avenue. There is currently no connection across the tracks, however safety and liability would need to be addressed to provide a connection. Future public parking may occur south of the Speed River, however at this time, future parking remains unconfirmed.

Through stakeholder consultation it was expressed that a dedicated parking lot along the Speed

River is not a desirable land-use for Milling Road. Further, an urban park and pedestrian space would be better suited for this location. With the desire for increased economic development along Milling Road, parking will be considered in the Guidelines to address parking constraints. A comparable precedent in the City of Cambridge is L.A. Franks on Water Street North. The riverfront restaurant has a large parking lot that supports business for L.A. Franks and surrounding businesses.

Parking

Milling Road currently has on-street parking on both sides of the road. The stalls are not formally marked with paint, and the layout does not follow the latest City standard. Vehicles parking bumper-to-bumper on both sides of the road results in approximately 20 stalls for the average size vehicle. The current roadway design, in combination with the parking configuration makes it challenging for the large delivery vehicles that enter both private properties on a weekly basis.

Gateways & Nodes

The entrance to Jacob's Landing is ideally located at the intersection of Milling Road and Guelph Avenue. The entrance consists of an open paving



Figure 8: Map of regeneration and transition areas inclusive of Milling Road.

area and a single blue spruce tree. The gateway could benefit from entry features that draw people into the site by defining and anchoring the intersection. Potential nodes include the City-owned easement between 25 Milling Road and 54 Guelph Avenue, as well as the western terminus of Milling Road.

Lighting & Furnishings

Lighting along Milling Road is currently limited, however pedestrian scale lighting within Jacob's Landing was integrated as part of the original design. Through the public engagement process, and in consultation with local business, it was noted that lighting could be an area of improvement to improve overall safety for both sites.

Milling Road does not currently include any furnishings, such as benches, waste receptacles or bicycle parking. Jacob's Landing has plenty of seating opportunities at both the upper and lower levels. Waste bins are located in the park and emptied by City staff.

Planning Policy Review

Official Plan Policy Overview

Land Use

The Site is located within the Hespeler Village “Community Core Area” (Map 5) in the Cambridge Official Plan. Community Core Areas are envisioned as mixed-use areas that include commercial, office, institutional, recreational and cultural facilities. Residential uses are permitted above the street level and must ensure compatibility with neighbouring uses. Compact development is encouraged by reduced parking requirements.

Further to the Community Core Area designation, the Site is part of a “Regeneration Area” (Map 6) in the Cambridge Official Plan. Regeneration Areas are areas in the city that are anticipated to undergo a transition of uses, typically from former industrial to mixed-use or residential activities.

Such areas are not deemed employment areas for planning function purposes.

Floodplains & Natural Areas

The Site is located within the “Significant Valleys” in the Regional Environmental Features map (Map 9) of the Cambridge Official Plan. The Significant Valleys designation aims to maintain the Canadian Heritage River recognition by enhancing cultural heritage resources of recreational and scenic value.

Also, the Site is within the Regulatory Storm Floodplain designation (Map 10) in the Cambridge Official Plan, which is regulated by the GRCA. Development within floodplain zones is restricted or prohibited, while recreation and conservation activities are permitted subject to approval by the GRCA. Hespeler Village specifically is a Two Zone Floodplain Policy Area (Map 11), capturing both a Floodway portion (limited to existing buildings)

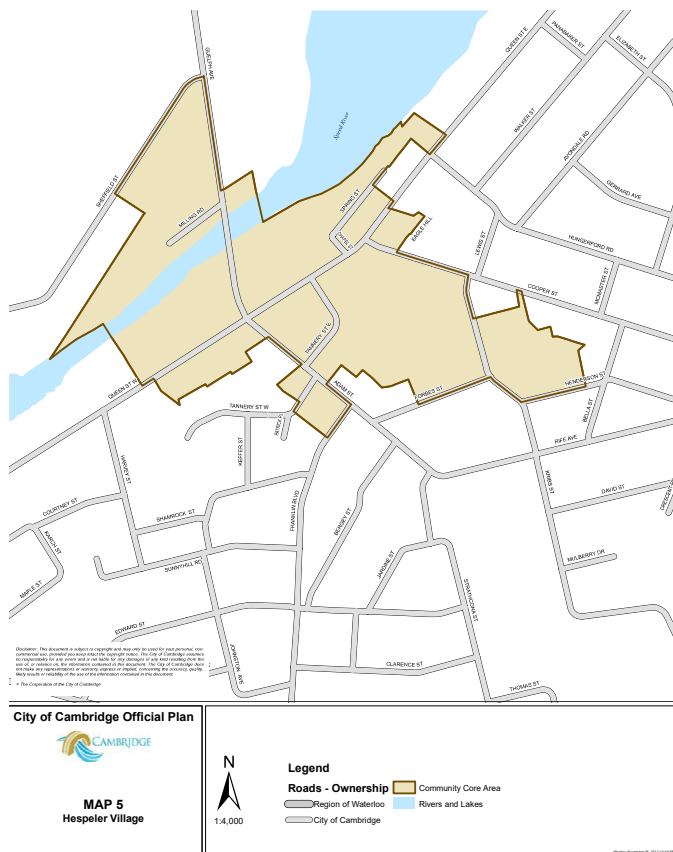


Figure 9: Official Plan Community Core Area inclusive of Milling Road.

versus Flood Fringe portion (new development subject to floodproofing and safety criteria). The Site is entirely within the Floodway portion.

Zoning By-Law Overview

The Site is subject to Cambridge Zoning By-law No. 150-85, the current in-force zoning for Cambridge. The privately owned properties of the Site are zoned Industrial (M3), which allows general industrial uses such as manufacturing, warehouse, storage and other similar uses. The Jacob’s Landing Park portion of the Site is zoned Open Space (OS1), which generally permits recreation activities.

The City is currently in the process of a new comprehensive zoning by-law that will replace the existing By-law 150-85 in keeping with the newer Cambridge Official Plan. The most current publicly-visible draft of the new comprehensive zoning by-law proposes to change the Jacob’s Landing Park portion of the Site to a Floodway (F) Zone and the private properties portion principally to a Core Mixed Use One (CMU1) Zone. The CMU1 Zone broadens the range of permitted uses from the current M3 Zone, including a mixture of different commercial uses and residential uses all subject to floodplain provisions and requirements.

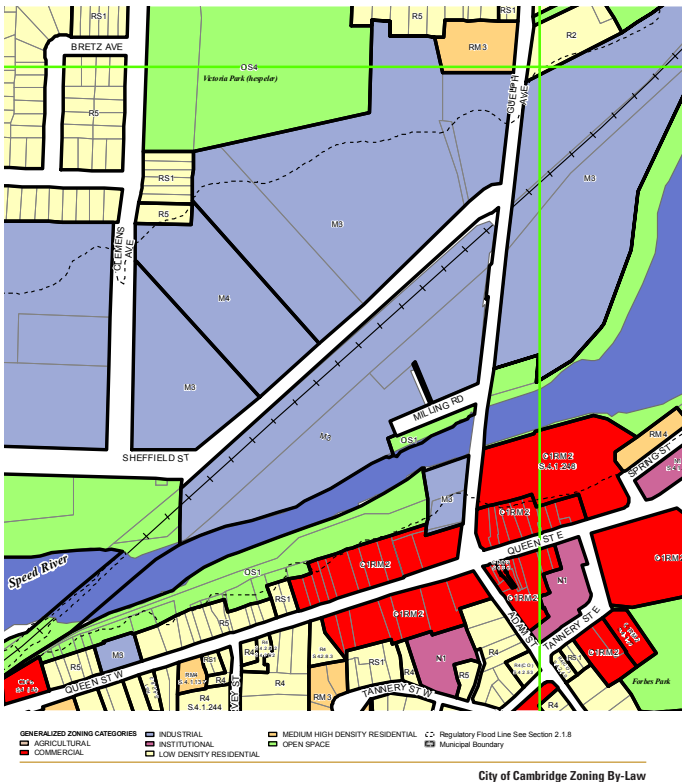


Figure 10: Map of the zoning for Hespeler Village, Milling Road and lands to the north.

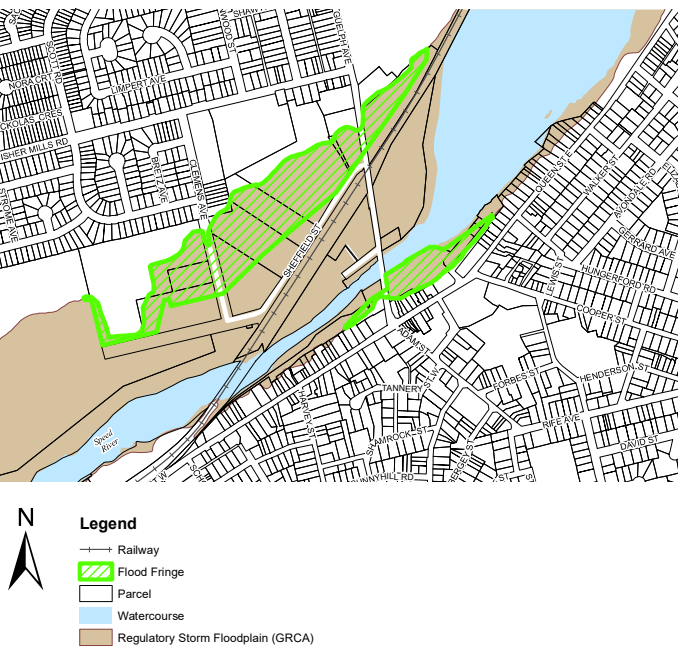


Figure 11: Map of the floodplain and flood fringe inclusive of Milling Road.

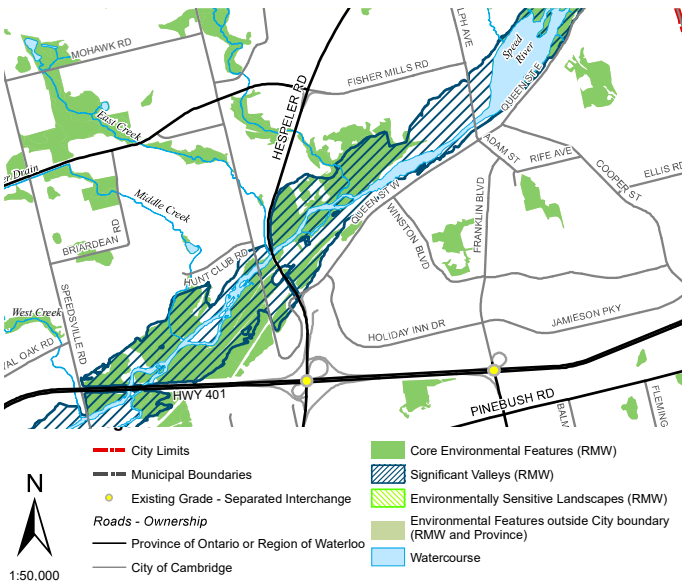


Figure 12: Map of environmental features, significant valleys and sensitive landscapes inclusive of Milling Road.

Public Engagement

Stakeholder Meetings

Through consultation with stakeholders, it was clear that the public building at 30 Milling Road is a priority, and stakeholders would like to see the building preserved and restored for adaptive reuse. The BIA and Councillors expressed interest in preserving the buildings for their historic value and economic opportunities to local businesses in Hespeler Village. Should the building be preserved and renovated, the site offers unique opportunities with its historic appeal and context along the Speed River. Commercial zoning may offer a unique dining experience or small business venture to draw an audience from the greater geographic area. Small businesses along Milling Road, and foot traffic through Jacobs Landing should also increase with strategic reuse of the public building.

The stakeholders also expressed that the removal of the building should be considered in the Guidelines, to provide a comprehensive set of options for opportunities on the site. Zoning restricts the construction of new buildings within the floodplain, therefore options for removal will require careful consideration. Given the scope and recommendations of the final Report, it may be recommended that a separate project be completed to address the public building at 30 Milling Road.

The BIA voted on their preferred concept and support Concept 2 - Phase 2A. This road design is curved with staggered parking on either side and where the existing City owned building is left intact.

Public Information Centre

The Public Information Centre was held in July 2021 to review the draft guidelines and concept designs and receive additional comments from the public. The event received a low turnout, so it is recommended that additional public engagement should be conducted during detailed design to allow additional opportunities for public input.

During the Public Information Centre, attendees reiterated the message that 30 Milling Road is an important part of Hespeler Village's fabric and should be remain as such. The subject lands are

part of the community core, and are therefore an important part of Hespeler's downtown. Efforts should be made to bolster this connection by creating opportunities for economic development and social programming, all in an effort to instill life back in to Milling Road and the surrounding community.

EngageCambridge First Survey

The first survey took place after the background study, inventory, analysis and planning policy review were complete.

During the first survey, 278 respondents provided answers to the survey questions in various format types. In total there were 11 single answer questions, 2 multiple answer questions, and 2 essay format questions. Respondents were able to skip each question, however the response rate was high for all questions.

1. 82% of respondents answered they have been to the site, suggesting that 18% of respondents live and/or work in the area, or are simply interested in the opportunities for Milling Road.
2. The most common use of the site is walking (with and without dogs), followed by: fishing, enjoying the views, relaxation, small gatherings, canoe/kayak launch, photography and access to Mill Run Trail.
3. Respondents were asked how they would like to use Milling Road and Jacob's Landing in the Future, choosing all options that apply. Of the 276 responses, 216 selected leisurely visits, 132 selected small gatherings and events, 191 selected farmers' market and 144 selected shopping.
4. 74% of respondents would like to see more commercial business opportunities on Milling Road, suggesting there are opportunities to increase social engagement and economic opportunities along the road.
5. Respondents were asked if they would like Milling Road to be designed as a traditional curbed road, or a curbless road with traffic calming measures. The response was even with a 48/52% split, respectively.

6. Respondents were asked what amenities they would like to see on Milling Road. Of the 277 responses, 237 selected seating, 245 selected pedestrian lighting, 197 selected bicycle parking and 244 selected waste receptacles.
7. On-street parking was carefully considered by City staff, GSP Group and stakeholders during the consultation process. We asked respondents if they would like Milling Road to have on-street parking, and 50% selected single-sided parking, 32% selected no on-street parking and 18% selected parking along both sides of the road. This suggests a reduction from the current condition of parking along both sides of Milling Road.
8. When asked whether Milling Road should have unique placemaking traits, 57% of respondents selected a historical theme. The other themes offered as options were industrial, creative, contemporary, and no theme. Each of these options were less than 10%, suggesting that a historical theme should be considered to play off the architecture and past of Hespeler Village and Milling Road.
9. When considering sustainability, respondents were asked if they would like to see measures implemented that exemplify sustainable practices (such as stormwater management, selective planting, material choices, etc.). 95% selected yes, suggesting additional considerations should be made to ensure sustainable practices are used throughout the design process.
10. When asked if street trees should be included as part of the streetscape design, 93% of respondents selected yes. Currently there are large mature trees along the south side of Milling Road. Measures may be taken to preserve these trees, or propose new trees within Milling Road and Jacob's Landing.
11. The current signage along Milling Road is limited to the intersection of Milling & Guelph within Jacob's Landing, and business signage along Milling Road. 68% of respondents agree that additional signage and wayfinding measures along Milling Road would be beneficial.
12. We received comments from stakeholders regarding perceived comfort and security within the site. When posed to respondents as part of the survey, they echoed these comments by suggesting that additional measures (such as lighting, traffic calming and broader sight lines) would be beneficial.
13. Respondents were asked if they would like Milling Road to be more animated with a sense of identity through public art. 77% of respondents answered yes. Developing a sense of identity can be achieved in many ways, however the survey suggests an interest in public art as one means of achieving this.
14. When asked if the City-owned building at 30 Milling Road were to be re-purposed the preferred re-use was food service at 46%. Event space came in at 12%, retail at 19% and removal at 8%.
15. The final survey question was for additional feedback in essay format. For the full list of feedback, refer to Appendix B. In summary, we received the following feedback:
 - The social, cultural and architectural history of Hespeler Village should contribute to the sense of place
 - The current trees should be saved
 - Careful consideration to the needs of 25 Milling Road and 54 Guelph Avenue
 - Remove existing City-owned building and extend Jacob's Landing
 - Connect Mill Run Trail through Milling Road
 - Community and pedestrian friendly design
 - Include retail, rental space and apartments
 - Promote the natural feel and maintain the stone feature
 - Restore building for shopping or residences
 - Enhance historic built form and "small town" charm
 - Promote idea of a town square
 - Restaurants, outdoor dining and food trucks
 - Provide more trees
 - Materiality to respect context
 - Pedestrian-only space

- Dog friendly space
- Less is more. Maintain and restore the site
- Provide more small shops
- Large blackboard for communal art, similar to the one at Guelph & Queen E
- Community garden
- Pollinator plants
- Family-oriented
- Children's play area
- Interactive elements such as art installations, historic information and animal conservation efforts.
- Maintain waterfront access
- City-owned building is the location of the first long distance hydro transmission line from Niagara Falls
- Art murals on the City-owned building
- Arts-oriented community district
- Carefully consider additional traffic
- Currently no secure bike parking
- Plow not able to sufficiently clear snow with parking on both sides of the road
- Parking should match anticipated use
- Build on existing pretense of brewerings with a district
- Proposed development should have proportionate density and height to maintain views
- Live/work opportunities
- Pedestrian crossing at Guelph & Milling

EngageCambridge Second Survey

The second survey took place once the draft guidelines were developed, and respondents were asked to comment on the concept designs and guidelines in their draft form.

In contrast to the first survey, only 25 respondents provided answers to the second survey. In total there were 5 single answer questions, 1 multiple answer question and 1 essay format question.

Milling Road Streetscape & Urban Design Guidelines

Respondents were able to skip the question for additional feedback (essay format), and approximately 1/2 chose to provide additional input.

1. When asked which streetscape concept they preferred, 12 selected the curving layout and 13 selected the straight layout. The number of responses are too small to extrapolate, however throughout the engagement process both options were supported, with slight preference to curving the road alignment.
2. Respondents were asked if specific activities should be given additional consideration (multiple answer). 8 respondents selected dog amenities, 12 selected space for children and play, 14 selected space for gatherings, 6 selected boating and 14 selected a stronger connection to Mill Run Trail.
3. Throughout the engagement process, the future of the building at 30 Milling Road was discussed at length. It is important to many engaged in the discussion that significant be made to preserve the buildings. When the question was posed in the survey, 0 respondents strongly preferred to preserve the building at all costs. 11 respondents believe significant efforts should be made to preserve the building, however they were open to the full park development. 14 respondents prefer to remove the building and see a larger park with more amenities. Given the small number of respondents in this survey, and the advocacy in previous engagement results, significant effort should be made to study this item in further detail.
4. Respondents were asked if they had a preference for paving type of the streetscape materials. 14 responded that the materials should contribute to the theme (historic or otherwise) by using materials such as unit pavers or stamped concrete. 8 respondents noted a preference for budget-conscious materials with priority given to the pedestrian paving types. 4 responded that they had no preference to streetscape materials.
5. When asked if the conceptual designs satisfy the draft vision statement, 0 respondents felt the vision statement should be revised, 2 respondents selected Concept 1 (alternate design) satisfied the vision statement, 4 respondents selected Concept 2 (preferred design) satisfied the vision statement, 16

respondents selected both concepts satisfy the vision statement, and 3 selected that neither concept satisfies the vision.

6. Respondents were asked if they would like to see additional opportunities for public art in the concept designs. 5 responded that multiple pieces is sufficient, 12 responded that additional public art in the streetscape should be considered to contribute to the sense of place, and 8 selected that they did not have a preference.
7. The final survey question was for additional feedback in essay format. For the full list of feedback, refer to Appendix B. In summary, we received the following feedback:
 - Wondering if there's potential for a raised platform to extend over the shoreline to connect with Mill Run Trail
 - Sports courts
 - Skate park
 - Single-sided parking makes it easier for drivers to anticipate movement and provides better sightlines.
 - Market space
 - Riverside dining
 - Provide opportunities for local business
 - Additional parking considerations for business on Milling Road
 - Develop the building into small business opportunities or residential units

Part Two

DESIGN

Built Form

Cambridge has a tradition of sympathetic but progressive design. All future development should match the scale, materiality and design features of the traditional industrial façade of 25 Milling Road. High quality progressive design is encouraged. During the detail design stage a Design Review Panel should be considered ensure appropriate and sensible design is presented for Milling Road.

The collection of small, single storey, traditional City owned masonry buildings at 30 Milling Road provide a unique and charming riverfront vista. They are also an excellent opportunity to animate and share this valuable public space with the larger community. Having lasted many decades intact, these buildings have the rare potential to tie the history of Hespeler through to the future. With minimal intervention, these structures should be able to be renovated in a manner that can retain their essential form, and at the same time allow for contemporary building programming.

The schematic design sketches show redevelopment strategies that provide greater flexibility, accessibility and transparency as well as the ability for a wide variety of functions to be accommodated. The goal for the renovations would be to create buildings that encourage public use and flexibility over time. This would be accomplished by opening up both the street and river side elevations to provide light and views into and through the structures as well as constructing a riverfront boardwalk and barrier free access to the river. These moves, accompanied by a possible rooftop terrace and increased internal connections, would allow for a wide variety of public or private sector amenities. These could include a gallery, a café or takeout restaurant, canoe, kayak or bike rentals, as well as interpretive or limited community centre type functions.

Contrary to some of the building audit material previously prepared, the structures at 30 Milling Road appear to be in relatively sound condition and should provide a solid basis for renovation into usable buildings. The existing masonry structures appear to provide the potential to be renovated to retain much of their traditional character while at the same time accommodating modern

building systems. A relatively small investment may provide the community with a significant, long term riverfront asset. Under no circumstances should these buildings be left to deteriorate or be demolished.

All architectural changes are subject to approval by the City of Cambridge and Grand River Conservation Authority, and should adhere to Hespeler's Two-Zone policies. Notwithstanding Policy 3.B.6.1.10, minor additions or alterations may be undertaken on buildings legally existing in the floodplain at the time of adoption of this Plan provided that:

1. no new dwelling units are created;
2. no new floor space (including basements) created within the floodplain is below the elevation of existing first floor space; and
3. such minor additions or alterations including mechanical and electrical services are generally floodproofed to the Regulatory Flood elevation. Under certain circumstances, reductions in the level of floodproofing may be permitted where deemed appropriate and feasible by the GRCA.

Building additions that extend towards the river or affect flood flow are not likely to be supported by the Conservation Authority.

Given that 30 Milling Road is located in a Two-Zone floodplain, there may be limitations that prevent certain commercial/retail business from occupying the space. The zoning provides more flexibility to municipal buildings, therefore it is preferable for 30 Milling Road to remain under City ownership to provide more business opportunities. For example, a business in the food service industry (ex. restaurant/pub, brewery, ice cream shop) or service industry (ex. Canoe rentals) may be difficult to approve under private ownership.

Built form content informed by MartinSimmons Architects.



Figure 13: Conceptual northern elevation of 30 Milling Road. Rendering prepared by MartinSimmons Architects.



Figure 14: Conceptual axonometric view of 30 Milling Road, including a roof top patio. Rendering prepared by MartinSimmons Architects.

Streetscape Design

Traffic Circulation

The current layout of Milling Road prioritizes vehicular traffic and on-street parking. Tenants and employees of 25 Milling Road access the property at the terminus of Milling Road. Tenants and customers of 54 Guelph Avenue access the parking lot from Milling Road. On-street parking does not follow the latest standard, as vehicles currently park bumper-to-bumper on both sides of the road. The current roadway design, in combination with the parking configuration makes it challenging for the large delivery vehicles that enter both private properties on a weekly basis.

The preferred layout of Milling Road aims to improve the current configuration by creating defined drive aisles and limiting parking to one side of the road at any given time. The preferred configuration staggers the on-street parking to create a jog in the road. This layout provides on-street parking on both sides of the road to benefit businesses to the north and park-goers to the south. It provides a traffic-calming effect while reducing the need for pedestrians to cross the road.

Fire truck and vehicular turnarounds are constrained within the Milling Road right-of-way. Careful consideration should be made during the design process in coordination with the City of Cambridge and adjacent landowners.

Pedestrian Circulation

Pedestrian circulation is currently limited to the north side of Milling Road, and through Jacob's Landing. With no sidewalk along the south side of Milling Road, the connection between business to the north and parkland to the south is lacking.

Stakeholders and survey respondents voiced interest in (1) prioritizing the pedestrian experience, and/or (2) transitioning Milling Road to pedestrian-only. The design of Milling Road should seriously consider the impacts of a pedestrian-only road, because the businesses of 25 Milling Road and 54 Guelph Avenue currently rely on vehicular access for parking, deliveries and the pickup of goods at 3 locations along Milling Road.

The preferred layout of Milling Road can function as a curbed or curbless road, however a curbless layout reinforces the pedestrian experience by creating a seamless north/south access. The paving treatments should reinforce the seamless experience by selecting patterns colours and tones that reinforce the seamless transition.

A 3m wide path framed by a double row of trees, also known as an allee, extends the length of Milling Road and aligns with the Mill Run Trail Connection across Guelph Avenue to reinforce the relationship with the Trail. Considerations should be made to allow future phases of work to reinforce the Trail connection.

Planting Opportunities

Proposed plant material should respect the heritage of Milling Road while balancing maintenance and drought tolerance. Planting within LID systems should be specifically suited to deal with the unique conditions. Mature tree cover should be considered a priority, and reasonable measures should be taken to expedite this process. This may include; preserving all existing trees where possible, specifying large caliper trees, amending existing site soils, specifying quality planting soil that meets the various moisture conditions and pH requirements, limiting exposure to pollutants such as salt.

Proposed street trees should be both unified in appearance and diverse in plant genus. The trees should share a similar habit (ex.. vase-shaped) to maintain a balanced canopy and symmetry along Milling Road. Providing a variety of tree genus will provides resilience to account for unknowns, such as: salt exposure, pest pressures, ice damage, etc.

Low Impact Development

Active and passive stormwater measures should be considered to slow the infiltration of runoff during storm events. Proximity to the Speed River warrants careful consideration, and LID practices should be coordinated with the Grand River Conservation Authority for input and approval.

Accessibility

Milling Road is relatively flat, and all business doorways along the north side provide unimpeded access. The grade at Guelph Avenue is a few metres higher than 30 Milling and 25 Milling, and this grade change presents a few challenges with accessibility into Jacob's Landing. To reinforce the narrative of Milling Road as a pedestrian environment, particular care should be given to address these grade changes through the use of retaining walls/ramps. These will likely be required as interim and final conditions along the interface between Milling Road and Jacob's Landing.

The preferred design does provide accessible access down to a viewing platform, however due to constraints of space and grades full accessible access down to water's edge is not attainable. Considerations should be made to reduce the grade change and allow future phases of work to provide a seamless transition for pedestrians at all other areas on the site.

Materiality

Decorative paving treatments across the sidewalk, boulevard and roadway should be coordinated to suit the design theme and historic character of Milling Road. The paving treatments should contribute to the pedestrian experience and be part of a cohesive materials board.

Additional consultation with 25 Milling Road should be conducted to integrate a cohesive paving design along the building outside the Milling Road right-of-way.

Furnishing selection, hardscape treatments and planting style should have a strong relationship to the local context and history. The Hespeler Village SUDG provide thorough recommendations on materiality and style. Refer to the Guidelines for additional information.

Street Lighting

Through various site visits and stakeholder consultations it was apparent that safety and lack of lighting was a high priority and an important design element that needs to be incorporated into the final design. The style of the proposed lighting should match/coordinate with the historical style that already exists within Hespeler's downtown area. The light should use the latest technologies such as LED luminaries and dark sky compliance.

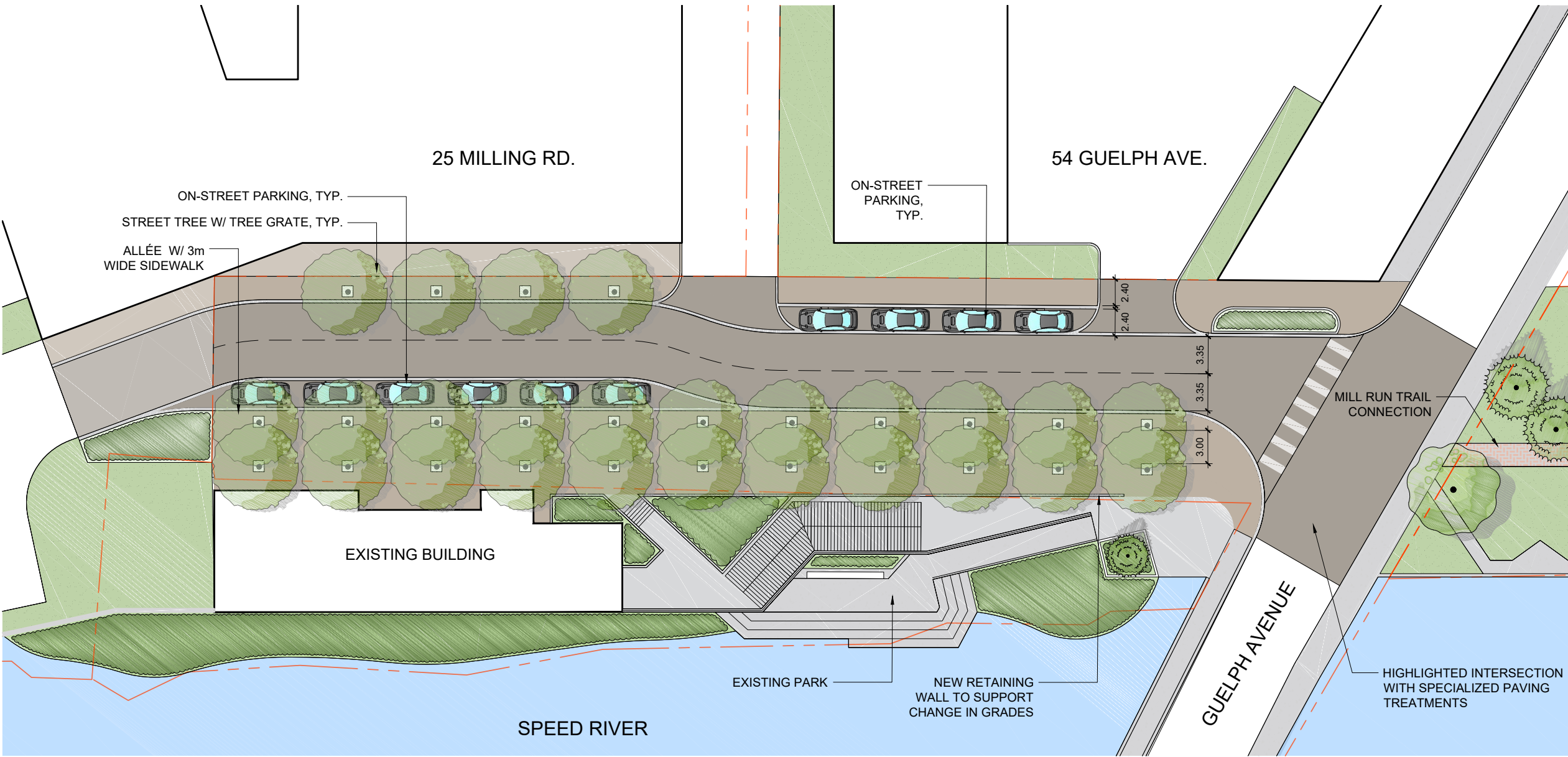
Lighting levels for all pedestrian areas and roadway along Milling Road should meet the City of Cambridge's minimum standards.

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN

SUMMARY

- STREETSCAPE:
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLEE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK
- MODIFICATIONS TO STREET FRONTAGE TO ACCOMMODATE NEW STREETSCAPE DESIGN



Preferred Streetscape Design

Figure 15: Conceptual design of Milling Road, including a curved roadway layout to provide traffic calming and stagger parking to serve both sides of the road.. A double row of trees frame a walkway on the south side of Millng Road, and is aligned with the Mill Run Trail connection on the east side of Guelph Avenue. The Existing Building and Jacob's Landing Park are shown for reference. Conceptual design and rendered plan prepared by GSP Group.

Park Design

Vision

Jacob's Landing should be designed as a local attraction for residents and tourists to enjoy seasonal events, such as farmers' markets and dining experiences. A historic design language should be considered to tie in the neighbouring architecture of Hespeler Village. Amenities within the park should strongly reflect the desired programming mentioned throughout the public engagement process to foster ownership and use by the public.

Programming

The public would like the space to be adaptable to their needs while providing opportunities for play, gathering and relaxing along the river. The park design should include a balance of flexible programming space and designed attractions. This may include open space for food trucks and events, and programmed space for quiet reflection or children's play. Open space should be proportionate to the size of Jacob's Landing to avoid excess hardscape treatment. Vertical elements are important to break up the visual plane in open space and maintain an appropriate sense of scale within the site.

Signature elements should be integrated into the programming to contribute to the sense of place. These elements may include, but not be limited to water features, gateway features, public art, riverfront dining and sun loungers. The park design concept includes a terraced deck at the upper

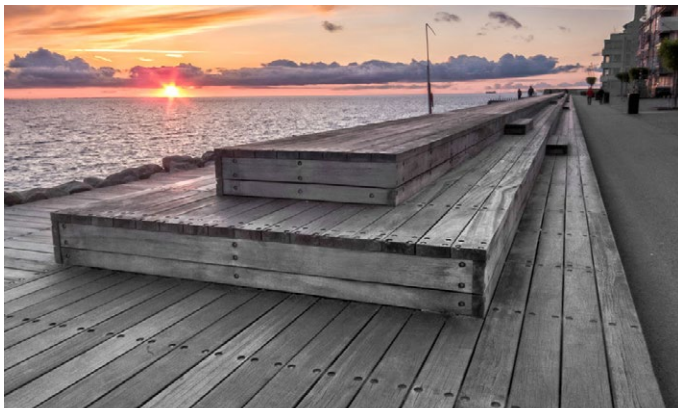


Figure 16: Terraced wood decking in Vastra Hamnen, Malmo, Sweden.

level to provide south-facing seating and views towards the Speed River. It also includes terraced seating and views inwards to the site, where users can view site programming (play area, food trucks, events, etc.).

Connection to the river has also been strength with providing accessible access down to a lower viewing deck. This elevated lower viewing deck maintains the original deck extents but at a higher and accessible point. Off to the side of the deck an access to the river via steps has been provided for water activities such as kayaking, canoeing, fishing etc. All works at this lower deck should be accessible and be subject to conservation, provincial and federal approvals for flood flows and in-water works for any renovations to the support structure.

Lighting & Furnishings

Lighting and furnishing selection should have a strong relationship to the local context and history. The Hespeler Village SUDG provide thorough recommendations on materiality and style. Refer to the Guidelines for additional information.

Planting Opportunities

Jacob's Landing currently has a wide variety of traditional plant species that perform well in the site. Proposed plant material should respect the heritage of Milling Road while balancing maintenance and drought tolerance. Planting within LID systems should be specifically suited to deal with the unique conditions. Mature tree cover should be considered a priority, and reasonable measures should be taken to expedite this process. This may include; preserving all existing trees where possible, specifying large caliper trees, amending existing site soils, specifying quality planting soil that meets the various moisture conditions and pH requirements, limiting exposure to pollutants such as salt.

Low Impact Development

Active and passive stormwater measures should be considered to slow the infiltration of runoff

during storm events. Proximity to the Speed River warrants careful consideration, and LID practices should be coordinated with the Grand River Conservation Authority for input and approval.

Accessibility

Interface with the City-owned building, river access and streetscape all require careful consideration to provide accessible transitions across the site. When possible, ramps and walls should be used sparingly to maintain seamless access with open sight lines.

Materiality

All materials should be selected for durability and low-maintenance. Detailing should consider ease of installation, maintenance and disassembly.

Decorative paving treatments should be coordinated to suit the design theme and historic character of Milling Road. The paving treatments should contribute to the pedestrian experience and be part of a cohesive materials board.

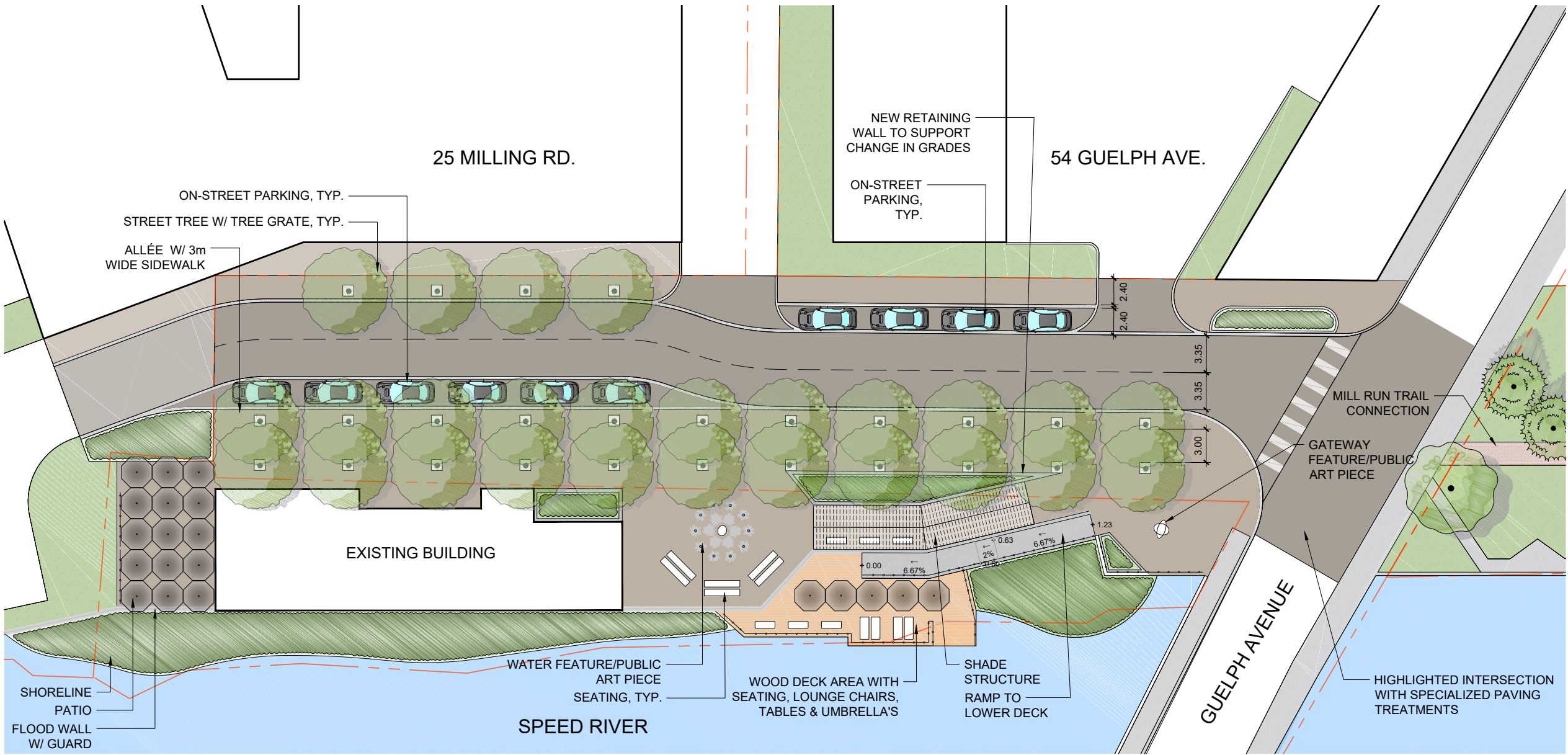
Furnishing selection, hardscape treatments and planting style should have a strong relationship to the local context and history. The Hespeler Village SUDG provide thorough recommendations on materiality and style. Refer to the Guidelines for additional information.

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN
- WOOD DECKING

SUMMARY

- STREETSCAPE:
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLEE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK
- WATER FEATURE WITH CUSTOM SEATING
 - SHADE STRUCTURE
 - WOOD DECK VIEWING AREA AT LOWER LEVEL WITH ACCESS TO WATER - ,MAINTAINS FOOTPRINT OF EXISTING AREA
 - STRENGTHEN ENTRANCE TO PARK WITH GATEWAY FEATURE/PUBLIC ART PIECE - OPPORTUNITY FOR COMMUNITY INVOLVEMENT



Park Renovation with Existing Building

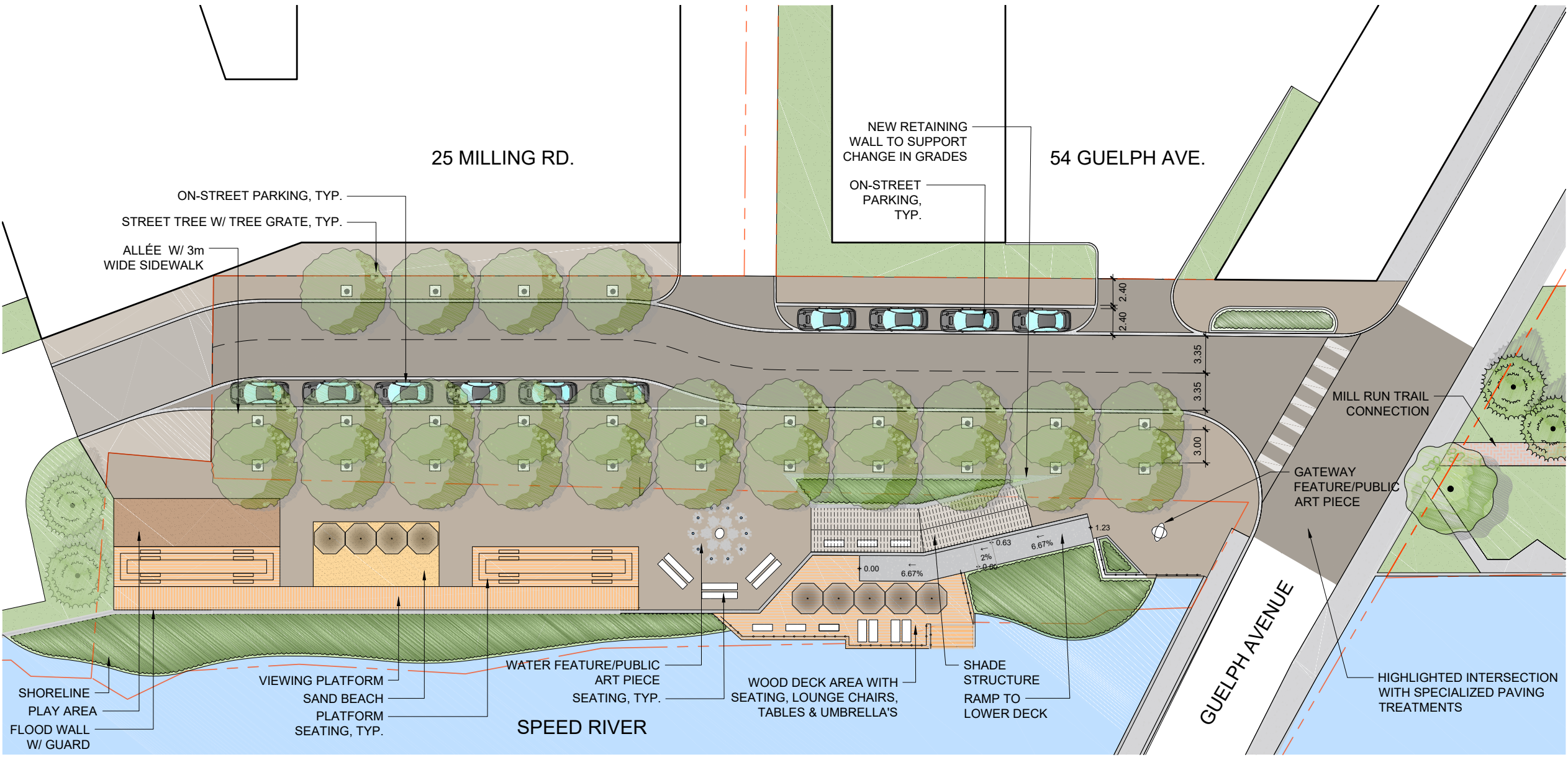
Figure 17: Conceptual design of Jacobs Landing Park including a the existing building and conceptual streetscape layout for reference. The Park design preserves the existing shorewall and lower boat launch. New park programming includes a patio for 30 Milling Road, water feature and/or public art, wood decking with lounge seating on the footprint of the lower boat launch, and a gateway feature at the corner of Guelph Avenue. Conceptual design and rendered plan prepared by GSP Group.

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN
- WOOD DECKING

SUMMARY

- STREETSCAPE:
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLEE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK
- FULL BUILD OUT OF PARK IN ADDITION TO CONCEPT 1A
 - VIEW AREA ALONG RIVERS EDGE WITH PLATFORM SEATING
 - PLAY AREA
 - BEACH FOR LOUNGING
 - OPEN SPACE FOR EVENTS AND FOOD TRUCKS



Full Park Development

Figure 18: Conceptual design of Jacobs Landing Park including the full park build-out with the building at 30 Milling Road removed. The preferred streetscape layout is shown for reference to integrate with the design. The Park design preserves the existing shorewall and lower boat launch. New park programming includes a viewing platform, platform seating, sand beach, play area, open space for flexible programming such as food trucks and events, a water feature and/or public art, wood decking with lounge seating on the footprint of the lower boat launch, and a gateway feature at the corner of Guelph Avenue. Conceptual design and rendered plan prepared by GSP Group.

Phasing & Costing

Assumptions

Costing summary provided for the preferred concept only. Costing for alternate concept provided in appendices for reference.

All costs associated with the restoration of the building at 30 Milling Road have not been considered within these Guidelines. A separate study is required to estimate the total costs associated with restoration. Such a study should consider the adaptive reuse recommendations provided by stakeholders and survey respondents in Appendix A & B.

Throughout the process of developing these Guidelines, three phases of development were considered.

Phase 1 - Streetscape

Based on the current condition of Milling Road and assets in the right-of-way (utilities, furnishings, pavement, etc.) the streetscape should be considered a priority over Jacob's Landing. The reconstruction of Milling Road should be well coordinated with local business to reduce disruption. Stakeholders noted a preference for a pedestrian-focused environment, with particular attention to vehicular circulation.

Preliminary high level cost summary has been provided below. A more detailed break down as been provided in the appendices.

Phase 2A - Jacob's Landing Park Renovation with Existing Building

Jacob's Landing Park may be phased separately from the roadway contract and should respond to design considerations laid out in phase one of the Guidelines. Given it's current condition, Jacob's Landing is not in urgent need of renovations, however it could better suit the needs of the community by integrating input received through the engagement process. In this phase the existing City owned building remains.

As mentioned above the preliminary high level cost summary provided below does not include costs for the building restoration. A more detailed break down for Phase 2A has been provided in the

appendices.

Phase 2B - Full Park Development

If it is determined that the City owned building is to be removed, it is our recommendation that the park renovation extend into the entire 30 Milling Road property. The design as shown in the preferred concept should respond to design considerations laid out in phase one of the Guidelines.

Preliminary high level cost summary for the full renovation of the park as been provided below. A more detailed break down for Phase 2B has been provided in the appendices.

Building Renovation - 30 Milling Road

It is recommended that the preservation and potential renovation of the existing building of 30 Milling Road be further investigated. At a very high level, building renovation budget can be anticipated to run anywhere around \$1.75 to \$2.5 million subject to final scope and further studies. This budget would potentially include full envelope renovation and replacement (ex. roof, walls, masonry, insulation, windows and doors, mechanical and electrical upgrades, new washrooms, lobbies, and basic interior finishes). Key risks to the budget are potential structural renovations at the riverfront, possible sewer and/or electrical service upgrades, and any possible environmental remediation.

There is risk that if the building is left to deteriorate further it can become unsalvageable to renovate and become a hazard to pedestrians. Certain sections of the building are already deemed unsafe as outlined in the Corporate Facilities Building Condition Assessment already completed in 2018 for this building. Immediate action in regards to 30 Milling Road is recommended.

Phase 1 - Streetscape

Costing

Part 1 - Site Preparation and Temporary Facilities	\$ 103,000.00
Part 2 - Paving	\$ 540,280.00
Part 3 - Planting	\$ 344,180.00
Part 4 - Site Elements	\$ 347,500.00
Part 5 - Site Furnishings	\$ 110,000.00
Subtotal	\$1,444,960.00
10% Contingency	\$ 144,496.00
13% Taxes	\$ 206,629.28
Total	\$1,796,085.28

Phase 2A - Park Renovation

Costing

Part 1 to 5	\$1,444,960.00
Part 6 - New Park	\$ 441,000.00
Subtotal	\$1,885,960.00
10% Contingency	\$ 188,596.00
13% Taxes	\$ 269,692.28
Total	\$2,344,248.28

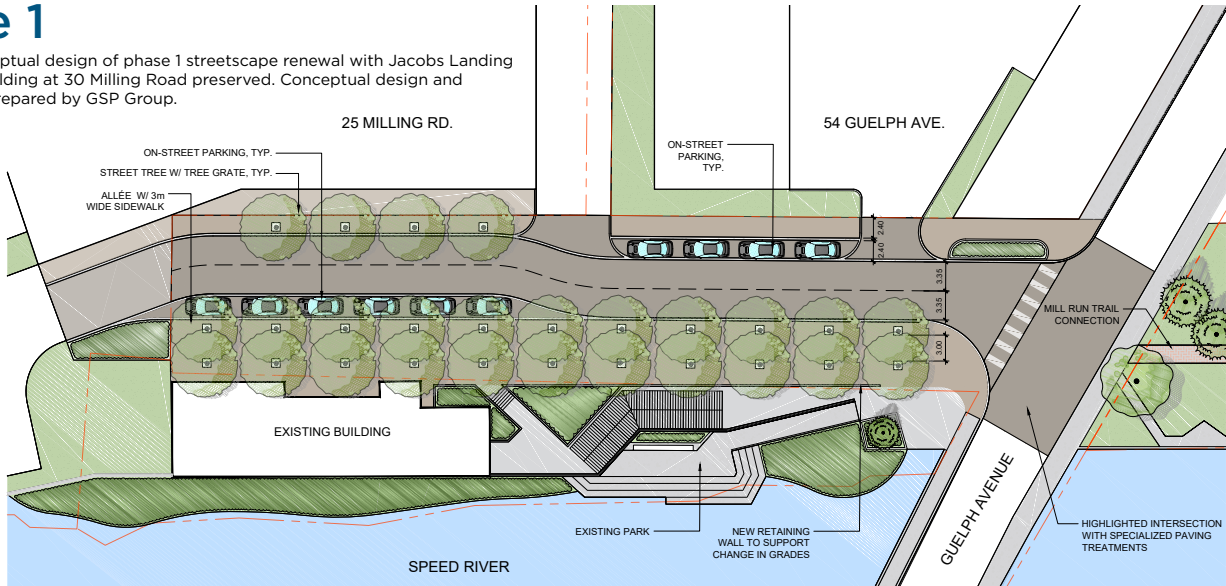
Phase 2B - Full Park Development

Costing

Part 1 to 5	\$1,444,960.00
Part 6 - New Park - Full Site	\$ 766,000.00
Subtotal	\$2,210.960.00
10% Contingency	\$ 221,096.00
13% Taxes	\$ 316,167.28
Total	\$2,748,223.28

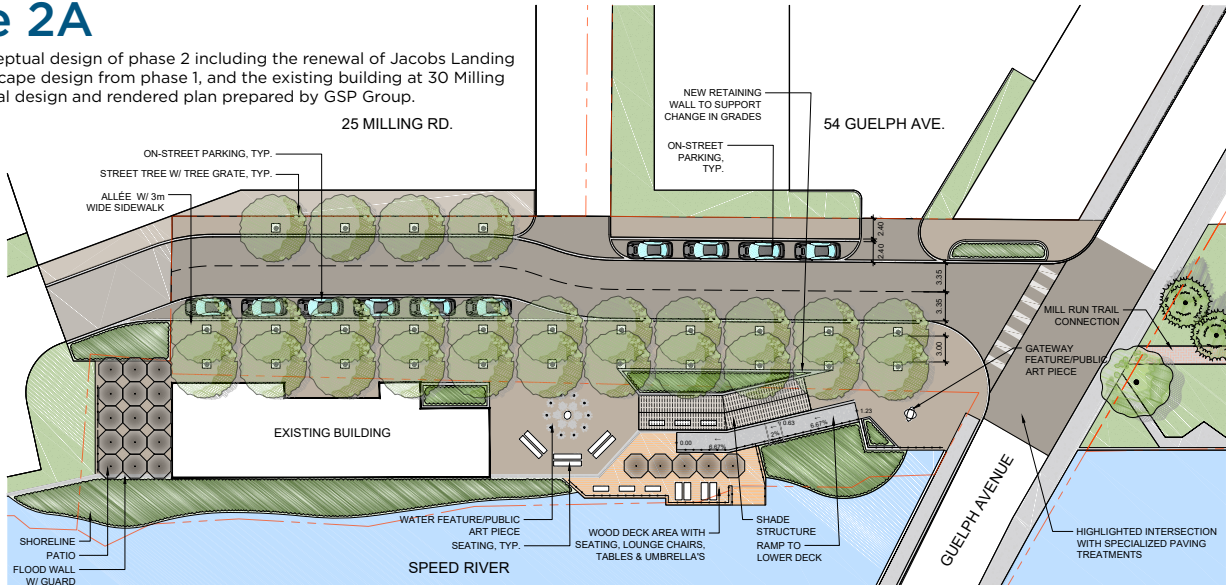
Phase 1

Figure 19: Conceptual design of phase 1 streetscape renewal with Jacobs Landing Park and the building at 30 Milling Road preserved. Conceptual design and rendered plan prepared by GSP Group.



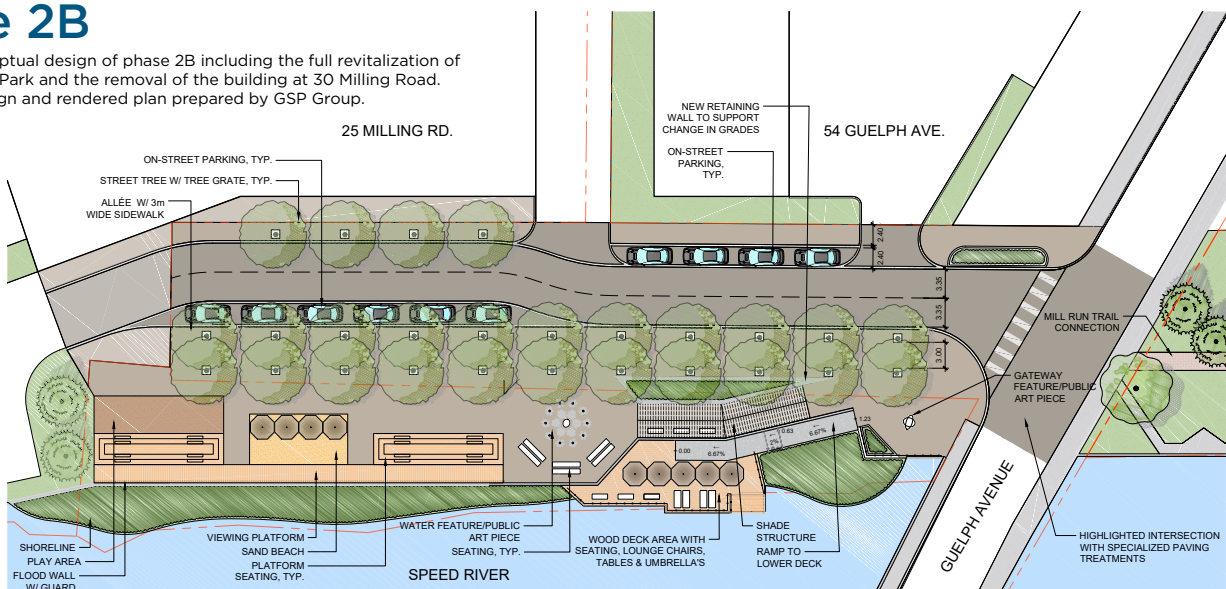
Phase 2A

Figure 20: Conceptual design of phase 2 including the renewal of Jacobs Landing Park, the streetscape design from phase 1, and the existing building at 30 Milling Road. Conceptual design and rendered plan prepared by GSP Group.



Phase 2B

Figure 21: Conceptual design of phase 2B including the full revitalization of Jacobs Landing Park and the removal of the building at 30 Milling Road. Conceptual design and rendered plan prepared by GSP Group.



Future Recommendations

Mill Run Trail Connection

During the consultation process with adjacent landowners, it was discussed whether the Mill Run Trail could be extended through 25 Milling Road as part of the study area. At the time of preparing the Guidelines, the landowners would like to maintain their property and continue serving tenants and at present there is not a desire to extend the trail through 25 Milling Road. It is recommended that the City look into property acquisition in the future, to potentially extend Mill Run Trail through 25 Milling Road.

Pedestrian Crossing

Further review would need to be conducted in order to determine if a controlled crossing (PXO) would be warranted at the intersection of Milling Road & Guelph Avenue. Pedestrian volumes would need to be studied to determine the type of control warranted for this crossing. A pedestrian refuge island and other measures should be considered, and truck turning movements at the intersection need to be factored into the outcome.

Vehicular Access & Parking

Considerations should also be made to extend beyond the limits of the site and include opportunities for regeneration and greater connectivity with Hespeler Village. The potential for property acquisition presents opportunities to adjust vehicular circulation through the site, while providing multi-modal transportation for private and public benefit. For example, private property between the tracks and buildings at both 25 Milling Road and 54 Guelph Avenue appears to be utilized for storage of equipment and light vehicular use. Through property acquisition there is an opportunity to reroute service vehicles and parking along the north side of the buildings to free up Milling Road for pedestrian use. Refer to Appendix A: Rear Building Access Concept.

Appendices

Appendix A

Conceptual Designs

Concept 1 - Alternate Streetscape Design

Traffic Circulation

The current layout of Milling Road prioritizes vehicular traffic and on-street parking. Tenants and employees of 25 Milling Road access the property at the terminus of Milling Road. Tenants and customers of 54 Guelph Avenue access the parking lot from Milling Road. On-street parking does not follow the latest standard, as vehicles currently park bumper-to-bumper on both sides of the road. The current roadway design, in combination with the parking configuration makes it challenging for the large delivery vehicles that enter both private properties on a weekly basis.

The alternate layout of Milling Road aims to improve the current configuration by consolidating on-street parking to the north. This layout is simple and requires less space than the preferred concept, and impedes less into Jacob's Landing. Vehicular traffic may prefer this configuration, however considerations should be made to prioritize safety, connectivity and accessibility for pedestrian circulation.

Pedestrian Circulation

Pedestrian circulation is currently limited to the north side of Milling Road, and through Jacob's Landing. With no sidewalk along the south side of Milling Road, the connection between business to the north and parkland to the south is lacking.

Stakeholders and survey respondents voiced interest in (1) prioritizing the pedestrian experience, and/or (2) transitioning Milling Road to pedestrian-only. The design of Milling Road should seriously consider the impacts of a pedestrian-only road, because the businesses of 25 Milling Road and 54 Guelph Avenue currently rely on vehicular access for parking, deliveries and the pickup of goods at 3 locations along Milling Road.

The alternative layout of Milling Road can function as a curbed or curbless road, however a curbless layout reinforces the pedestrian experience by creating a seamless north/south access. The paving treatments should reinforce the seamless experience by selecting patterns colours and tones that reinforce the seamless transition.

A 2.1m wide sidewalk on both sides of Milling Road provide pedestrians with options to walk the entire length of Milling Road. Considerations should be made to allow future phases of work to reinforce the Trail connection.

Planting Opportunities

Proposed plant material should respect the heritage of Milling Road while balancing maintenance and drought tolerance. Planting within LID systems should be specifically suited to deal with the unique conditions. Mature tree cover should be considered a priority, and reasonable measures should be taken to expedite this process. This may include; preserving all existing trees where possible, specifying large caliper trees, amending existing site soils, specifying quality planting soil that meets the various moisture conditions and pH requirements, limiting exposure to pollutants such as salt.

Proposed street trees should be both unified in appearance and diverse in plant genus. The trees should share a similar habit (ex.. vase-shaped) to maintain a balanced canopy and symmetry along Milling Road. Providing a variety of tree genus will provides resilience to account for unknowns, such as: salt exposure, pest pressures, ice damage, etc.

The alternate layout provides continuous street tree planting along both sides of Milling Road. The road is framed by street trees to provide symmetry along the length of Milling Road to unify the subject lands.

Additional consultation with 25 Milling Road should be conducted to integrate a cohesive planting design along the building outside the Milling Road right-of-way.

Low Impact Development

Active and passive stormwater measures should be considered to slow the infiltration of runoff during storm events. Proximity to the Speed River warrants careful consideration, and LID practices should be coordinated with the Grand River Conservation Authority for input and approval.

Street tree planting in the alternate layout is

raised to provide protection from pollutants while offering opportunities for stormwater management within the planting area. Best practices should be considered to develop a passive stormwater management strategy.

At the northwest corner of the Milling Road & Guelph Avenue intersection there is a large planter that provides an opportunity for additional stormwater infiltration, in coordination with the roadway grading.

Accessibility

Milling Road is relatively flat, and all business doorways along the north side provide unimpeded access. The grade at Guelph Avenue is a few metres higher than 30 Milling and 25 Milling, and this grade change presents a few challenges with accessibility into Jacob's Landing. To reinforce the narrative of Milling Road as a pedestrian environment, particular care should be given to address these grade changes.

Retaining walls will likely be required as interim and final conditions along the interface between Milling Road and Jacob's Landing. Considerations should be made to reduce the grade change and allow future phases of work to provide a seamless transition for pedestrians.

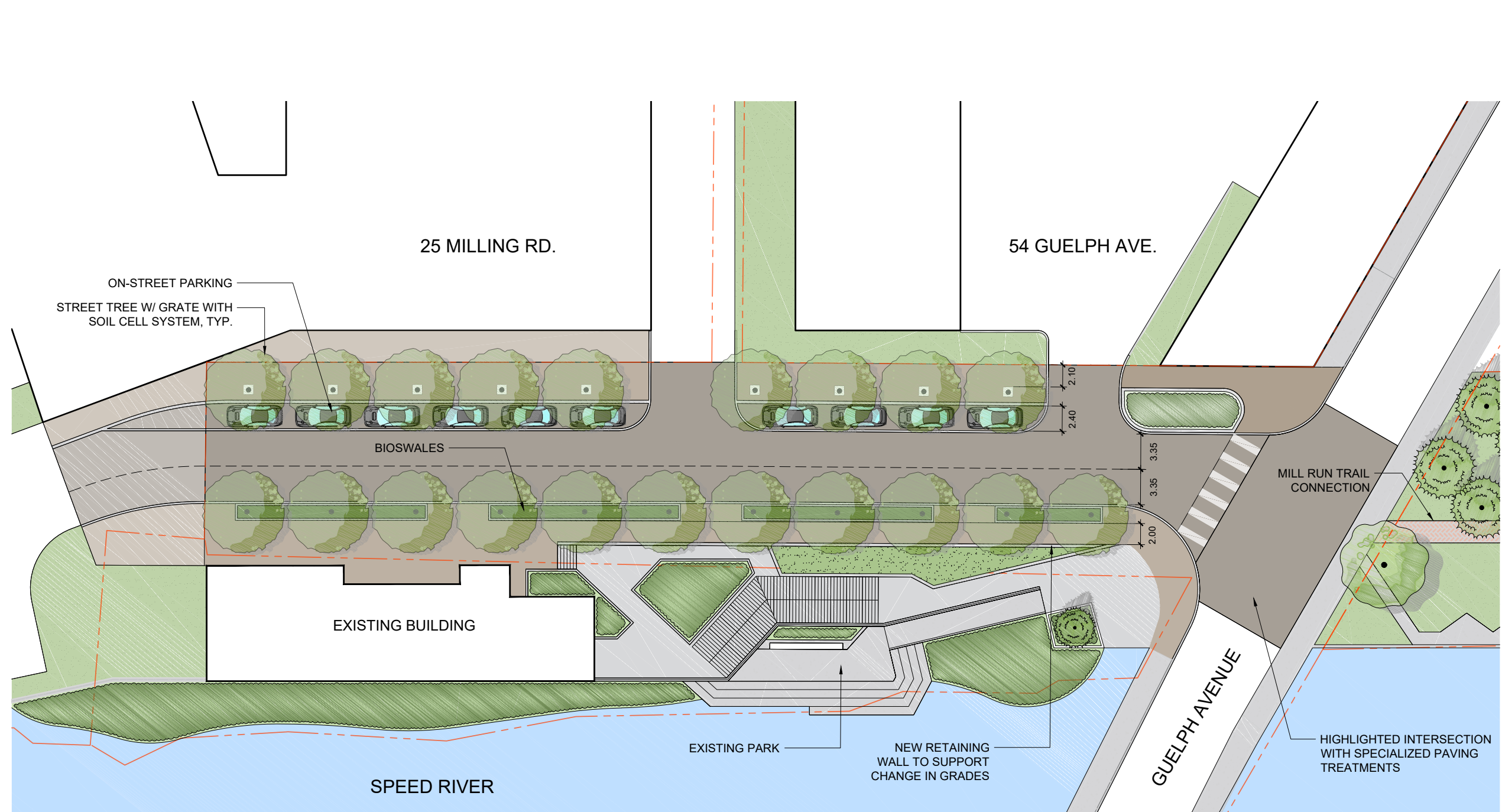
Materiality

Decorative paving treatments across the sidewalk, boulevard and roadway should be coordinated to suit the design theme and historic character of Milling Road. The paving treatments should contribute to the pedestrian experience and be part of a cohesive materials board.

Additional consultation with 25 Milling Road should be conducted to integrate a cohesive paving design along the building outside the Milling Road right-of-way.

Furnishing selection, hardscape treatments and planting style should have a strong relationship to the local context and history. The Hespeler Village SUDG provide thorough recommendations on materiality and style. Refer to the Guidelines for additional information.

Alternate Streetscape Design



LEGEND

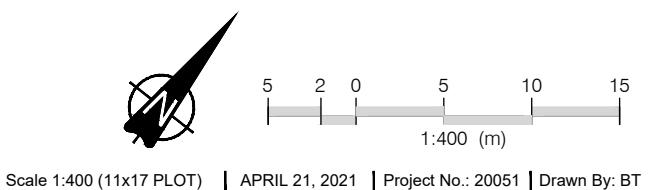
- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN

SUMMARY

- STREETSCAPE:**
- CURBED ROADWAY - MOUNTABLE
 - ON STREET PARKING ON NORTH SIDE - TOTAL 9 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - BIOSWALES WITH TREE PLANTINGS ON SOUTH SIDE TO CAPTURE STORMWATER RUN OFF
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK**
- MODIFICATIONS TO STREET FRONTAGE TO ACCOMMODATE NEW STREETSCAPE DESIGN

MILLING ROAD STREETSCAPE DESIGN CONCEPT 1 - PHASE 1: STREETSCAPE ONLY

Milling Road Streetscape & Urban Design Guidelines



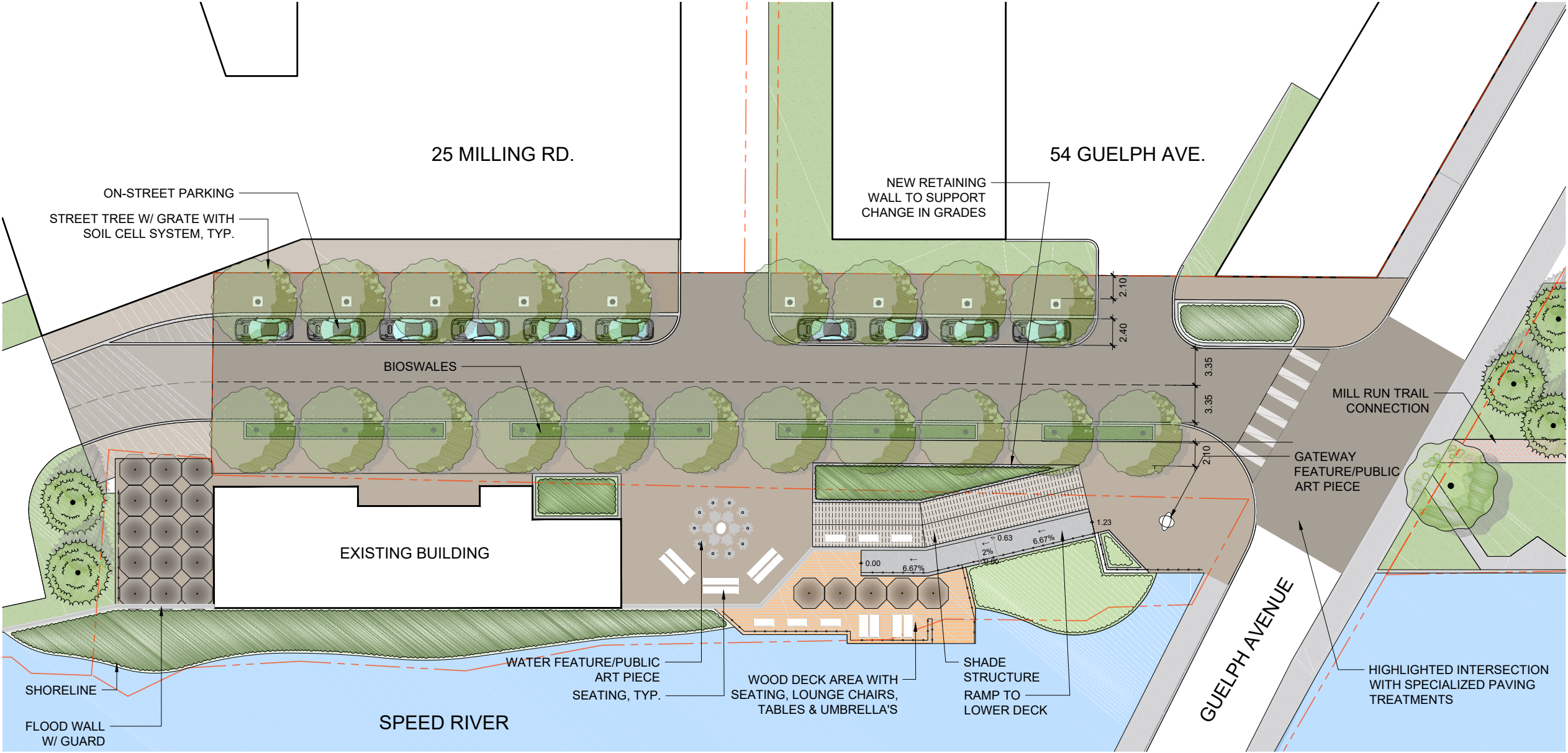
Alternate Streetscape Design

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN
- WOOD DECKING

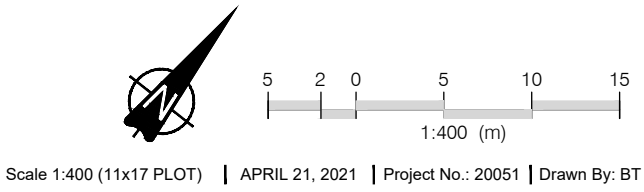
SUMMARY

- STREETSCAPE:**
- CURBED ROADWAY - MOUNTABLE
 - ON STREET PARKING ON NORTH SIDE - TOTAL 9 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - BIOSWALES WITH TREE PLANTINGS ON SOUTH SIDE TO CAPTURE STORMWATER RUN OFF
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK**
- WATER FEATURE WITH CUSTOM SEATING
 - SHADE STRUCTURE
 - WOOD DECK VIEWING AREA AT LOWER LEVEL WITH ACCESS TO WATER - ,MAINTAINS FOOTPRINT OF EXISTING AREA
 - STRENGTHEN ENTRANCE TO PARK WITH GATEWAY FEATURE/PUBLIC ART PIECE - OPPORTUNITY FOR COMMUNITY INVOLVEMENT

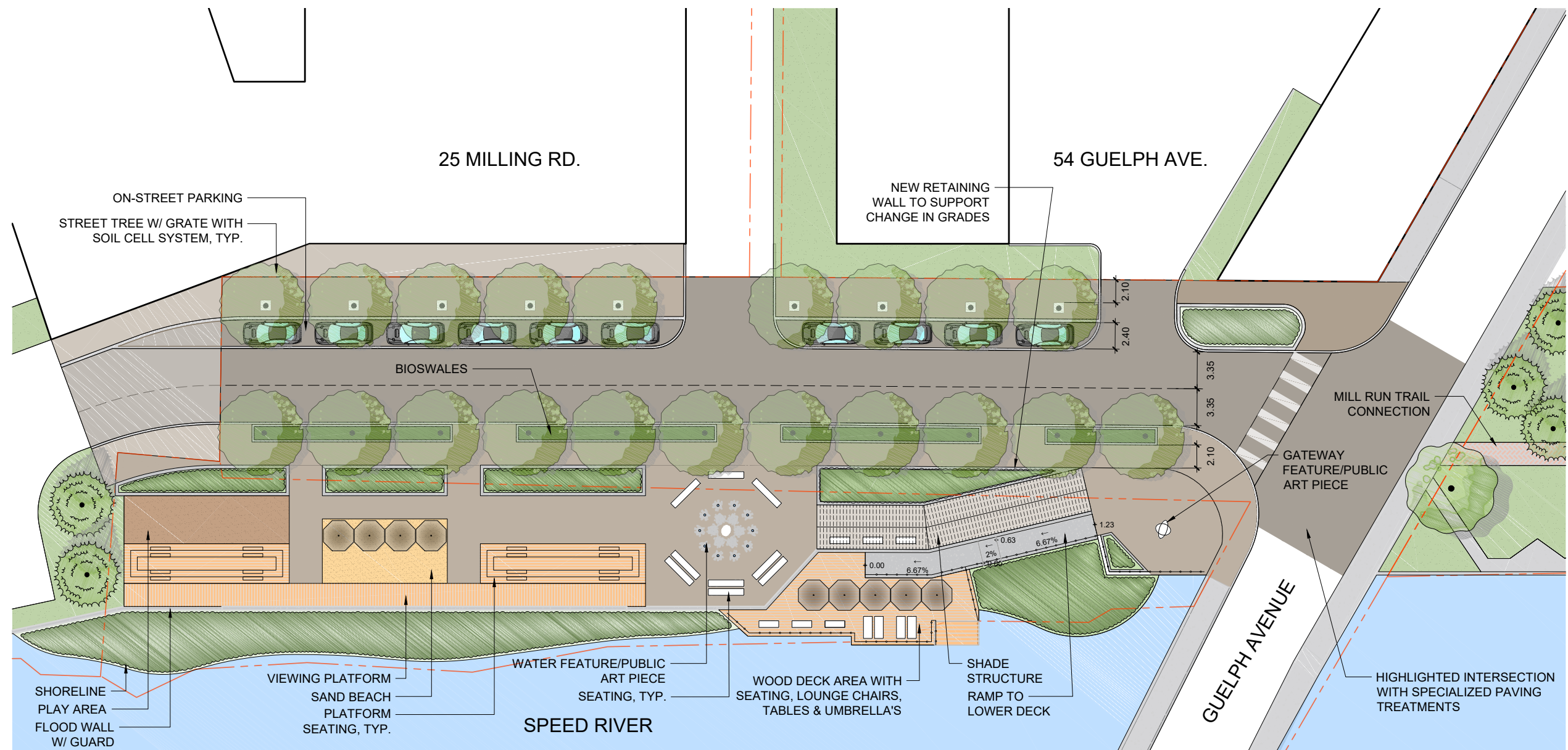


MILLING ROAD STREETSCAPE DESIGN CONCEPT 1 - PHASE 2A: EX. BUILDING & PARK RENOVATION

Milling Road Streetscape & Urban Design Guidelines



Alternate Streetscape Design



LEGEND

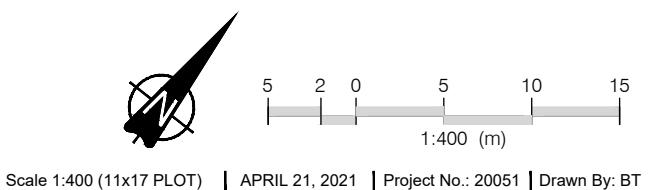
- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE
PAVING -
VEHICULAR
- DECORATIVE
PAVING -
PEDESTRIAN
- WOOD DECKING

SUMMARY

- STREETSCAPE:**
- CURBED ROADWAY - MOUNTABLE
 - ON STREET PARKING ON NORTH SIDE - TOTAL 9 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - BIOSWALES WITH TREE PLANTINGS ON SOUTH SIDE TO CAPTURE STORMWATER RUN OFF
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK**
- FULL BUILD OUT OF PARK IN ADDITION TO CONCEPT 1A
 - VIEW AREA ALONG RIVERS EDGE WITH PLATFORM SEATING
 - PLAY AREA
 - BEACH FOR LOUNGING
 - OPEN SPACE FOR EVENTS AND FOOD TRUCKS

MILLING ROAD STREETSCAPE DESIGN CONCEPT 1 - PHASE 2B: FULL PARK DEVELOPMENT

Milling Road Streetscape & Urban Design Guidelines



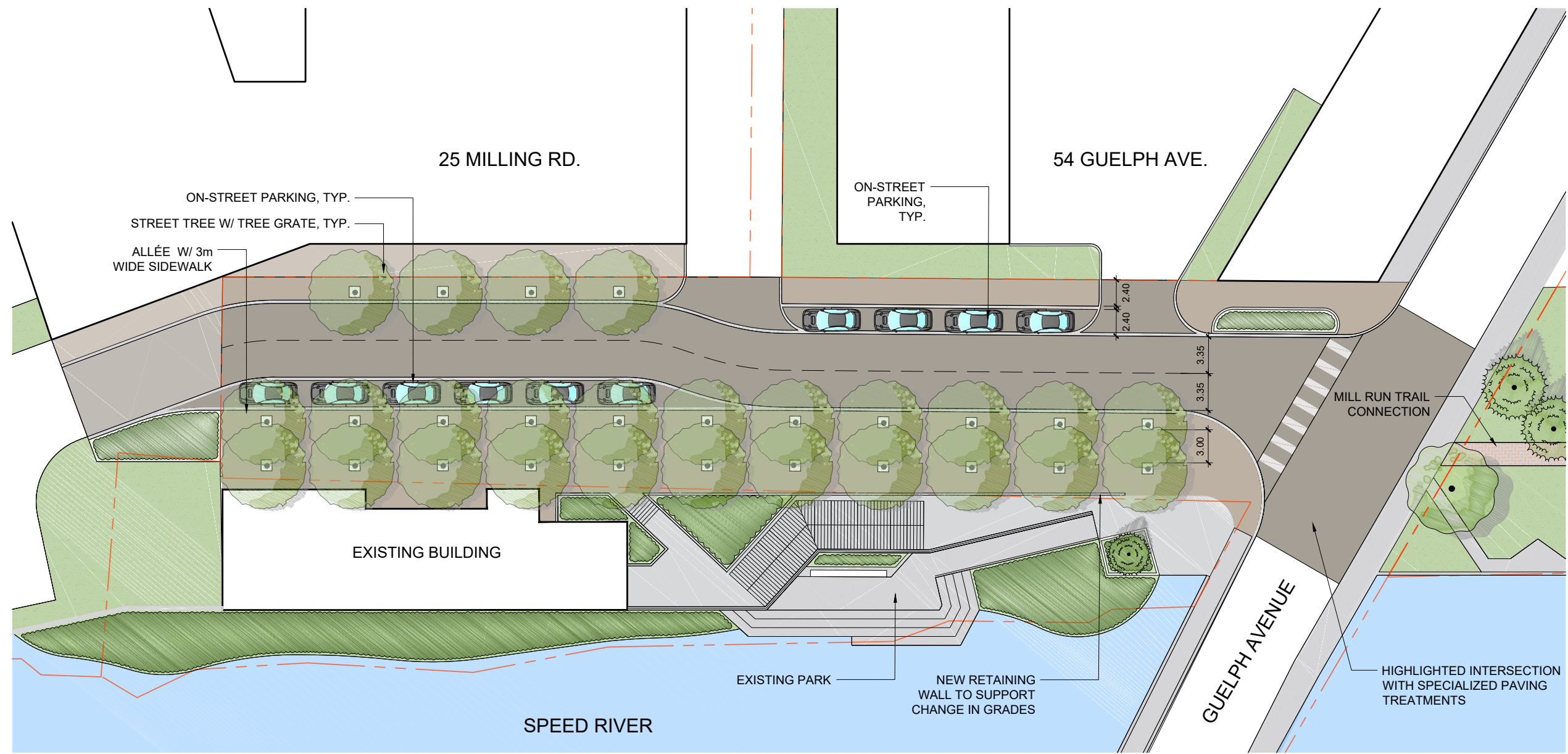
Preferred Streetscape Design

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN

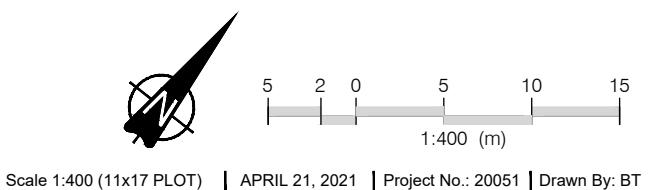
SUMMARY

- STREETSCAPE:
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLÉE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK
- MODIFICATIONS TO STREET FRONTAGE TO ACCOMMODATE NEW STREETSCAPE DESIGN



MILLING ROAD STREETSCAPE DESIGN CONCEPT 2 - PHASE 1: STREETSCAPE

Milling Road Streetscape & Urban Design Guidelines



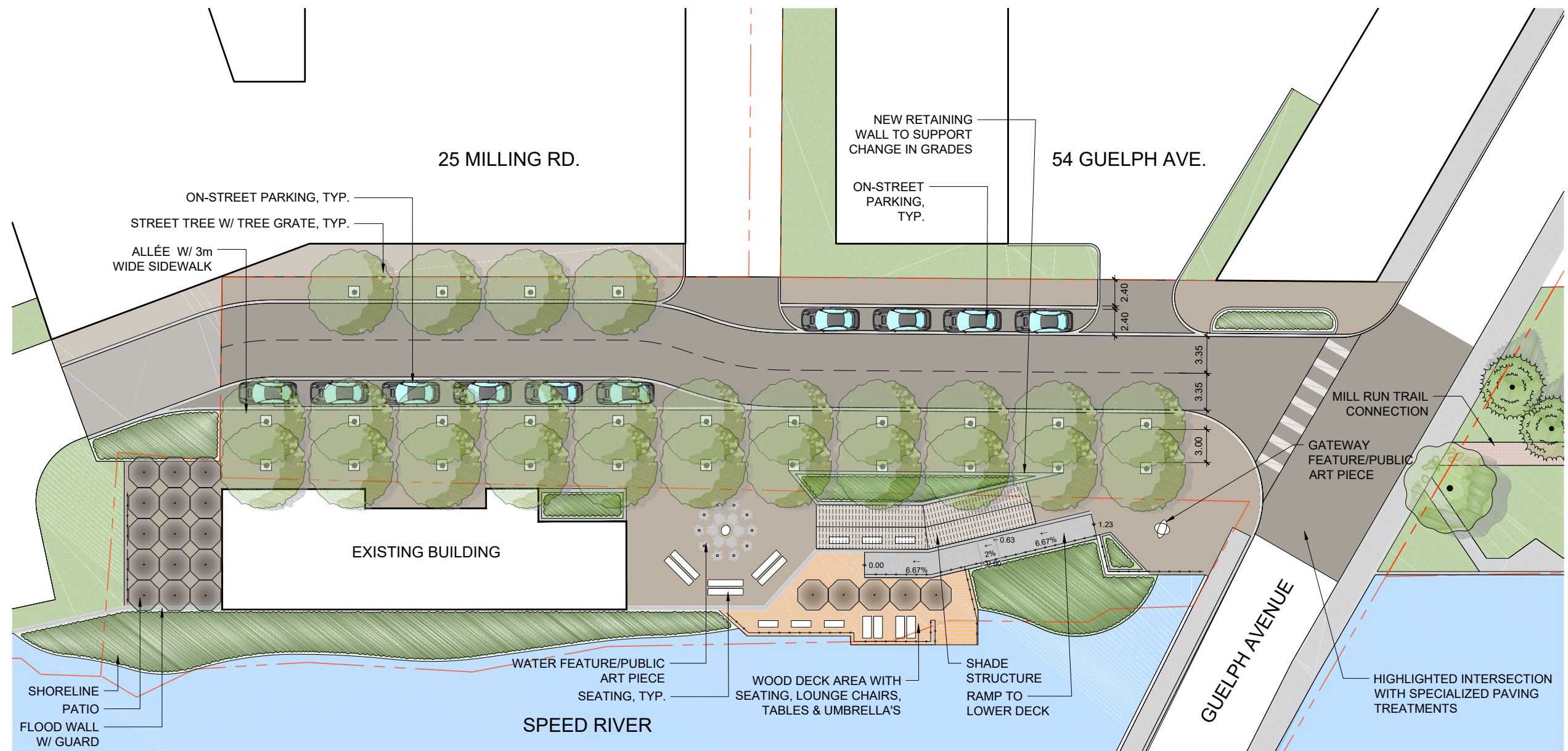
Preferred Streetscape Design

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN
- WOOD DECKING

SUMMARY

- STREETSCAPE:**
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLEE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK**
- WATER FEATURE WITH CUSTOM SEATING
 - SHADE STRUCTURE
 - WOOD DECK VIEWING AREA AT LOWER LEVEL WITH ACCESS TO WATER - MAINTAINS FOOTPRINT OF EXISTING AREA
 - STRENGTHEN ENTRANCE TO PARK WITH GATEWAY FEATURE/PUBLIC ART PIECE - OPPORTUNITY FOR COMMUNITY INVOLVEMENT



MILLING ROAD STREETSCAPE DESIGN CONCEPT 2 - PHASE 2A: EX. BUILDING & PARK RENOVATION

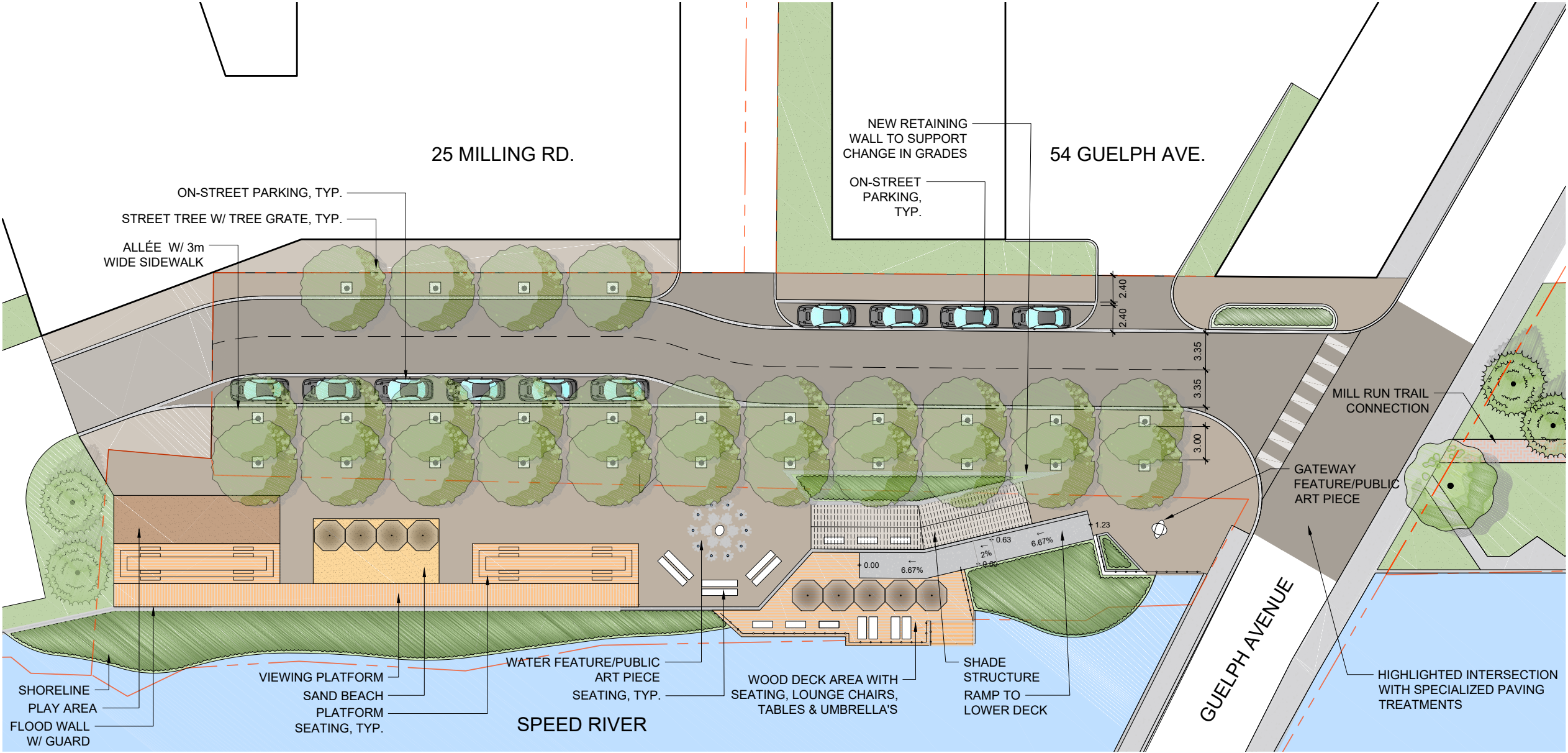
Preferred Streetscape Design

LEGEND

- PROPERTY LINE
- EXISTING TREES
- PROPOSED TREES
- PLANTING BEDS
- SOD
- CONCRETE CURBS
- DECORATIVE PAVING - VEHICULAR
- DECORATIVE PAVING - PEDESTRIAN
- WOOD DECKING

SUMMARY

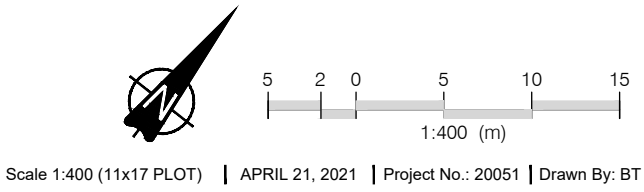
- STREETSCAPE:**
- CURBLESS ROADWAY - WOONERF STREET
 - ON STREET PARKING STAGGERED ON NORTH AND SOUTH SIDE - TOTAL 10 PARKING SPACES
 - STREET TREES IN PLANTING PITS WITH SOIL CELLS ON NORTH SIDE OF STREET
 - REDUCED LANE WIDTHS
 - TREE IN PITS CREATING A TREED ALLEE
 - WIDEN SIDEWALKS
- JACOB'S LANDING - PARK**
- FULL BUILD OUT OF PARK IN ADDITION TO CONCEPT 1A
 - VIEW AREA ALONG RIVERS EDGE WITH PLATFORM SEATING
 - PLAY AREA
 - BEACH FOR LOUNGING
 - OPEN SPACE FOR EVENTS AND FOOD TRUCKS



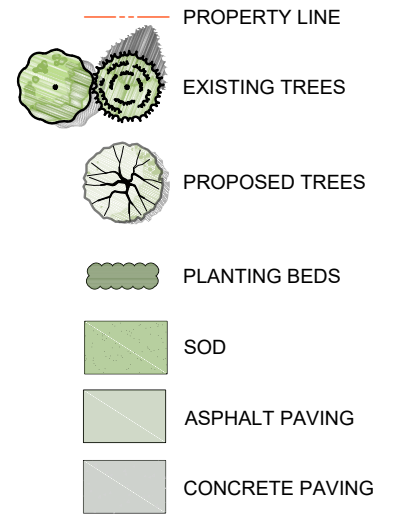
MILLING ROAD STREETSCAPE DESIGN

CONCEPT 2 - PHASE 2B: FULL PARK DEVELOPMENT

Milling Road Streetscape & Urban Design Guidelines

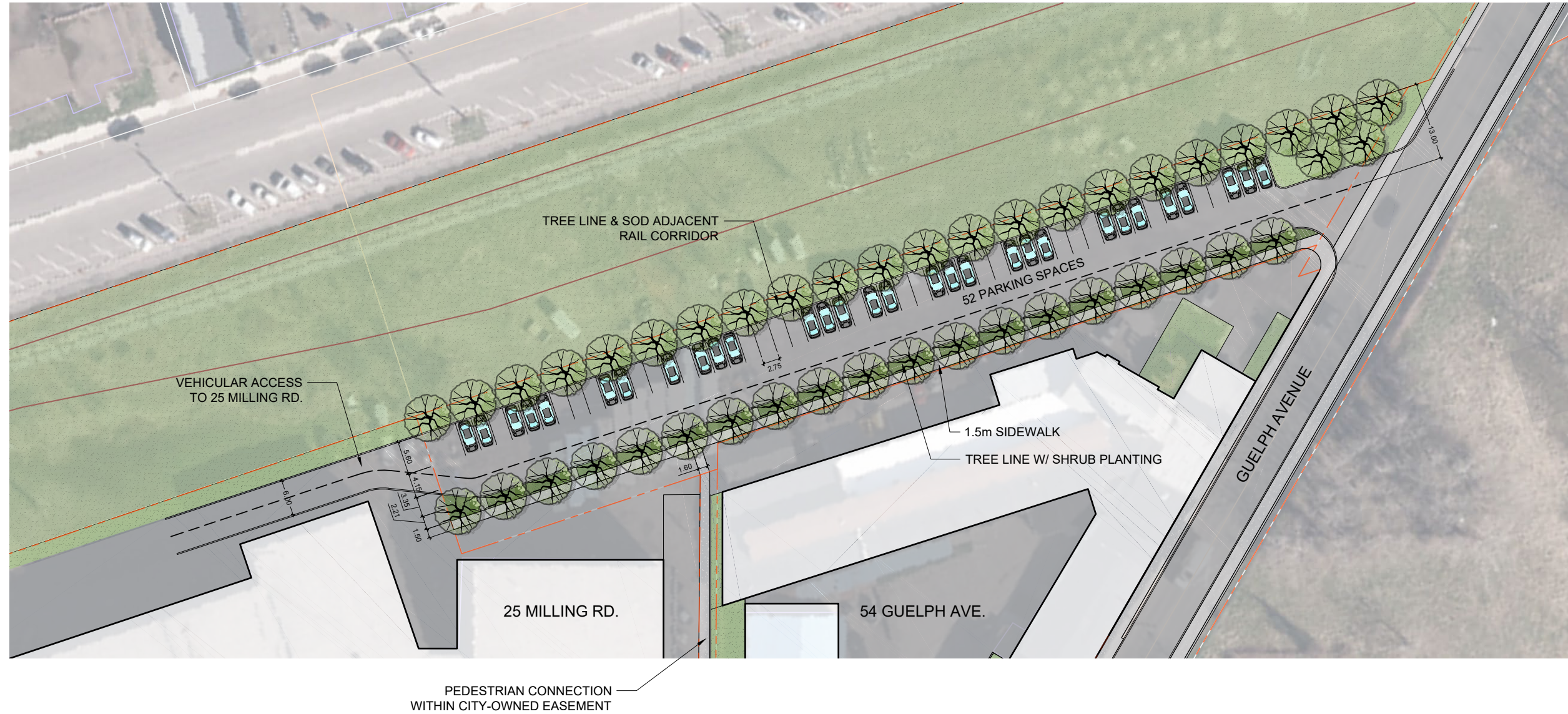


LEGEND



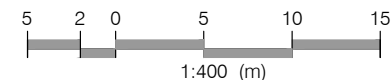
SUMMARY

- A parking lot along the rail corridor could be established through severance and property acquisition of 54 Guelph Avenue. This would provide benefit to local business and visitors of Jacob's Landing.
- Jacob's Landing and business along Milling Road are accessible via a pedestrian connection within the City-owned easement.



MILLING ROAD STREETScape DESIGN REAR BUILDING ACCESS

Milling Road Streetscape & Urban Design Guidelines



Scale 1:400 (11x17 PLOT) | APRIL 21, 2021 | Project No.: 20051 | Drawn By: BT



Appendix B

Cost Estimates

Itemized Cost Estimate Form LANDSCAPE DEVELOPMENT

MILLING ROAD REDEVELOPMENT - COSTING

Date: May 17, 2021

PRELIMINARY COSTING - CONCEPT 1 PHASE 1

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount
Part 1 Site Preparation and Temporary Facilities						
1.1		Demolition - Removals	1.0	LS	\$ 70,000.00	\$ 70,000.00
1.2		Silt Control, Construction Fencing, Tree Protection Fencing and Signage	1.0	LS	\$ 5,000.00	\$ 5,000.00
1.3		Mobilization/Demobilization - Field Office	1.0	m ²	\$ 25,000.00	\$ 25,000.00
1.4		Pre-Condition Survey	1.0	LS	\$ 3,000.00	\$ 3,000.00
SUBTOTAL - SITE PREPARATION						\$ 103,000.00
Part 2 Paving						
2.1		Concrete - Vehicular - Decorative	1300.0	m ²	\$ 250.00	\$ 325,000.00
2.1 B		Alternate: Unit Paving - Vehicular	1300.0	m ²	\$ 225.00	\$ 292,500.00
2.1 C		Alternate: Asphalt Paving - Vehicular	1300.0	m ²	\$ 90.00	\$ 117,000.00
2.2		Concrete - Pedestrian - Decorative	900.0	m ²	\$ 200.00	\$ 180,000.00
2.2. B		Alternate: Unit Paving - Pedestrian	900.0	LS	\$ 175.00	\$ 157,500.00
2.2. C		Alternate: Permeable Pavers - Pedestrian	900.0	m ²	\$ 225.00	\$ 202,500.00
2.3		Road Curbing - Mountable	280.0	lin m	\$ 126.00	\$ 35,280.00
SUBTOTAL - PAVING						\$ 540,280.00
Part 3 Planting						
3.1		Trees - 125mm Caliper	20.0	ea	\$ 1,500.00	\$ 30,000.00
3.2		Shrubs/Perennials	120.0	m ²	\$ 50.00	\$ 6,000.00
3.3		Soil Cells - Including planting soil (For 20 Trees)	20.0	ea	\$ 15,000.00	\$ 300,000.00
3.4		Planting Soil - Planting bed	100.0	m ³	\$ 65.00	\$ 6,500.00
3.5		Mulch (75mm Cover)	140.0	m ²	\$ 12.00	\$ 1,680.00
SUBTOTAL - PLANTING						\$ 344,180.00
Part 4 Site Elements						
4.1		Low Concrete Seat Walls (450mm Ht.)	150.0	lin m.	\$ 750.00	\$ 112,500.00
4.2		Decorative Lighting (Bollards, landscape, etc.)	1.0	LS	\$ 35,000.00	\$ 35,000.00
4.3		Electrical (including street lighting)	1.0	LS	\$ 200,000.00	\$ 200,000.00
SUBTOTAL - SITE ELEMENTS						\$ 347,500.00
Part 5 Site Furnishings						
5.1		Benches	10.0	ea	\$ 4,000.00	\$ 40,000.00
5.2		Waste/Recycle Receptacles (Combo Unit)	2.0	ea	\$ 2,500.00	\$ 5,000.00
5.3		Bike Racks	10.0	ea	\$ 2,000.00	\$ 20,000.00
5.4		Tree Grates	9.0	ea	\$ 5,000.00	\$ 45,000.00
SUBTOTAL - SITE FURNISHINGS						\$ 110,000.00

LANDSCAPING SUMMARY

SUBTOTAL - SITE PREPARATION	\$ 103,000.00
SUBTOTAL - PAVING	\$ 540,280.00
SUBTOTAL - PLANTING	\$ 344,180.00
SUBTOTAL - SITE ELEMENTS	\$ 347,500.00
SUBTOTAL - SITE FURNISHINGS	\$ 110,000.00
SUBTOTAL - SECTIONS 1 TO 5	\$ 1,444,960.00
CONTINGENCY - 10%	\$ 144,496.00
TAXES (HST) - 13%	\$ 206,629.28
ESTIMATED TOTAL	\$ 1,796,085.28

Not included in total

Not included in total

Not included in total

Not included in total

Itemized Cost Estimate Form LANDSCAPE DEVELOPMENT

MILLING ROAD REDEVELOPMENT - COSTING

Date: May 17, 2021

PRELIMINARY COSTING - CONCEPT 1 PHASE 2A

LANDSCAPING FROM PHASE 1 **\$ 1,444,960.00**

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount
Part 6 New Park with Existing Building						
6.1		Demolition - Removals	1.0	LS	\$ 40,000.00	\$ 40,000.00
6.2		Paving - Unit Paving Pedestrian	550.0	m ²	\$ 200.00	\$ 110,000.00
6.3		Wood Decking Area - River	140.0	m ²	\$ 400.00	\$ 56,000.00
6.4		Planting	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.5		Custom Shade Structure	1.0	LS	\$ 60,000.00	\$ 60,000.00
6.6		Site Furnishings	1.0	LS	\$ 55,000.00	\$ 55,000.00
6.7		Gateway Feature - Public Art Piece	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.8		Water Feature - Public Art Piece	1.0	LS	\$ 100,000.00	\$ 100,000.00
SUBTOTAL - NEW PARK						\$ 441,000.00

LANDSCAPING SUMMARY

LANDSCAPING FROM PHASE 1	\$ 1,444,960.00
SUBTOTAL - NEW PARK	\$ 441,000.00
SUBTOTAL - SECTIONS 1 TO 5	\$ 1,885,960.00
CONTINGENCY - 10%	\$ 188,596.00
TAXES (HST) - 13%	\$ 269,692.28
ESTIMATED TOTAL WITH PHASE 2A	\$ 2,344,248.28

PRELIMINARY COSTING - CONCEPT 1 PHASE 2B

LANDSCAPING FROM PHASE 1 **\$ 1,444,960.00**

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount
Part 6 New Park - Full Site						
6.1		Demolition - Removals	1.0	LS	\$ 150,000.00	\$ 150,000.00
6.2		Paving - Unit Paving Pedestrian	550.0	m ²	\$ 200.00	\$ 110,000.00
6.3		Wood Decking Area - River	240.0	m ²	\$ 400.00	\$ 96,000.00
6.4		Play Surfacing and Equipment	1.0	LS	\$ 30,000.00	\$ 30,000.00
6.5		Beach Area with Umbrellas	1.0	LS	\$ 15,000.00	\$ 15,000.00
6.6		Planting	1.0	LS	\$ 20,000.00	\$ 20,000.00
6.7		Custom Platform Seating	1.0	LS	\$ 100,000.00	\$ 100,000.00
6.8		Custom Shade Structure	1.0	LS	\$ 60,000.00	\$ 60,000.00
6.9		Site Furnishings	1.0	LS	\$ 75,000.00	\$ 75,000.00
6.10		Gateway Feature - Public Art Piece	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.11		Water Feature - Public Art Piece	1.0	LS	\$ 100,000.00	\$ 100,000.00
SUBTOTAL - NEW PARK						\$ 766,000.00

LANDSCAPING SUMMARY

LANDSCAPING FROM PHASE 1	\$ 1,444,960.00
SUBTOTAL - NEW PARK	\$ 766,000.00
SUBTOTAL - SECTIONS 1 TO 5	\$ 2,210,960.00
CONTINGENCY - 10%	\$ 221,096.00
TAXES (HST) - 13%	\$ 316,167.28
ESTIMATED TOTAL WITH PHASE 2B	\$ 2,748,223.28

Itemized Cost Estimate Form LANDSCAPE DEVELOPMENT

MILLING ROAD REDEVELOPMENT - COSTING

Date: May 13, 2021

PRELIMINARY COSTING - CONCEPT 2 PHASE 1

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount	
Part 1 Site Preparation and Temporary Facilities							
1.1		Demolition - Removals	1.0	LS	\$ 70,000.00	\$ 70,000.00	
1.2		Silt Control, Construction Fencing, Tree Protection Fencing and Signage	1.0	LS	\$ 5,000.00	\$ 5,000.00	
1.3		Mobilization/Demobilization - Field Office	1.0	m ²	\$ 25,000.00	\$ 25,000.00	
1.4		Pre-Condition Survey	1.0	LS	\$ 3,000.00	\$ 3,000.00	
SUBTOTAL - SITE PREPARATION						\$ 103,000.00	
Part 2 Paving							
2.1		Concrete - Vehicular - Decorative	1420.0	m ²	\$ 250.00	\$ 355,000.00	
2.1 B		Alternate: Unit Paving - Vehicular	1420.0	m ²	\$ 225.00	\$ 319,500.00	Not included in total
2.1 C		Alternate: Asphalt Paving - Vehicular	1420.0	m ²	\$ 90.00	\$ 127,800.00	Not included in total
2.2		Concrete - Pedestrian - Decorative	1000.0	m ²	\$ 200.00	\$ 200,000.00	
2.2. B		Alternate: Unit Paving - Pedestrian	1000.0	LS	\$ 175.00	\$ 175,000.00	Not included in total
2.2. C		Alternate: Permeable Pavers - Pedestrian	1000.0	m ²	\$ 225.00	\$ 225,000.00	Not included in total
SUBTOTAL - PAVING						\$ 555,000.00	
Part 3 Planting							
3.1		Trees - 125mm Caliper	26.0	ea	\$ 1,500.00	\$ 39,000.00	
3.2		Shrubs/Perennials	25.0	m ²	\$ 50.00	\$ 1,250.00	
3.3		Soil Cells - Including planting soil (For 20 Trees)	26.0	ea	\$ 15,000.00	\$ 390,000.00	
3.4		Planting Soil - Planting bed	20.0	m ³	\$ 65.00	\$ 1,300.00	
3.5		Mulch (75mm Cover)	55.0	m ²	\$ 12.00	\$ 660.00	
SUBTOTAL - PLANTING						\$ 432,210.00	
Part 4 Site Elements							
4.1		Decorative Lighting (Bollards, landscape, etc.)	1.0	LS	\$ 35,000.00	\$ 35,000.00	
4.2		Electrical (including street lighting)	1.0	LS	\$ 200,000.00	\$ 200,000.00	
SUBTOTAL - SITE ELEMENTS						\$ 235,000.00	
Part 5 Site Furnishings							
5.1		Benches	10.0	ea	\$ 4,000.00	\$ 40,000.00	
5.2		Waste/Recycle Receptacles (Combo Unit)	2.0	ea	\$ 2,500.00	\$ 5,000.00	
5.3		Bike Racks	10.0	ea	\$ 2,000.00	\$ 20,000.00	
5.4		Tree Grates	26.0	ea	\$ 5,000.00	\$ 130,000.00	
SUBTOTAL - SITE FURNISHINGS						\$ 195,000.00	
LANDSCAPING SUMMARY							
SUBTOTAL - SITE PREPARATION						\$ 103,000.00	
SUBTOTAL - PAVING						\$ 555,000.00	
SUBTOTAL - PLANTING						\$ 432,210.00	
SUBTOTAL - SITE ELEMENTS						\$ 235,000.00	
SUBTOTAL - SITE FURNISHINGS						\$ 195,000.00	
SUBTOTAL - SECTIONS 1 TO 5						\$ 1,520,210.00	
CONTINGENCY - 10%						\$ 152,021.00	
TAXES (HST) - 13%						\$ 217,390.03	
ESTIMATED TOTAL						\$ 1,889,621.03	

PRELIMINARY COSTING - CONCEPT 2 PHASE 2A

Itemized Cost Estimate Form LANDSCAPE DEVELOPMENT

MILLING ROAD REDEVELOPMENT - COSTING

Date: May 13, 2021

PRELIMINARY COSTING - CONCEPT 2 PHASE 2A

LANDSCAPING FROM PHASE 1 **\$ 1,520,210.00**

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount
Part 6		New Park with Existing Building				
6.1		Demolition - Removals	1.0	LS	\$ 40,000.00	\$ 40,000.00
6.2		Paving - Unit Paving Pedestrian	550.0	m ²	\$ 200.00	\$ 110,000.00
6.3		Wood Decking Area - River	140.0	m ²	\$ 400.00	\$ 56,000.00
6.4		Planting	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.5		Custom Shade Structure	1.0	LS	\$ 60,000.00	\$ 60,000.00
6.6		Site Furnishings	1.0	LS	\$ 55,000.00	\$ 55,000.00
6.7		Gateway Feature - Public Art Piece	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.8		Water Feature - Public Art Piece	1.0	LS	\$ 100,000.00	\$ 100,000.00
SUBTOTAL - NEW PARK						\$ 441,000.00

LANDSCAPING SUMMARY

LANDSCAPING FROM PHASE 1	\$ 1,520,210.00
SUBTOTAL - NEW PARK	\$ 441,000.00
SUBTOTAL - SECTIONS 1 TO 5	\$ 1,961,210.00
CONTINGENCY - 10%	\$ 196,121.00
TAXES (HST) - 13%	\$ 280,453.03
ESTIMATED TOTAL WITH PHASE 2A	\$ 2,437,784.03

PRELIMINARY COSTING - CONCEPT 2 PHASE 2B

LANDSCAPING FROM PHASE 1 **\$ 1,520,210.00**

LANDSCAPING

Item	Spec. No.	Description	Est'd Qty.	Unit	Unit Price	Amount
Part 6		New Park - Full Site				
6.1		Demolition - Removals	1.0	LS	\$ 150,000.00	\$ 150,000.00
6.2		Paving - Unit Paving Pedestrian	550.0	m ²	\$ 200.00	\$ 110,000.00
6.3		Wood Decking Area - River	240.0	m ²	\$ 400.00	\$ 96,000.00
6.4		Play Surfacing and Equipment	1.0	LS	\$ 30,000.00	\$ 30,000.00
6.5		Beach Area with Umbrellas	1.0	LS	\$ 15,000.00	\$ 15,000.00
6.6		Planting	1.0	LS	\$ 20,000.00	\$ 20,000.00
6.7		Custom Platform Seating	1.0	LS	\$ 100,000.00	\$ 100,000.00
6.8		Custom Shade Structure	1.0	LS	\$ 60,000.00	\$ 60,000.00
6.9		Site Furnishings	1.0	LS	\$ 75,000.00	\$ 75,000.00
6.10		Gateway Feature - Public Art Piece	1.0	LS	\$ 10,000.00	\$ 10,000.00
6.11		Water Feature - Public Art Piece	1.0	LS	\$ 100,000.00	\$ 100,000.00
SUBTOTAL - NEW PARK						\$ 766,000.00

LANDSCAPING SUMMARY

LANDSCAPING FROM PHASE 1	\$ 1,520,210.00
SUBTOTAL - NEW PARK	\$ 766,000.00
SUBTOTAL - SECTIONS 1 TO 5	\$ 2,286,210.00
CONTINGENCY - 10%	\$ 228,621.00
TAXES (HST) - 13%	\$ 326,928.03
ESTIMATED TOTAL WITH PHASE 2B	\$ 2,841,759.03

Appendix C

EngageCambridge First Survey

Milling Road Streetscape Plan, Urban Design Guidelines and Consultation

SURVEY RESPONSE REPORT

15 August 2018 - 25 April 2021

PROJECT NAME:

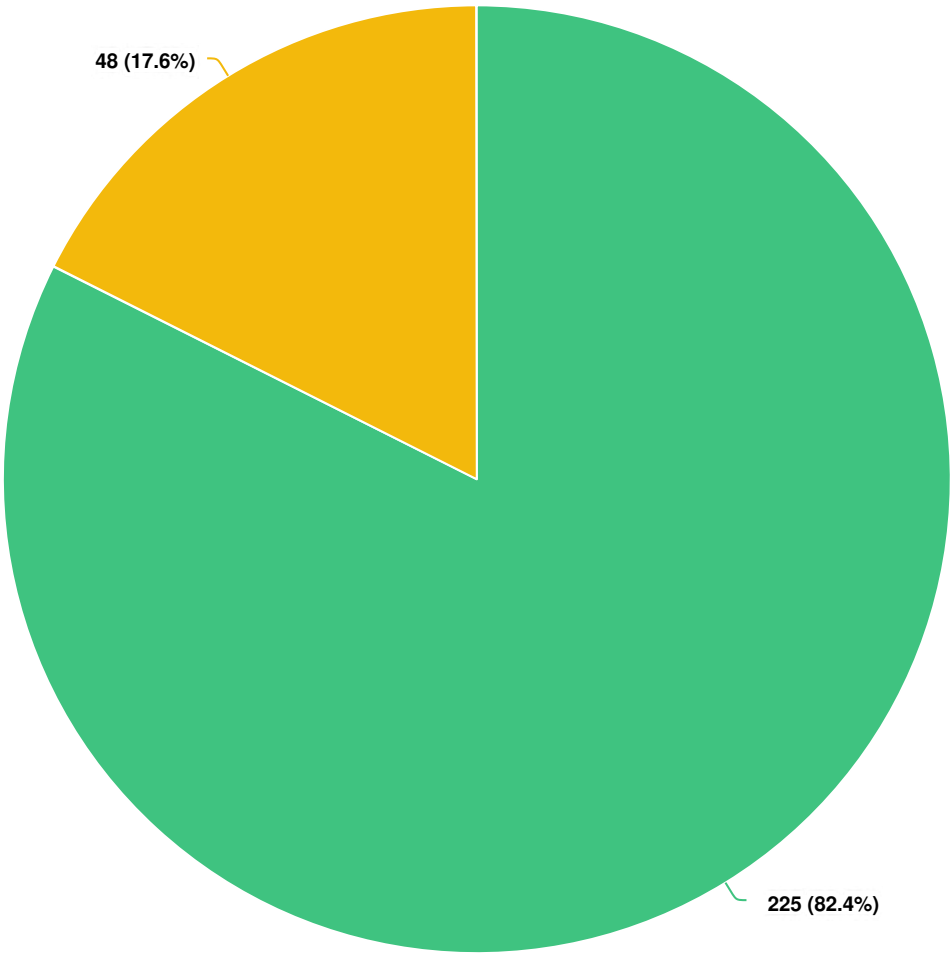
Milling Road Streetscape





SURVEY QUESTIONS


Q1 Do you currently visit Jacob's Landing on Milling Road?



Question options

Yes No

Q2 How do you use the space?


 Walking with family
3/12/2021 03:43 PM

Anonymous As a place to get away from traffic
3/17/2021 05:51 PM noice. Just to watch the river.

Anonymous I would usually cycle into Hespeler
3/18/2021 12:21 PM Village and visit Miling street for its intimate/human scale streetscape (building setbacks close to the narrow street with 3 storey red brick warehouses). I would sit on the landing abutting the river for the townscape looking south.

Anonymous walking through to observe the river.
3/20/2021 10:52 AM Sitting & watching the river

Anonymous Have office here and we come here
3/26/2021 08:30 AM to work daily

 Photography
3/31/2021 03:38 PM

Anonymous Go for walks
3/31/2021 05:59 PM

Anonymous Leisure
4/20/2021 10:43 AM

Anonymous I use the Mill Run Trail.
4/20/2021 10:45 AM

Anonymous Remote control speed boats with
4/20/2021 10:47 AM children, photo shoots, somewhere to walk too and then share an ice cream

Anonymous Walking and outdoor space
4/20/2021 10:51 AM

Anonymous walking
4/20/2021 10:54 AM

Anonymous Walking, photography
4/20/2021 11:01 AM

Anonymous Hiking and fishing
4/20/2021 11:02 AM

Anonymous Walking my dog and toddler
4/20/2021 11:06 AM

Anonymous Recreational
4/20/2021 11:06 AM

Anonymous Part of daily walks
4/20/2021 11:10 AM

Anonymous Nature walks through Mill Run Trail,
4/20/2021 11:10 AM picnics, family photos,

Anonymous Walk, photograph
4/20/2021 11:15 AM

Anonymous Gathering with friends
4/20/2021 11:25 AM

Anonymous Recreation, fishing.
4/20/2021 11:35 AM

Anonymous I occasionally walk the trail.
4/20/2021 11:35 AM

Anonymous We walk down from our house on
4/20/2021 11:39 AM Fisher Mills Road and enjoy walks along the river with our toddler.

Anonymous Dog walks
4/20/2021 11:43 AM

Anonymous Trail walking
4/20/2021 11:45 AM

Anonymous Walks and relaxing
4/20/2021 11:50 AM

Q2 How do you use the space?

Anonymous Taking kids and dog for a walk and
4/20/2021 11:51 AM play time

Anonymous Outdoor amenities. Enjoying the
4/20/2021 11:56 AM waterfall. Picnic space. Animal
watching. Brewery tour.

Anonymous Walking through to mill run trail
4/20/2021 12:04 PM

Anonymous Walking
4/20/2021 12:07 PM

Scenic walks
4/20/2021 12:07 PM

Anonymous Photo/relax
4/20/2021 12:14 PM

Anonymous I rollerskate in the area, and I like to
4/20/2021 12:15 PM sit and take tests here.

Anonymous Walking
4/20/2021 12:17 PM

Anonymous Leisurely visit
4/20/2021 12:22 PM

A place to rest when hiking.
4/20/2021 12:22 PM

Anonymous Frequently walk along the river trails.
4/20/2021 12:24 PM Sit and enjoy the river

Anonymous Enjoying the water. Part of my daily
4/20/2021 12:28 PM walk through Millpond trail

Anonymous Leisure.
4/20/2021 12:30 PM

Anonymous We use the trails and watch the
4/20/2021 12:31 PM wildlife with the kids

Walking and jogging. Looking at the
4/20/2021 12:32 PM river.

Anonymous Walking trails, fishing
4/20/2021 12:35 PM

Anonymous Leisure
4/20/2021 12:38 PM

Anonymous Family leisure like walks
4/20/2021 12:41 PM

Biking, hiking, picnics, family outings
4/20/2021 01:15 PM

Anonymous Access to Mill Run Trail
4/20/2021 12:43 PM

Anonymous For leisurely walks
4/20/2021 12:43 PM

Anonymous Sightseeing, walking, photography
4/20/2021 12:48 PM flying kites and releasing Chinese
lanterns.

Anonymous We use the trail system.
4/20/2021 12:51 PM

Anonymous Walking and parking
4/20/2021 12:58 PM

Anonymous Walking and hiking
4/20/2021 01:00 PM

Anonymous Meeting space, walking
4/20/2021 01:08 PM

Anonymous Hiking , picnics, photography,
4/20/2021 01:10 PM kayaking.

Anonymous walk
4/20/2021 01:10 PM

Q2 How do you use the space?

Anonymous Walks/relaxation
4/20/2021 01:15 PM

Anonymous Walking. Viewing river
4/20/2021 01:37 PM

Anonymous Walk
4/20/2021 01:15 PM

Anonymous Sitting and thinking
4/20/2021 01:41 PM

Anonymous Parking, kayak launching, sit and enjoy the view
4/20/2021 01:16 PM

Anonymous Family walks
4/20/2021 01:42 PM

Anonymous Walk
4/20/2021 01:16 PM

Anonymous Leisure
4/20/2021 01:44 PM

Anonymous Part of my dog walking hike - I walk this route often. It is beautiful.
4/20/2021 01:18 PM

Anonymous Recreational exercise.
4/20/2021 01:44 PM

Anonymous Photography and fishing
4/20/2021 01:18 PM

Anonymous Walking the trail, steps down to the waterfall for kids to get close to the water. Turtle watching.
4/20/2021 01:44 PM

Anonymous Walk the trails
4/20/2021 01:18 PM

Anonymous Walk and sit by the river
4/20/2021 01:50 PM

Anonymous Walks and views of the river
4/20/2021 01:19 PM

Anonymous Walking, and observing the river and mill pond birds
4/20/2021 01:57 PM

Anonymous I take pictures of my vehicle with the Hespeler Furniture Building as a backdrop.
4/20/2021 01:20 PM

Anonymous Walking sitting area
4/20/2021 02:07 PM

Anonymous Walking, enjoying nice weather by the water.
4/20/2021 01:21 PM

Anonymous Walking, resting.
4/20/2021 02:09 PM

Anonymous Walking my dogs sitting and just relaxing
4/20/2021 01:24 PM

Anonymous I frequently walk and bike ride the area
4/20/2021 02:23 PM

Anonymous enjoying quiet space
4/20/2021 01:24 PM

Anonymous Walking the dog
4/20/2021 02:29 PM

Anonymous Enjoying a walk and the river
4/20/2021 01:29 PM

Anonymous Walking in area
4/20/2021 02:31 PM

Anonymous Walking. Viewing river
4/20/2021 01:37 PM

Anonymous Scenic walks with my family and friends.
4/20/2021 02:33 PM

Q2 How do you use the space?

Anonymous Hiking, cycling, walking.
4/20/2021 02:40 PM

Anonymous Recreation (walking along the river).
4/20/2021 02:43 PM

Anonymous Nice place to walk with friends, family and dog
4/20/2021 03:08 PM

Anonymous nature, walking/hiking, enjoying outdoors and shops
4/20/2021 03:25 PM

Anonymous Park setting
4/20/2021 03:33 PM

Anonymous Walk through occasionally
4/20/2021 03:37 PM

Anonymous Walk
4/20/2021 03:38 PM

Anonymous Walking dog
4/20/2021 03:41 PM

██████ Sit by the river.
4/20/2021 03:44 PM

Anonymous Walk (live near)
4/20/2021 03:56 PM

Anonymous Walking
4/20/2021 03:59 PM

Anonymous I walk most every day around the Mill Pond.
4/20/2021 04:04 PM

Anonymous Walking the dog and we live on queen
4/20/2021 04:09 PM

Anonymous Visiting with friends, enjoying the river
4/20/2021 04:24 PM

Anonymous Walking
4/20/2021 04:25 PM

Anonymous Leisurely visit
4/20/2021 04:26 PM

Anonymous Walking and to visit Next Time Around
4/20/2021 04:32 PM

Anonymous Relaxing, scenery, occasional fishing
4/20/2021 04:38 PM

Anonymous Walking
4/20/2021 04:48 PM

Anonymous we host a canoe race launched from there each year. we also hike and walk there often
4/20/2021 04:59 PM

Anonymous Recreation.
4/20/2021 05:02 PM

Anonymous Walking& watching the River
4/20/2021 05:04 PM

Anonymous For hiking and watching wild life in the river.
4/20/2021 05:08 PM

Anonymous Pass through on walks to the businesses on Milling Road
4/20/2021 05:08 PM

Anonymous Walks with family, enjoying scenery and area.
4/20/2021 05:12 PM

Anonymous Trail access along the Speed River.
4/20/2021 05:14 PM

Anonymous Walk by with the dog, admire the history of the buildings and pray they can be revitalized to their fullest potential, while maintaining historical integrity.
4/20/2021 05:33 PM

Q2 How do you use the space?

Anonymous 4/20/2021 05:40 PM	Walking dog, riding bike	Anonymous 4/20/2021 07:35 PM	Walking.
Anonymous 4/20/2021 05:46 PM	Walks nearby and through it.	Anonymous 4/20/2021 07:41 PM	Enjoy the river
Anonymous 4/20/2021 05:46 PM	Walks with family and dog, running/trail - marathon training	Anonymous 4/20/2021 07:43 PM	Walking, scenery
Anonymous 4/20/2021 05:54 PM	Walking, watch the water	Anonymous 4/20/2021 07:47 PM	Walking the dog
Anonymous 4/20/2021 05:59 PM	Walking	Anonymous 4/20/2021 07:56 PM	Walking trails.
Anonymous 4/20/2021 06:04 PM	Leisure	Anonymous 4/20/2021 08:01 PM	Recreational
Anonymous 4/20/2021 06:17 PM	Walk my Dog, subs from Papou's, biking there.	Anonymous 4/20/2021 08:04 PM	Walking Trails.
Anonymous 4/20/2021 06:20 PM	Trail walking, fishing, enjoying the nature as it sits	Anonymous 4/20/2021 08:04 PM	Sit and watch water/birds
Anonymous 4/20/2021 06:23 PM	Walks and hikes bike riding	Anonymous 4/20/2021 08:06 PM	Walk down to river area
Anonymous 4/20/2021 06:24 PM	Walking and riding bikes	Anonymous 4/20/2021 08:08 PM	Walking dogs, walks after dinner
Anonymous 4/20/2021 06:51 PM	Picnic with my 2 yr old, biking and walking.	Anonymous 4/20/2021 08:09 PM	Walking
Anonymous 4/20/2021 06:58 PM	I love nearby and walk the area frequently	Anonymous 4/20/2021 08:10 PM	Photography
Anonymous 4/20/2021 07:05 PM	Walking	<div></div> 4/20/2021 08:43 PM	Walk , look around
Anonymous 4/20/2021 07:10 PM	Walking trails. Visit waterfall.	Anonymous 4/20/2021 08:57 PM	To look at the river with my toddler and enjoy the lights at night.

Q2 How do you use the space?

Anonymous
4/20/2021 08:59 PM
We Walk the trail and stop to enjoy the waterfall and animals. I usually bring my camera to take photos and my children enjoy the area.

Anonymous
4/20/2021 09:01 PM
walking

Anonymous
4/20/2021 09:14 PM
-Operate a business on Milling - parking

Anonymous
4/20/2021 09:17 PM
We walk around the area. Sometimes sit on the benches and eat ice cream

Anonymous
4/20/2021 09:19 PM
We go for walks around there

Anonymous
4/20/2021 09:27 PM
Walking Hiking Canoeing

Anonymous
4/20/2021 09:45 PM
I use the field to train my lab

Anonymous
4/20/2021 10:10 PM
Walking. Enjoying the view.

Anonymous
4/20/2021 10:14 PM
Sit by the river. Fish. Walk the trails.

Anonymous
4/20/2021 10:18 PM
Walking

Anonymous
4/20/2021 10:31 PM
Visual rest spot

Anonymous
4/21/2021 04:47 AM
Outdoor space, walking, bike riding

Anonymous
4/21/2021 06:18 AM
Walk my dog

Anonymous
4/21/2021 06:52 AM
Launch kayak and take dog for a walk behind the mill. Also fish there from time to time. Park on the street and walk the trail along the pond.

Anonymous
4/21/2021 07:28 AM
We own and operate a business (Sparrow Brewing & Roasting Co.) www.SparrowBrewCo.com . We visit with our children to walk the trails and enjoy the nature. Looking for more opportunity to spend more time in the area...ie commercial activities.

Anonymous
4/21/2021 07:31 AM
Leisurely walk and/or fishing

Anonymous
4/21/2021 07:55 AM
Walk the dog

Anonymous
4/21/2021 08:01 AM
Running biking

Anonymous
4/21/2021 08:22 AM
Walking, looking at some of Hespeler's historic building that are still standing.

Anonymous
4/21/2021 08:27 AM
Walking, exercise, relax

Anonymous
4/21/2021 08:32 AM
Walking

Anonymous
4/21/2021 09:04 AM
Family walks to downtown to visit the breweries, restaurants, ice cream shop, Cafe and little stores etc

Anonymous
4/21/2021 09:15 AM
Walking with and without dogs and grandkids. Looking at the river.

Anonymous
4/21/2021 09:29 AM
Use businesses and walk trails

Anonymous
4/21/2021 09:31 AM
Quiet contemplation

Q2 How do you use the space?

Anonymous Look at the river
4/21/2021 10:28 AM

Anonymous Walking and sights
4/21/2021 10:29 AM

Anonymous recreational
4/21/2021 10:38 AM

Anonymous Relax and enjoy the view
4/21/2021 10:41 AM

Anonymous I visit daily, for walking with my dog,
4/21/2021 11:24 AM enjoying nature, and to see the
waterfall and wildlife- fish, turtles,
birds.

Anonymous Walking
4/21/2021 11:41 AM

Anonymous Walking/ local businesses
4/21/2021 11:45 AM

Anonymous walk and cycle
4/21/2021 12:00 PM

Anonymous Walking
4/21/2021 12:10 PM

Anonymous Walk the area
4/21/2021 12:12 PM

Anonymous To enjoy the scenery near the river
4/21/2021 12:32 PM

Anonymous Leisurely walks with family or pets
4/21/2021 12:37 PM

Anonymous sitting area overlooking river
4/21/2021 12:44 PM

Anonymous My partner and I like to sit and look
4/21/2021 02:54 PM out at the water.

Anonymous walking
4/21/2021 03:13 PM

Anonymous CrossFit gym, yoga studio
4/21/2021 03:51 PM

Anonymous Exercise
4/21/2021 04:01 PM

Anonymous A spot to hang out with the dog, walk,
4/21/2021 04:05 PM watch the river and look forward
turtles etc with the kids

Anonymous Walk the dog - enjoy looking at the
4/21/2021 04:38 PM River and nature.

Anonymous Just sit feel peaceful for a few
4/21/2021 04:55 PM minutes.

Anonymous Bike and walk and sit.
4/21/2021 07:10 PM

Anonymous For walking
4/21/2021 07:26 PM

Anonymous Enjoy the view!
4/21/2021 09:20 PM

Anonymous Just walking with my children along
4/21/2021 09:38 PM the waterfront

Anonymous Access to trails and had previously
4/21/2021 09:53 PM been going to registered massage
therapy in the red brick building

Walking my dog, visiting Jacob's
4/21/2021 10:50 PM Landing after going to ice cream
downtown

Q2 How do you use the space?

Anonymous
4/22/2021 06:03 AM
As a trail access point and photography.

Anonymous
4/22/2021 11:17 AM
Walks, general recreation

Anonymous
4/22/2021 04:46 PM
Walks along the river side. Street level small shops in heritage style would be nice

Anonymous
4/22/2021 07:50 PM
Trail

Anonymous
4/22/2021 09:26 PM
Walk. Take photos. Relax

Anonymous
4/23/2021 07:09 AM
Relaxing hiking.

Anonymous
4/23/2021 08:52 AM
leisure

Anonymous
4/23/2021 10:29 AM
Use the trail for walking and biking

Anonymous
4/23/2021 11:10 AM
Leisure walks

Anonymous
4/23/2021 07:03 PM
Walking and jogging

Anonymous
4/23/2021 07:14 PM
Walking

Anonymous
4/23/2021 07:58 PM
To access walk trail and massage at building

Anonymous
4/24/2021 08:44 AM
Walks

Anonymous
4/24/2021 09:21 AM
Walks, sitting down to enjoy the water, and using the path to get to and from Mill Pond neighbourhood and Downtown Hespeler.

Anonymous
4/24/2021 11:45 AM
Walking my dogs

Anonymous
4/24/2021 11:56 AM
Mostly just walk through, not much to do there.

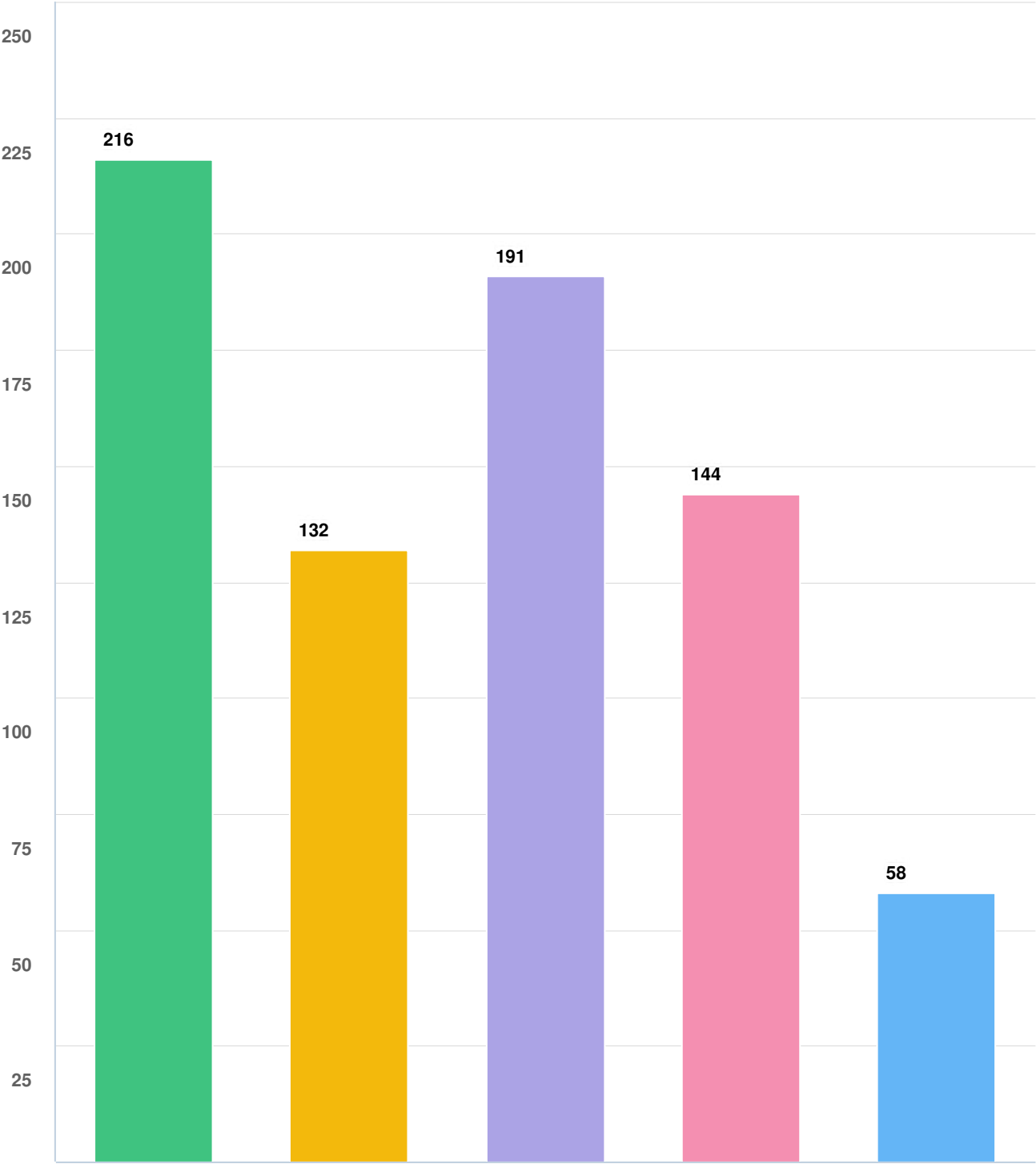
Anonymous
4/24/2021 12:00 PM
Overlook the river, walk by

Anonymous
4/24/2021 06:31 PM
Leisurely walks and relaxation

Anonymous
4/25/2021 03:00 PM
Fishing

Anonymous
4/25/2021 09:09 PM
Fishing, walking

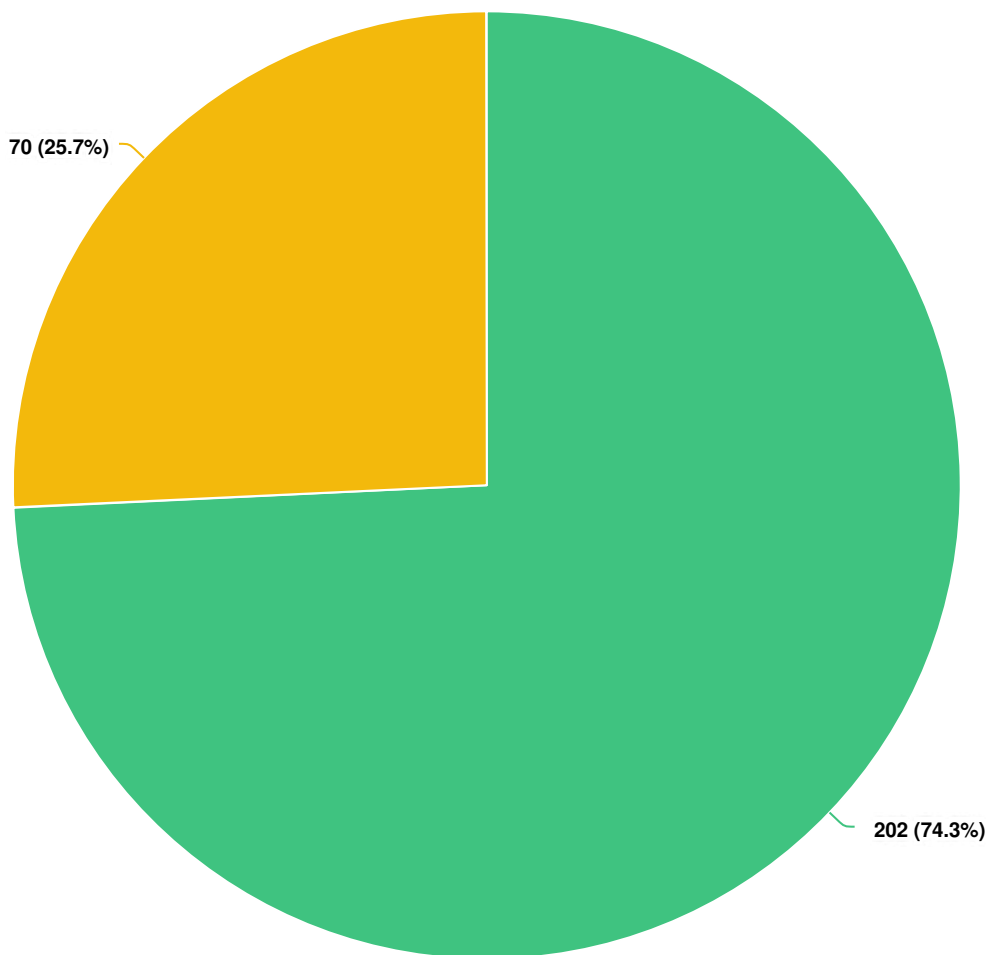
Q3 How would you like to use Milling Road and Jacob's Landing in the future? (choose all that apply)



Question options

- Leisurely visit
- Small gatherings & events
- Farmers' Market
- Shopping
- Other (please specify)

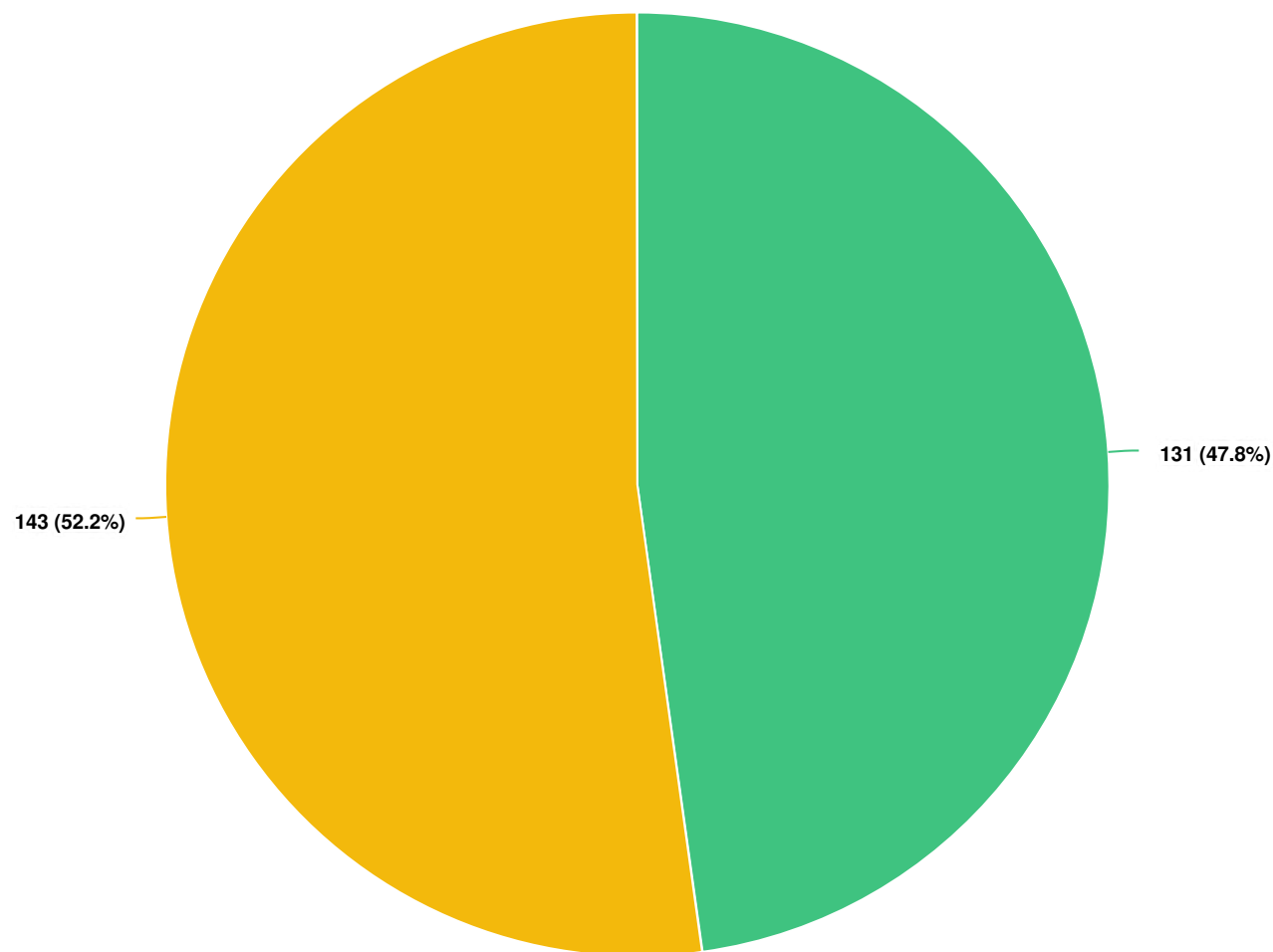
Q4 Would you like to see more commercial business opportunities on Milling Road?



Question options

Yes No

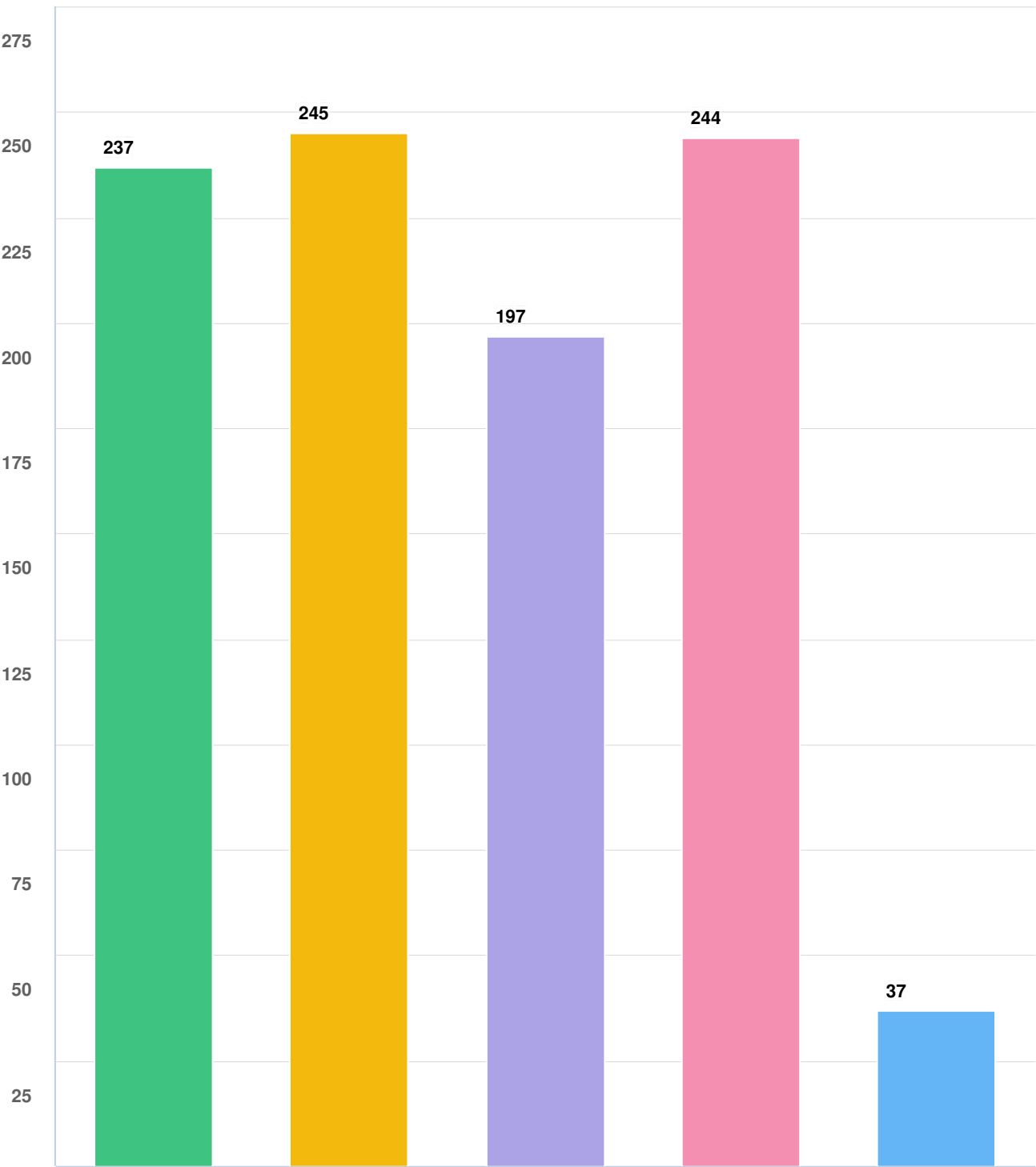
Q5 Should Milling Road be designed as a curbed road or a curbless road?



Question options

- Design Milling Road as a traditional curbed road to provide separation from pedestrians and vehicles.
- Design Milling Road as a curbless road with traffic calming measures to provide flexibly space for pedestrians while providing vehicular access and parking.

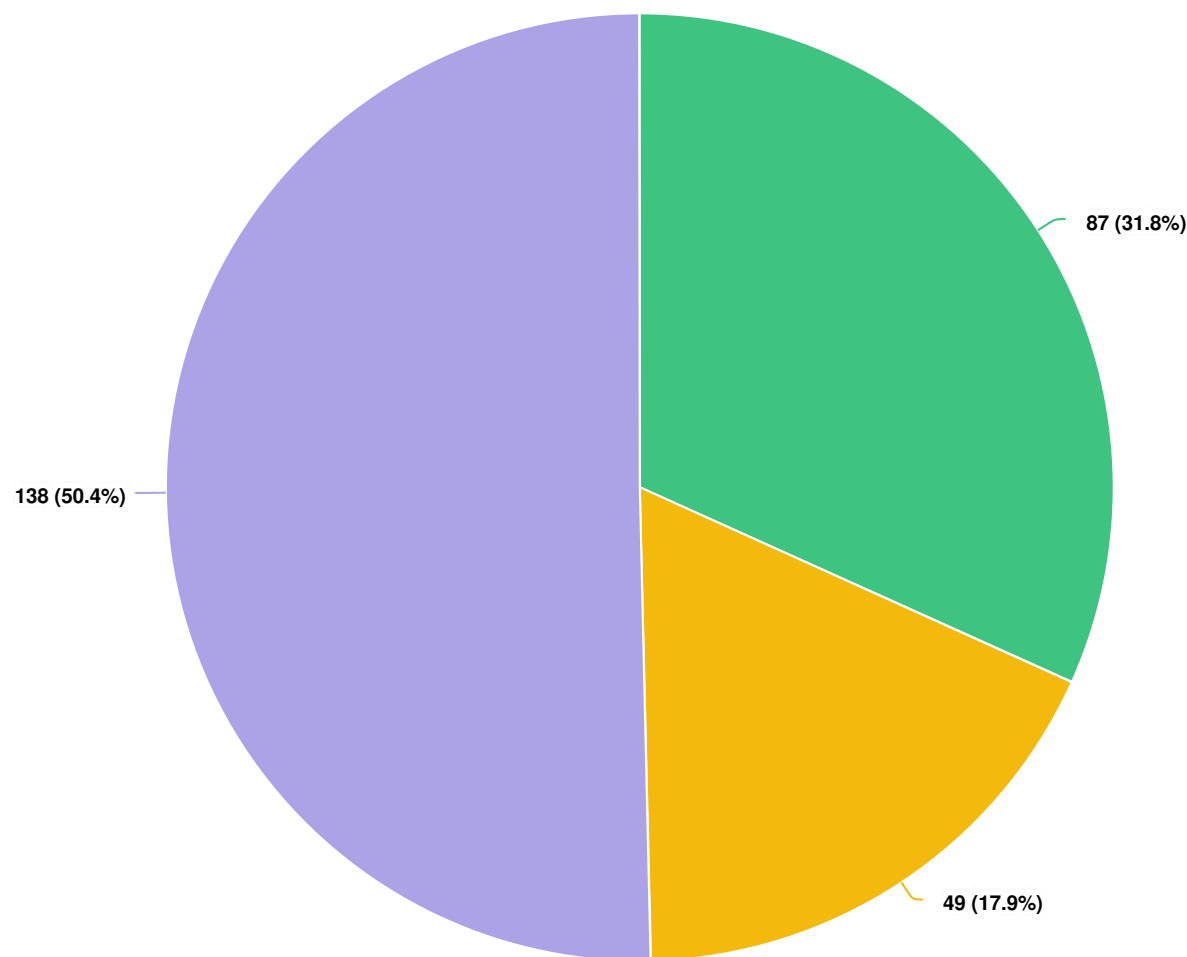
Q6 What type of amenities would you like to see on Milling Road? (choose all that apply)



Question options

- Seating ● Pedestrian lighting ● Bicycle parking ● Waste receptacles ● Other (please specify)

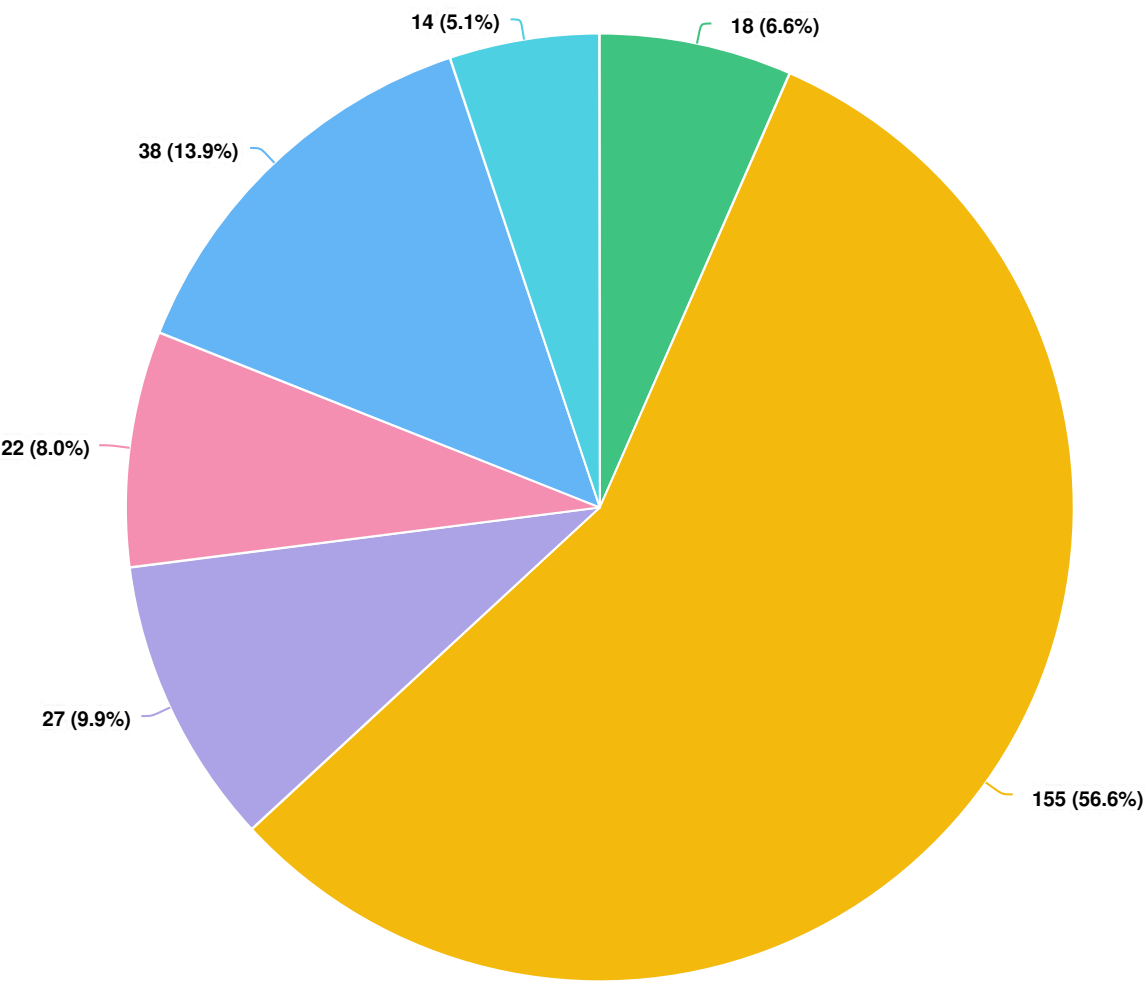
Q7 Would you like Milling Road to have on-street parking?



Question options

- No on-street parking
- Yes, parking along both sides
- Yes, parking along one side

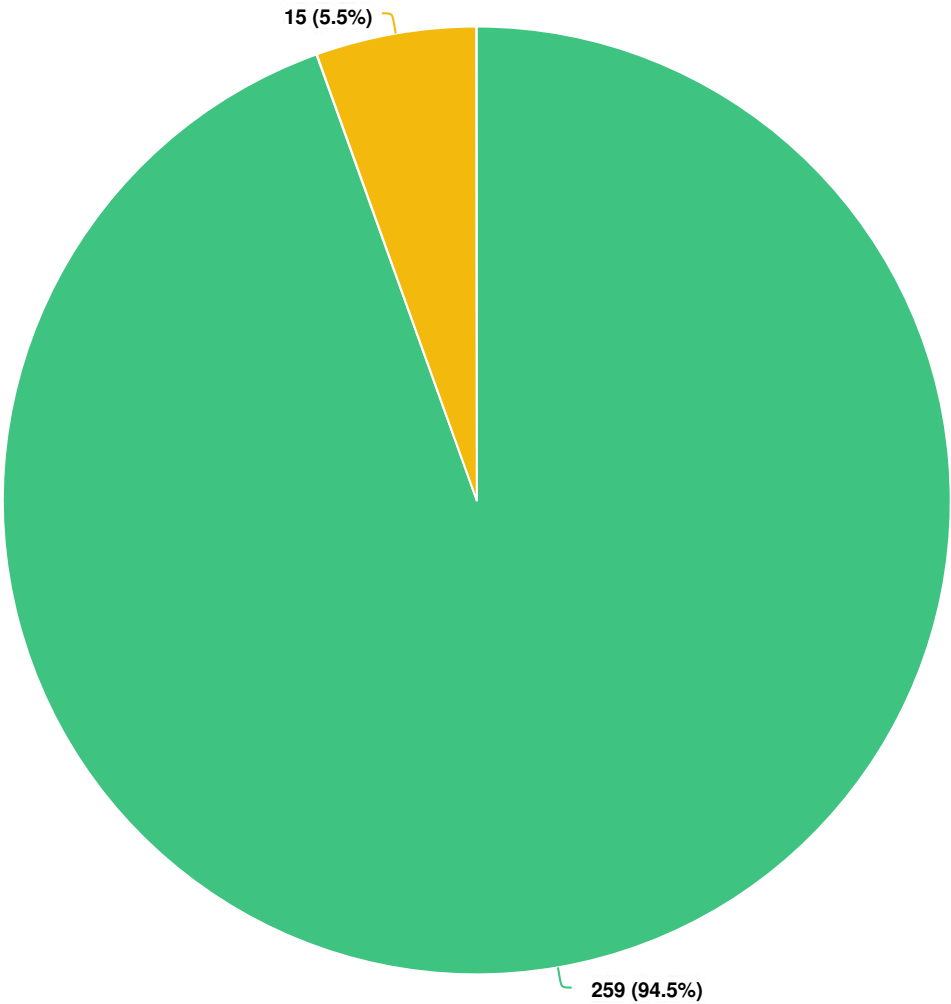
Q8 | Would you like Milling Road to have unique placemaking traits, such as a design theme through paving, signage, site furnishings, etc.?



Question options

- No theme Historical Industrial Contemporary Creative Other (please specify)

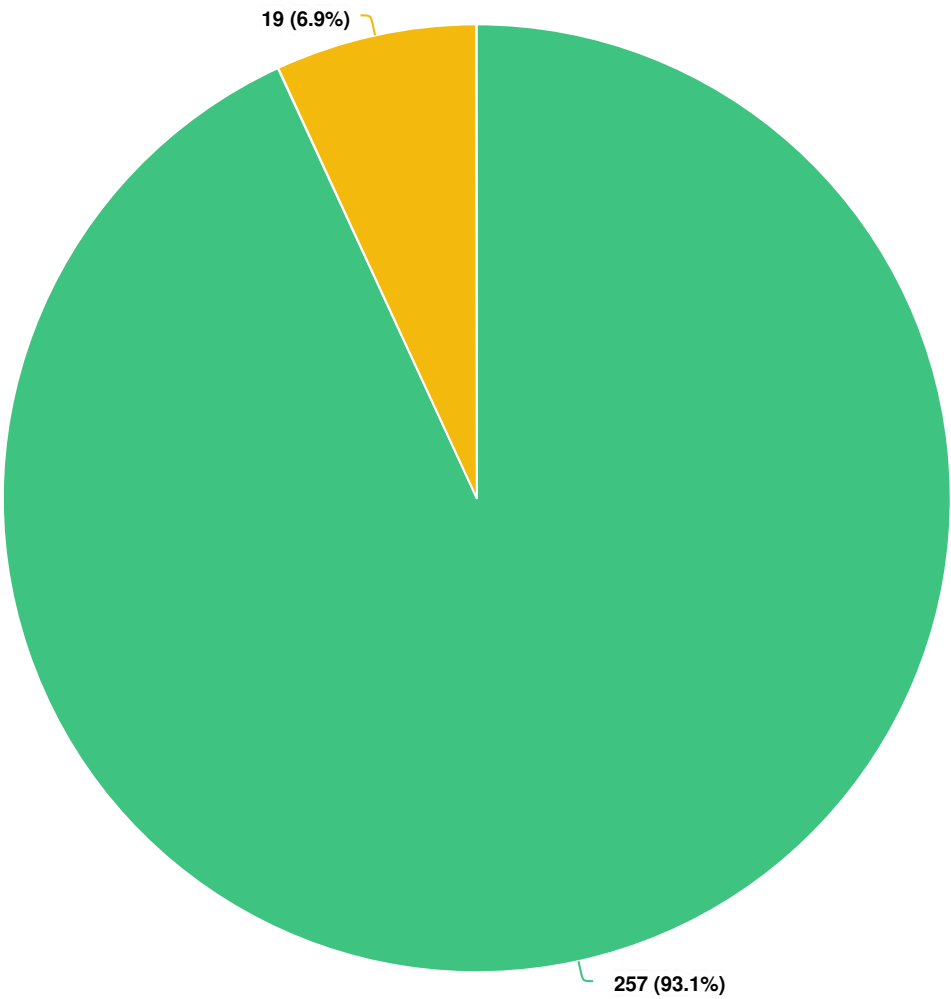
Q9 | Would you like Milling Road to be designed with sustainability features, such as permeable paving and drought-tolerant planting?



Question options

Yes No

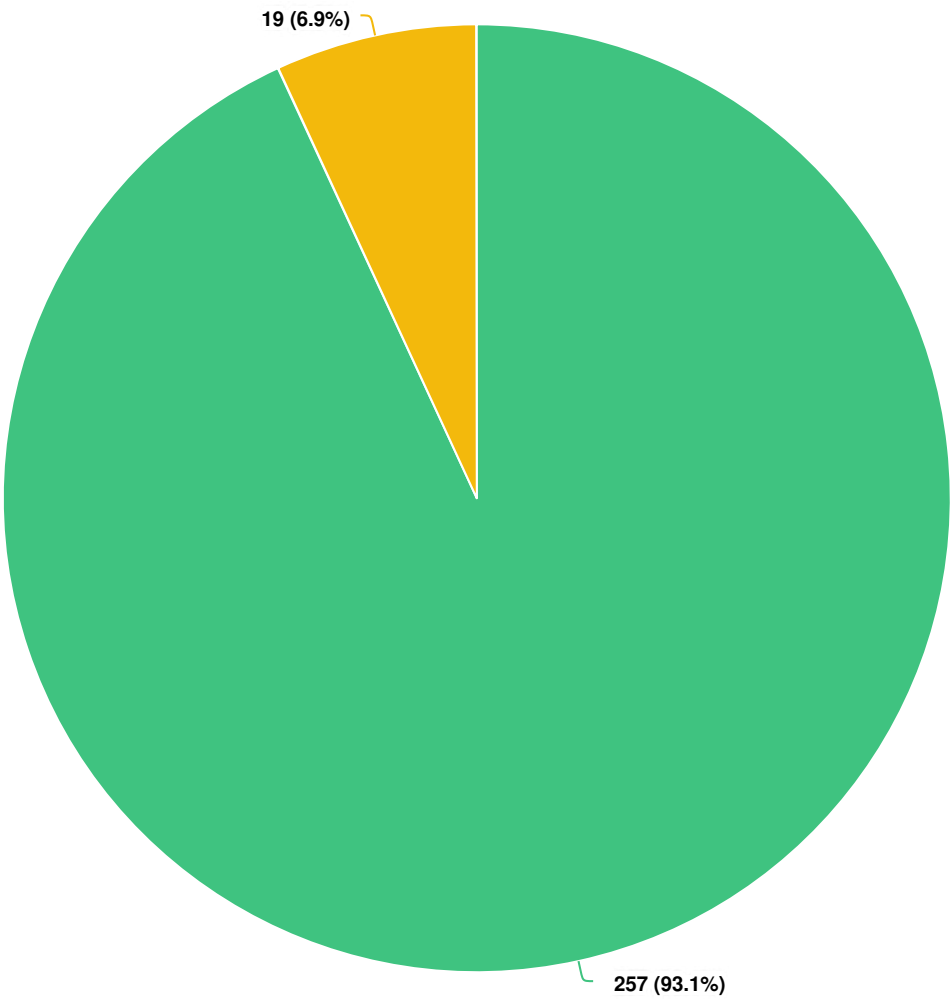
Q10 Should street trees be included in the design of Milling Road?



Question options

- Yes
- No

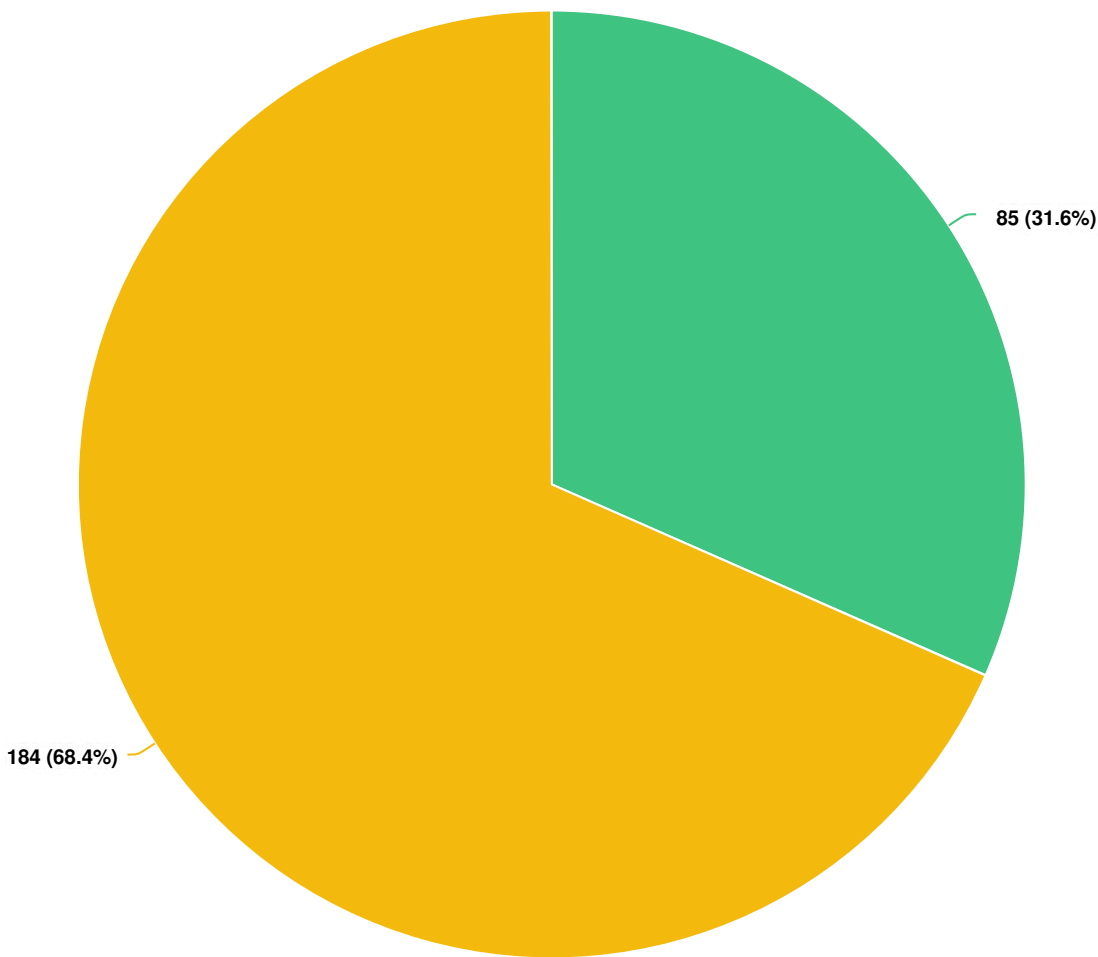
Q10 Should street trees be included in the design of Milling Road?



Question options

- Yes
- No

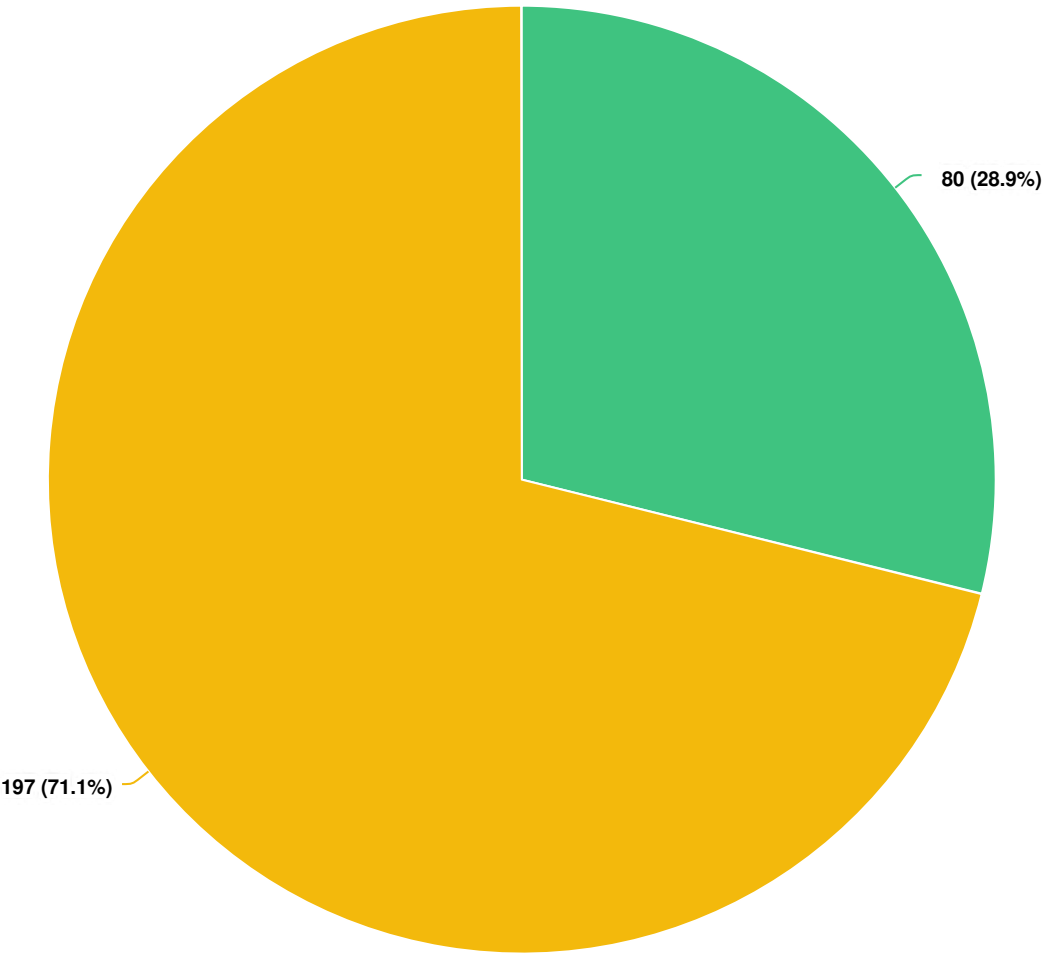
Q11 | Is the current signage on Milling Road effective, or could it benefit from additional signage and wayfinding?



Question options

- The current signage is effective
- It could benefit from additional signage

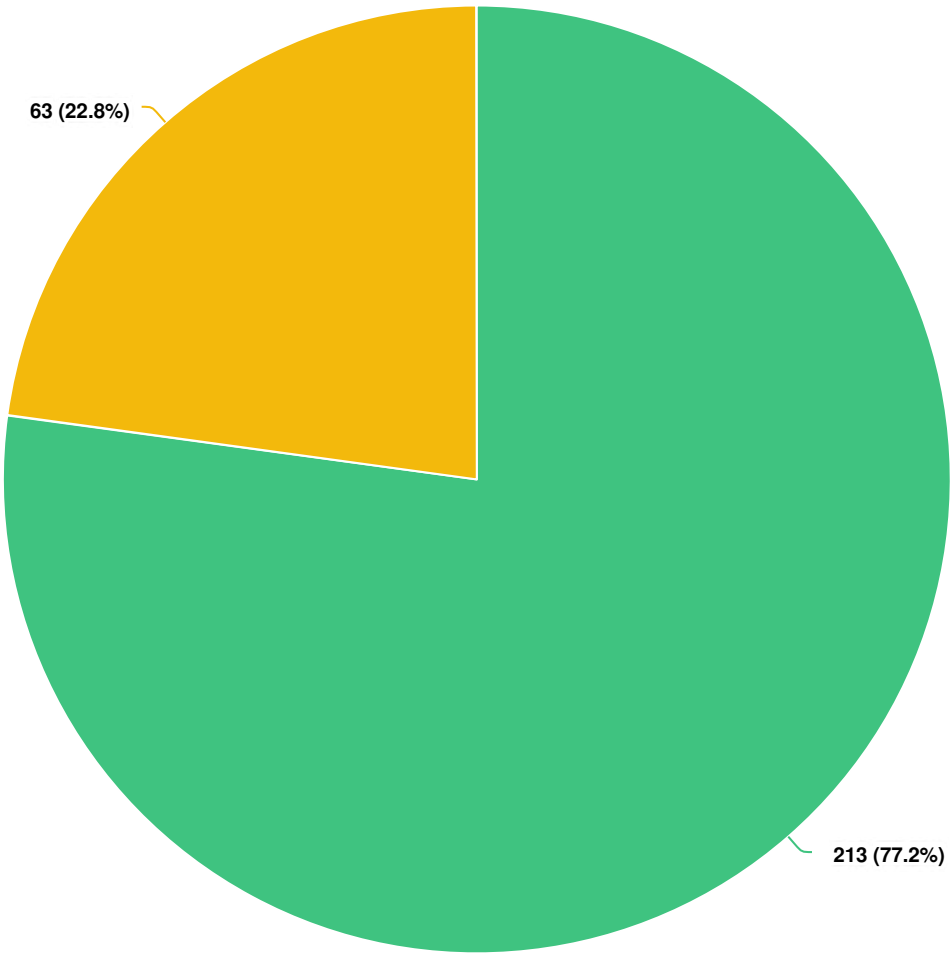
Q12 | Do you feel comfortable walking down Milling Road, or would it benefit from additional security such as lighting, traffic calming measures, and increased visual surveillance (broader sight lines)?



Question options

- I feel comfortable as it is
- It could benefit from additional security

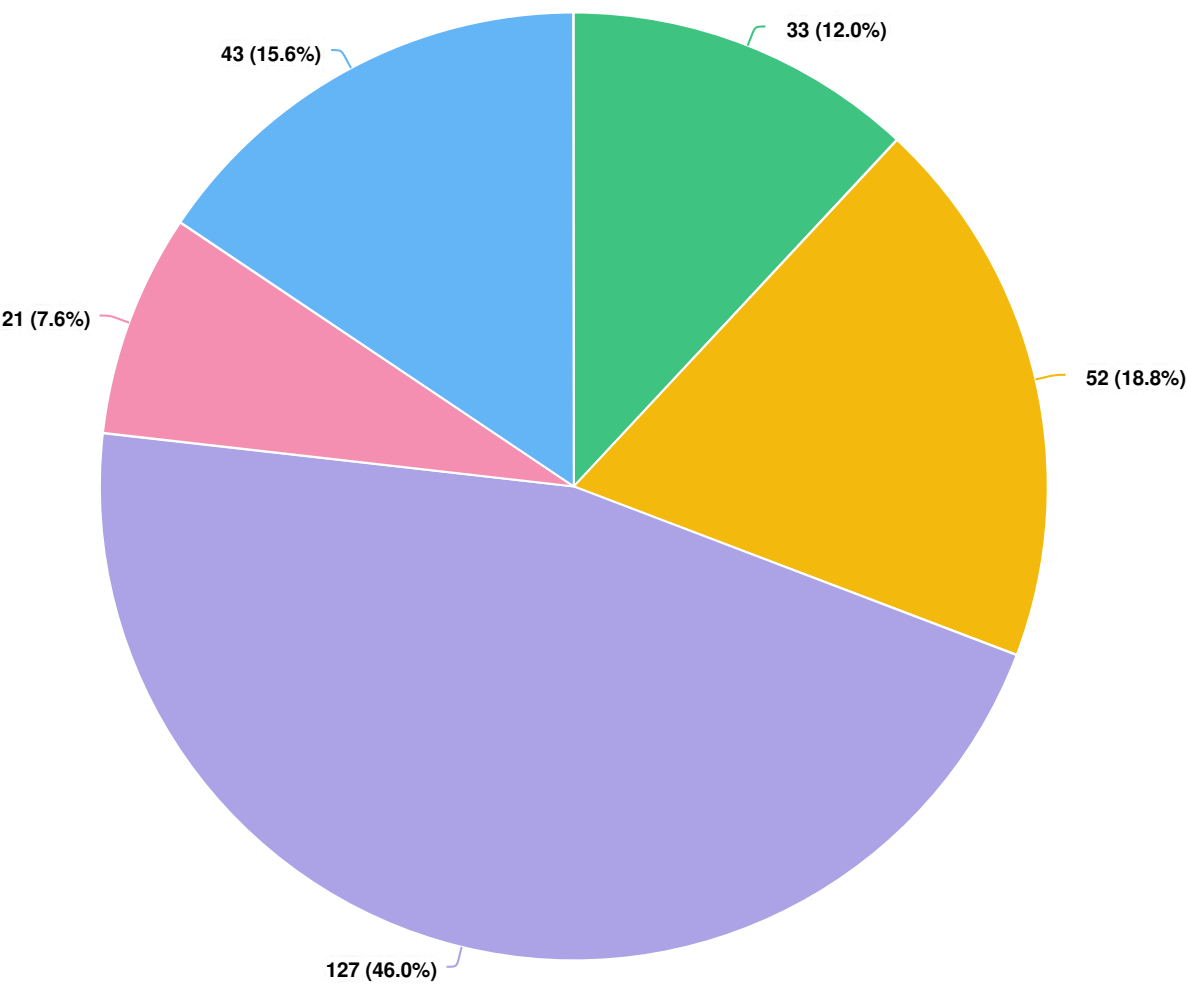
Q13 Would you like Milling Road to be more animated with a sense of identity though public art?



Question options

Yes No

Q14 | If the City buildings beside Jacob’s Landing were to be repurposed, what kind of use would you like to see?



Question options

- Event space
- Retail
- Food services
- Just remove the buildings
- Other (please specify)

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
3/17/2021 05:51 PM

Access/ view to river and to trails

Anonymous
3/18/2021 12:21 PM

As a Cambridge resident and graduate urban design student from UCL in London UK, I have found that this site parallels greatly with Kings Cross's Granary Square. Hope you can use this as a strong precedent. Only thing that might limit this project based on this precedent is that planning and urban design is more flexible/creative in the UK (no zoning/by-laws) vs Ontario (perscriptive zoning/by-laws). Here are the comparisons: Granary Sqaure, London
https://www.google.ca/maps/@51.5348501,-0.1247258,3a,75y,306.03h,89.27t/data=!3m6!1e1!3m4!1s57CCO8M6J6qRYBhbW_dTXg!2e0!7i13312!8i6656
Jacobs Landing, Hespeler
https://www.google.ca/maps/@43.432401,-80.3111919,3a,49.2y,290.23h,90.08t/data=!3m6!1e1!3m4!1sL9k_Jb-Xg4pRSNUfoCVheg!2e0!7i16384!8i8192
Theres great potential here.. take notes from it :)

Anonymous
3/20/2021 10:52 AM

I would like to see Milling Rd's social, cultural and architectural history identified by becoming a place that contributes to the livability and appeal of Hespeler Village. A mix of affordable residential living and commercial business could provide a multicultural, sustainable historic area. The current trees should be saved.

Anonymous
3/26/2021 07:01 AM

Please be advised that the Mill trail can not continue through the property parking lot at 25 Milling Road. We accept trucks and need parking and this entire area for our current occupants

Anonymous
3/26/2021 08:30 AM

we have trucks delivered here daily as this is an industrial warehouse Needs to have proper access for truck and trailer traffic. has been a furniture factory and warehouse location for over 100 years now. thanks

3/31/2021 03:38 PM

If the City buildings are no longer operational, they should be decommished and demolished to allow the Jacobs landing pavilion to be extended. It would be ideal for the private property owner at the end of Milling Street to allow the multi-use trail along the south boundary along the river bank to connect with the current Mill Run trailhead. Only allow car parking on the north side of Milling Street to allow plantings & street-scaping on the south side of the street.

Anonymous
4/20/2021 10:43 AM

Just looking for something that is community/pedestrian friendly. Be nice to have a relaxing destination with seating, shopping, entertainment.

Anonymous
4/20/2021 10:47 AM

This could be a great area, with retail spaces/rental and apartments above while including landscaping for leisure.

Anonymous
4/20/2021 10:51 AM

Keep the feel of the area and promote nature. Keep stone features.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 11:06 AM
It's a beautiful building that for sure could be restored as shopping or more condos, I believe it'll help bring business to downtown hespeler

Anonymous
Trails connections to adjacent trails

Anonymous
4/20/2021 11:10 AM
This area is very charming and has high historical value that contributes to the "small town" feel of downtown Hespeler. I think it's really important that whatever these buildings are used for, the design still has historical character and we dont just lose these buildings to modern industrial-looking architecture. It ends up looking tacky. It would be great to keep the beauty and greenery and have some more spaces for small business to open up or more small restaurants.

Anonymous
4/20/2021 11:25 AM
N/A

Anonymous
4/20/2021 11:35 AM
This could also become an actual town square for Hespeler that is flanked on most of its sides by food services and food trucks.

Anonymous
4/20/2021 11:39 AM
Please include tree planting and greenspace, even if small, in your consideration. We need more trees in Hespeler with 400 of them being removed from the Forbes estate close by to this area. Please do not make this another concrete space, especially given the proximity to the river and conservation area.

Anonymous
4/20/2021 11:43 AM
Pedestrian only space. Should not be able to drive down. Food services and retail. Patio spaces for outdoor dining. Art. There are so many young families in the area, there needs to


Anonymous
4/20/2021 11:51 AM
Bring more urban living. Need to have more people in the area in other for business to survive.

Anonymous
4/20/2021 11:56 AM
Refurbished building facade.
Potential multi use commercial/ brewery and retail.

Anonymous
4/20/2021 12:14 PM
Less is best...just a bit of TLC..

Anonymous
4/20/2021 12:15 PM
Some small shops that could hold a coffee shop/bakery, maybe a place to grab lunch then go and sit at some outdoor tables! Keep to Hespeler's historic roots while being creative!

Anonymous
4/20/2021 12:22 PM
Please visit/review Spencer Smith Park in Burlington. I know our space isn't as large but we could build something comparable to really increase visitation. Burlington's festivals are extremely well known.


4/20/2021 12:32 PM
I think these beautiful buildings have the potential to house both retail and restaurant/food businesses, as well as event space. It would add so much to the feel of the community feel of the village while preserving its history.

Anonymous
4/20/2021 12:38 PM
We have a wonderful opportunity in Hespeler to turn this space into something similar to gaslight in Galt and the distillery in Toronto. It is a perfect place for creative food and retail that draw people to hespeler and give the hespeler community a place to consume together.

Anonymous
4/20/2021 12:41 PM
Please preserve the beautiful historic details of downtown.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 12:48 PM
put a big blackboard so people of hespeler can contribute their own art, thoughts and ideas just like the one that was at the corner of Guelph Ave and Queen St. Beside the Aging Oak. It was great to personalize Hespeler and its people who were willing to share and get creative! Thank you for asking:)

Anonymous
4/20/2021 12:51 PM
Please ensure any new building especially mid-high rise contain "bird friendly" windows to limit bird strikes. The river is an essential migration route. A community garden or a pollination garden would also be wondering.

Anonymous
4/20/2021 12:58 PM
Family oriented to make use of location by trail and river

Anonymous
4/20/2021 01:08 PM
I love that it is being redeveloped. It would be great to have outdoor seating, patio, food services, unique shops (much like shops found on Queen St), a kids natural play area, art installations, historical info, animal conservation efforts. Very exciting! Thanks for reaching out for input.

Anonymous
4/20/2021 01:10 PM
Less condos. More space for families and recreation, places to walk and just go to relax by the river. If condos go riverfront people cannot go riverfront and that is a very ignorant thing to do to people who live in Hespeler is remove the waterfront from them. Don't take away out water front and ecosystem .

Anonymous
4/20/2021 01:15 PM
Do something to presurve Hespeler

Anonymous
4/20/2021 01:18 PM
So much could be done here to involve community - definitely Hespeler Furniture Company could make wonderful apartments. But people from Cambridge can't afford to 'buy' so more well priced rental is needed. Hopefully this will be geared to the people who live here. But yes, trails, walks, markets, all so close to downtown would be lovely.

Anonymous
4/20/2021 01:18 PM
Recognize that the city building has huge historical importance for Hespeler, Ontario, Canada and the world as it is the location of the first long-distance hydro transmission line from Niagara Falls.

Anonymous
4/20/2021 01:20 PM
I do not wish to see the buildings be torn down. It's very picturistic as is. I would also love to see art murals on relatively bland looking worn out walls.

Anonymous
4/20/2021 01:21 PM
I do NOT want to see more eyesore apartment buildings being put up. Hespeler cannot handle more traffic in its small core.

Anonymous
4/20/2021 01:24 PM
Creating an arts oriented development like Toronto's distillery district would be a wonderful addition the the region, incorporating retail, different specialty markets, dining and the arts

Anonymous
4/20/2021 01:24 PM
Please make it like a Kensington market or st Lawrence market vibe. I moved from Toronto and this is missing in the area. Too much big box store/restaurants. Need more culture.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

- Anonymous
4/20/2021 01:24 PM Please keep our History ! it is exciting to see this area having a purpose but please keep the history the city building would be a great restaurant
- Anonymous
4/20/2021 01:24 PM dont do anything that brings too much traffic to the area - it would be nice for locals to continue to enjoy this hidden treasure.
- Anonymous
4/20/2021 01:29 PM In accordance to question 14, if it wasn't retail or a riverside restaurant, event space would be good- better yet a restaurant space overlooking the river with event space built in, or repurpose the space for the farmers market. I love the farmers market where it is currently, however an indoor or partially indoor space would be great so that inclement weather isn't a factor. Refurbishing the steps and creating a larger and more accessible space for everyone, with an eye to beautification, or a secondary space to enjoy the river after the city buildings further down the river would be lovely. I feel like the area could be better used, and better enjoyed by the people of hespeler if the infrastructure and trails existed. Maybe even a trail that hooks up to the mill run on the other side of the railway bridge!
- Anonymous
4/20/2021 01:41 PM It would be a great attraction if it was pedestrians only walking street with patio restaurants and second story shopping. Butcher shop or bakery yoga studio art studio.
- Anonymous
4/20/2021 01:44 PM A natural playscape for kids would be great

- Anonymous
4/20/2021 02:06 PM Room for 9 outdoor patio, outdoor cafe seating etc.. A space with seating right along water would be ideal
- Anonymous
4/20/2021 02:09 PM Make the area more friendly/safe for people walking to/from trails and safe to those who wish to bike. Too much traffic on downtown roads with current and future development. Connect the Millpond trail to the trail along the Speed River to Riverside Park. Restrict the height of any buildings to 3 to 4 stories. Retain historical features.
- Anonymous
4/20/2021 02:20 PM Over 20 yrs ago a City funded study stated to turn the city building on the river into retail use, like a restaurant, and you are still talking about it. Do it now. NO more development of apartment buildings. Buy the property as recommended in the Hespeler River Activation Plan across from Milling Road/Guelph Ave. Part Lot 31, Plan 803, Part Lot 2, Plan 802 and more particularly described as Part 3 on Reference Plan 67R-3039; for an active greenspace as the Plan stated.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 02:31 PM Make it habitable and go big 30 story highrise

Anonymous
4/20/2021 02:43 PM I think a general revitalization of the street would be great, but, it would be beneficial to keep the character of Hespeler (e.g., quaint small-town Ontario). I would love to see small shops, places to eat/takeout, cafes. It would be neat also to see some patio space where people could sit/walk along the river.

Anonymous
4/20/2021 03:01 PM I would like to see Milling St to be closed to cars and only be accessible to pedestrians and bikes. With searying to eat or farmers market. Restaurants and pubs or retail. Much like the distilleries in TO.

Anonymous
4/20/2021 03:25 PM Mixed use - small businesses, coffee shops, eateries, breweries, event space etc..

Anonymous
4/20/2021 03:33 PM I think this is a great project and any effort to help extend and revitalize the downtown area is great

Anonymous
4/20/2021 03:37 PM Room for kids/families

Anonymous
4/20/2021 03:41 PM I always advocate repurposing buildings! They are beautiful spaces and would be great for additional eateries overlooking the water etc. A farmers market would also be lovely (perhaps something like Kitchener market, with both vendors and eateries?) More greenery would also be nice, perhaps a small food forest?

Anonymous
4/20/2021 03:56 PM It should look like the Distillery district in Toronto... amazing old brick near river. [REDACTED]

Anonymous
4/20/2021 03:59 PM Not to much of an addition as the downtown core has Already grown exponentially.

Anonymous
4/20/2021 04:04 PM Make a connection to the Mill Run Trail directly from this street rather than having to go along Sheffield.

Anonymous
4/20/2021 04:09 PM Shops, restaurant, maybe an event space for weddings.

Anonymous
4/20/2021 04:38 PM I think it would be a good place to showcase some of the charm of hespeler. Almost an extension of queen street, a place for small businesses and outdoor events, such as the hespeler farmers market. Maybe some sort of opportunities for renting flea market booths for people who make and sell stuff, without having to pay for a storefront.

Anonymous
4/20/2021 04:59 PM no big concrete ugly buildings; parking lot in area behind the street

Anonymous
4/20/2021 05:02 PM Maintain the historical charm of the area. Stop modernizing the Hespeler region with ugly highrises.

Anonymous
4/20/2021 05:08 PM It would be great as a pedestrian space with parking available behind the current buildings. With restaurants, and open air market.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 05:08 PM
As a regular visitor to a business on Milling Road, the biggest opportunities are parking or cycling facilities. The street parking that exists now is often insufficient if you're visiting during the workday. When I cycle to my appointments on Milling Road, there is no secure space to lock my bike. I usually lock my bike to the stair handrail inside the entrance to the business I am visiting. During the winter months, with street parking on both sides of the road, a plow cannot get down the street and for months it is an sheet of rutted ice. I wouldn't want to see this situation recreated in the new design.

Anonymous
4/20/2021 05:33 PM
I am most pleased the have this opportunity to provide input and equally as thrilled of the potential outcome as we revitalize such a quaint village we can share proudly.

Anonymous
4/20/2021 05:40 PM
I have no affiliation with any brewery, I just feel that the area provides an excellent opportunity to consolidate Cambridge's many micro breweries in one area, create a unique visitor experience, and clean up and class up downtown Hespeler at the same time. This would be beneficial for businesses, community, and the overall region to create such a District.

Anonymous
4/20/2021 05:46 PM
Plenty of walking space, extension to the trail, and preserve historical features. Perhaps a small park space as well.

Anonymous
4/20/2021 05:54 PM
Make it like a mini distillery district. Small events, dining - outdoor in good weather, pedestrian friendly. Think about future residential- loft style condos.make it cool

Anonymous
4/20/2021 06:17 PM
I understand the need for more H-town housing, but please make it proportionate height/density for a town like ours. I objecte to the proposed 8 and 6-story apartment buildings which will block the view for hundreds of people used to enjoying the view/sunset from the other side of Queen St. Please reconsider making the old tower the highest part of the development, or at least a Milling Square centerpiece.

Anonymous
4/20/2021 06:20 PM
Listen to the residents. Just listen and do what WE want. I've been to many council meetings and our voices - lots of us there- were not heard. We live here. Stop taking away what makes our village special. Film industry comes here for a reason. It's OUR village and we have the final say

Anonymous
4/20/2021 06:53 PM
as far as art is concerned NOT the current abstract is found in down town Hespeler now There is a place for his work but it is tooway over done for a small village atmosphere

Anonymous
4/20/2021 06:58 PM
Hespeler village is a great area with some real treasures - the ice cream shop, four fathers and the aging oak. The area could really use a refurbishment of that road and more opportunities for unique businesses, outdoor events and fitness spaces.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 07:05 PM
Keep the view of the water and access for canoeing/kayaking. Buildings not to be too tall like the apartment on Queen St. Which I find is an Eye sore seeing it tower the main street.

Anonymous
4/20/2021 07:10 PM
Connection to Mill run trail without going all the way to Sheffield.

Anonymous
4/20/2021 07:41 PM
We need more competition in the bar/restaurant business downtown Hespeler, not enough variety

Anonymous
4/20/2021 08:04 PM
There are lots of homes in the area. I'd like to see this as a space where people can live but also work in Hespeler.


Anonymous
4/20/2021 08:06 PM
Having restaurant space would be unique and well attended. Overlooking the water is critical here, not obstructed by another building in front

Anonymous
4/20/2021 08:08 PM
PLEASE KEEP THE HISTORY. We are losing so much already. The American standard building looks great, that is the direction we should be going. I'm 27 and can honestly say that every one of my friends and family appreciates the historical landmarks that make Hespeler what it is. Not everything needs to be contemporary to draw people in. That's when you become like every other town.

Anonymous
4/20/2021 08:10 PM
I'd like to see it be a tourist destination like the strip in Elmira. Don't know what you do with industrial tenants currently there. Would love to see an artisan village.

Anonymous
4/20/2021 08:13 PM
More of a entertainment district with condo living

Anonymous
4/20/2021 08:23 PM
This is an area of Hespeler that all people like to go because of the River. Keep it simple, not restricted for profit of buildings. Access for all to enjoy the area.


4/20/2021 08:43 PM
Keep it looking like Hespeler and add farmers market

Anonymous
4/20/2021 08:57 PM
Vehicle traffic should be redirected around the north of the buildings, so that Milling Road can be a pedestrian mall. There should also be improvements to make crossing Guelph Avenue safer.

Anonymous
4/20/2021 08:59 PM
I would love to see a permanent all year farmers market like Galt has. Some food/Cafe with outside seating. A kids store would be awesome too, like boutique like. We don't have enough of baby/kids toy/apparel stores in Cambridge. I live very near this location and would love to see the structure be repurposed and lively again.

Anonymous
4/20/2021 09:01 PM
farmers' market

Anonymous
4/20/2021 09:03 PM
Hespeler enjoys a unique quality, different than the rest of Cambridge. It is very important Hespeler maintains its uniqueness and celebrates its individual history. Repurposing the historic buildings while maintaining the fundamentals of what makes Hespeler special is important to the local community.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/20/2021 09:14 PM
Milling Road is a high traffic street with transport truck utilizing it many times a day to access the factory so please take that into account when planning for parking, pedestrian safety, walk ways and adding amenities to the street.

Anonymous
4/20/2021 09:27 PM
Please tour great city's in our province Ottawa / Stratford / Guelph Use their best ideas in our city. No chain link no barriers

Anonymous
4/20/2021 10:10 PM
I look forward to the revitalization if the area.

Anonymous
4/20/2021 10:31 PM
We really need a safe crossing on Guelph Ave at that location.


Anonymous
4/21/2021 08:22 AM
Leave the area alone. Every historical part of Hespeler is being removed or changed and this part of Hespeler should be left alone.

Anonymous
4/21/2021 08:27 AM
A rich cultural destination. Place for a market A place to walk, window shop, sit and get a coffee/snack at a patio cafe. An extension of jacobs landing. A place where people gather.

Anonymous
4/21/2021 08:56 AM
This area is ripe for food services and restaurants.

Anonymous
4/21/2021 09:15 AM
Whatever goes there keep the buildings LOW. No more gray high rise communist block style buildings. Somewhere we need good parking but not curbside.

Anonymous
4/21/2021 09:16 AM
Such potential in that spot on the river sight to invite pedestrians and bikes with outdoor cafes and restaurants and outdoor park like seating. A magnet.


4/21/2021 09:17 AM
With the river, dam and mill pond and the its history Hespeler has the opportunity to become a destination. For this to happen visitors need places to park and the public amenities around the river need to be expanded.

Anonymous
4/21/2021 09:29 AM
It should become Hespeler's Distillery District. Old market theme, event space and unique shopping.

Anonymous
4/21/2021 09:40 AM
Do something that will benefit the community

Anonymous
4/21/2021 09:57 AM
social gathering space - a cool restaurant / bar

Anonymous
4/21/2021 10:28 AM
St Jacobs the mall there

Anonymous
4/21/2021 10:41 AM
food service and hospitality businesses

Anonymous
4/21/2021 11:24 AM
Please consider what the residents would like to see. Also, natural habitat for birds, turtles etc. Should also remain.

Anonymous
4/21/2021 11:41 AM
It would be nice to see the buildings being repurposed and the waste land developed.

Anonymous
4/21/2021 11:45 AM
Building would be perfect for a LA Frank's style restaurant

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

Anonymous
4/21/2021 12:00 PM
I said it all in #3 Please, please, please put some imaginative thinking into creating something unique in this amazing space. Something that will benefit the residents of Cambridge in their daily lives as well as bring tourists (and their wallets) to the area.

Anonymous
4/21/2021 12:12 PM
Somewhat unrelated but a walkable loop with a Boardwalk over the water to connect downtown, the trails, and the milling road area

Anonymous
4/21/2021 12:32 PM
Such a beautiful and scenic area that is so under utilized. Would love something to attract more visitors to this area

Anonymous
4/21/2021 03:05 PM
Food services and restaurants!

Anonymous
4/21/2021 03:13 PM
Riverside Restaurant please

Anonymous
4/21/2021 03:51 PM
Keep the Hespeler feel, small waterside village, no more condos

Anonymous
4/21/2021 04:05 PM
This little area is such a jewel and already has such a cool contemporary industrial vibe going on with the old furniture factory. I've always felt that the space is wasted. So much to build on. A spot for live music, a cafe or restaurant right on the river (think LA Frank's) some kind of a lounge to draw the evening crowd. Live music and martinis. A doughnut shop, a Hawaiian ice stand. Things that will bring in visitors and tourists. Like Hess village in Hamilton. Would lend itself so well to the breweries we already have downtown

Anonymous
4/21/2021 05:47 PM
It would be nice to see a few more restaurants to compliment the location. Make it more of a destination, add to the historical feel that Four Fathers has established a blend of the old and the new.

Anonymous
4/21/2021 07:10 PM
A boardwalk with food trucks would add to it. Such as beaver tails, French fries, ice cream, etc.

Anonymous
4/21/2021 08:09 PM
Farmers market, food service and/or restaurants please and thanks!

Anonymous
4/21/2021 09:53 PM
Is there a master plan for hespeler village? I think this needs to be integrated with the overall vision for the whole area. What does hespeler village need to grow into a self sustaining area to live/work/play? I'm thinking access to groceries and services, food markets, breweries, retail, pharmacy, access to transportation etc. I would really like to see this as part of a bigger plan not a separate entity.

Q15 Please provide any additional feedback on what you'd like to see for the Milling Road area.

[REDACTED]
4/21/2021 10:50 PM

This is such an under-utilized and under-appreciated area! Either retail, or some other type of commercial animation would be a huge benefit. Turn Jacob's Landing into a destination, add a funky historical vibe, like a small Distillery District. 'The Jacob District'? After one town's founder? Maybe Four Fathers would like to relocate to a more central location, the old Milling Road building that is city owned would make a fantastic brewery or massive cafe. Is there also something that can be done with the end of Milling Road? Kind of ends in an awkward parking lot that is not paved. Not sure if it can be looped around to Sheffield because of the railroad, but might be interesting to consider? Or a fancy pedestrian friendly roundabout that cars can turnaround in, but pedestrians are given priority. Maybe a gorgeous red brick small traffic circle at the end for passenger drop off/pick up? Might be nice to add a small parking lot, as I know parking in downtown Hespeler is always hard. Being born and raised in Hespeler, I would love to see Milling Street turned into a destination! Reach out if I can provide any more suggestions.

Anonymous
4/22/2021 06:03 AM

<https://www.marathonvillage.net/#the-scoop> Like this!

Anonymous
4/22/2021 11:17 AM

It would be nice to configure the area as an exclusive pedestrian street, with parking in a designated lot nearby. Restaurants, shopping, and events similar to the vibe of Hess Villiage in Hamilton.

Anonymous
4/22/2021 04:46 PM

A gathering space which showcases Our rich history and promotes small Business..

Anonymous
4/23/2021 07:09 AM

Farmers market crafts patios and coffee/ bake sbops

Anonymous
4/23/2021 10:29 AM

I know that it is out of the project scope, but accessing the area as a pedestrian or cyclist from the other side of the river is not easy and requires going through the intersection of Queen and Guelph which is not pedestrian or cyclist friendly

Anonymous
4/23/2021 01:19 PM

Revitalization is terrific if it keeps historic feels for area and does not promote modern structures or more traffic congestion

Anonymous
4/24/2021 11:56 AM

Children's park.

Anonymous
4/25/2021 09:16 AM

The area could be cicely developed a grocery store, and some other commercial and restaurant would be nice in this area with condominiums above.

Appendix D

EngageCambridge Second Survey

Survey Responses

15 August 2018 - 18 July 2021

Public Information Centre

EngageWR

Project: Milling Road Streetscape



VISITORS

28

CONTRIBUTORS

24

RESPONSES

25

5

Registered

0

Unverified

19

Anonymous

5

Registered

0

Unverified

20

Anonymous



Respondent No: 1

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jun 29, 2021 20:56:54 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 1 (straight)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Children/play

Gatherings

Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)

Q4. Do you prefer a specific paving type?

The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Both concepts satisfy the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

Multiple pieces of public art within Jacob's landing is sufficient

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

not answered



Respondent No: 2

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jun 30, 2021 13:19:36 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 2 (curving)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Children/play

Gatherings

Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)

Q4. Do you prefer a specific paving type?

The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Concept 2 satisfies the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

Multiple pieces of public art within Jacob's landing is sufficient

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

Is there a possibility of a raised platform over the shoreline to connect Mill Run Trail to Jacobs Landing ? That way getting around the private property both literally and figuratively ? Also, there is no level outdoor sports court in Hespeler. Many



Respondent No: 3

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 02, 2021 08:06:37 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Gatherings
Boating
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer?
- Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type?
- The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?
- Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?
- Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 4

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 02, 2021 12:24:48 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Children/play
Gatherings
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer?
- I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type?
- The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?
- Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?
- Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- This would be a great area for patio restaurants
-



Respondent No: 5

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 04, 2021 16:12:40 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 1 (straight)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Concept 1 satisfies the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

No preference

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

Having parking exist on a single side makes it easier for drivers to anticipate movement and for better views of pedestrians possibly crossing the street haphazardly. It will be a distracting space for drivers, so any ability to reduce distractions the better.



Respondent No: 6

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 05, 2021 07:23:54 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Children/play
Gatherings
-
- Q3. Which do you prefer? I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Concept 2 satisfies the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
not answered
-



Respondent No: 7

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 06, 2021 11:59:02 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Gatherings
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer? I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type? No preference
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
not answered
-



Respondent No: 8

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 06, 2021 12:37:58 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 1 (straight)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

No preference

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Both concepts satisfy the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

Provide additional public art within the streetscape to contribute to the sense of place

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

Connection to the Mill Run Trail is of paramount importance to the success of this project. Excluding this connection is a half-measure that will undermine all the other excellent work that has been done.



Respondent No: 9

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 07, 2021 17:09:30 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Gatherings
Boating
-
- Q3. Which do you prefer? I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Neither concept satisfy the vision
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- It feels like a very small piece towards realizing the vision of what this area could be. I'd like to see access to the waterfront improved as it's currently limited. Buildings are not in short supply.
-



Respondent No: 10

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 11:54:52 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 1 (straight)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Children/play

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

No preference

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Both concepts satisfy the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

No preference

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

not answered



Respondent No: 11

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 12:02:53 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Dog amenities
Children/play
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? No preference
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- Please get to doing this idea!! Hespeler needs it to be brought back to life!
-



Respondent No: 12

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 12:19:22 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer? I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Concept 2 satisfies the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 13

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 12:37:46 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Dog amenities
Gatherings
Boating
-
- Q3. Which do you prefer? I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? No preference
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
not answered
-



Respondent No: 14

Login: [REDACTED]

Email: [REDACTED]
[REDACTED]

Responded At: Jul 09, 2021 13:25:27 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 2 (curving)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Dog amenities
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Both concepts satisfy the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

No preference

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

not answered



Respondent No: 15

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 13:29:28 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Dog amenities
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Multiple pieces of public art within Jacob's landing is sufficient
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 16

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 14:00:07 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?	Concept 1 (straight)
Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?	Children/play Gatherings Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
Q3. Which do you prefer?	I prefer to remove the building and see a larger park with more amenities (Phase 2B)
Q4. Do you prefer a specific paving type?	The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?	Both concepts satisfy the vision statement
Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?	Provide additional public art within the streetscape to contribute to the sense of place
Q7. Please provide any additional feedback for Concepts 1 & 2 below.	not answered



Respondent No: 17

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 14:06:14 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Dog amenities
Children/play
Gatherings
-
- Q3. Which do you prefer?
- Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type?
- The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?
- Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?
- No preference
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 18

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 14:18:32 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 2 (curving)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Neither concept satisfy the vision

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

Multiple pieces of public art within Jacob's landing is sufficient

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

Plans look great. Would be great to have a market area there. Some riverside dining or something similar. from what I can see, this is another park, which is great but would be good to enhance the area with some local business as well.



Respondent No: 19

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 16:09:07 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Children/play
Boating
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer?
- I prefer to remove the building and see a larger park with more amenities (Phase 2B)
-
- Q4. Do you prefer a specific paving type?
- The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?
- Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?
- Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 20

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 17:54:52 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Dog amenities
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Multiple pieces of public art within Jacob's landing is sufficient
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 21

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 18:25:05 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

Q1. Which concept do you prefer?

Concept 1 (straight)

Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?

Gatherings

Q3. Which do you prefer?

I prefer to remove the building and see a larger park with more amenities (Phase 2B)

Q4. Do you prefer a specific paving type?

No preference

Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village?

Concept 1 satisfies the vision statement

Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient?

No preference

Q7. Please provide any additional feedback for Concepts 1 & 2 below.

Both concepts make the parking situation a major issue for businesses on Milling. Please leave both sides available for parking.



Respondent No: 22

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 20:55:06 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Neither concept satisfy the vision
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.

If you keep the existing building it should be converted to be used for small businesses such as cafes and other food or retail. The traffic calming, tree-filled and curbless (concept 2) design details are really good but such a place will be dramatically improved if the right kind of small businesses are present. If it is possible to facilitate having small food



Respondent No: 23

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 09, 2021 22:42:32 pm

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 1 (straight)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration? Children/play
Gatherings
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? No preference
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
Develop the building into shops or condos
-



Respondent No: 24

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 10, 2021 10:11:07 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Dog amenities
Children/play
Gatherings
Boating
Mill Run Trail connection (understanding that the private land owners do not wish to connect across their land at this time)
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should contribute to the theme (historic or otherwise) by using materials like unit pavers or stamped concrete.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Both concepts satisfy the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-



Respondent No: 25

Login: [REDACTED]

Email: [REDACTED]

Responded At: Jul 15, 2021 11:03:38 am

Last Seen: [REDACTED]

IP Address: [REDACTED]

-
- Q1. Which concept do you prefer? Concept 2 (curving)
-
- Q2. Regarding the Concept Designs, which activities (if any) should be given additional consideration?
- Dog amenities
Children/play
Gatherings
Boating
-
- Q3. Which do you prefer? Significant efforts should be made to preserve the building, however I'm open to the full park development (Phase 2A & 2B)
-
- Q4. Do you prefer a specific paving type? The streetscape materials should use a mixture of materials to be budget-conscious, with priority given to the pedestrian paving.
-
- Q5. Do you think the proposed Concept Designs meet the vision statement, or should the vision statement be refined to better meet the needs of Milling Road and Hespeler Village? Concept 2 satisfies the vision statement
-
- Q6. Would you like to see additional public art incorporated into the streetscape design, or is public art in Jacob's Landing sufficient? Provide additional public art within the streetscape to contribute to the sense of place
-
- Q7. Please provide any additional feedback for Concepts 1 & 2 below.
- not answered
-

Additional Comments

The following comments were received by email:

1. I was wondering if consideration was given to a skateboard park at Milling Road or anywhere else in Hespeler.
2. I spend a lot of time looking at cycling issues in Cambridge. As you may know, Guelph Avenue is a part of Cambridge's plan for a "network spine" of cycling routes connecting the major areas of the city. The TransformWR climate change plan just passed by Cambridge and all regional councils calls for the network spine to be built so that people of "all abilities" can use it for active transportation. In cycling terms, that means either a multi-use trail or separated bike lanes (not just painted bike lanes on the road).

I know that Guelph Avenue is fairly narrow, but I believe it could take a multi-use trail in place of a sidewalk on one side (just my opinion, I realize the city planners have to figure out the best approach). The bridge is a different issue - it is even narrower. My question is - do either of the alternatives for Milling Road have any pros or cons with regard to developing proper cycling facilities on Guelph Avenue? If so, I hope that issue will be an important consideration in the final decision.

August 24, 2021

Slobodanka Lekic, B.Arch., LEED® AP BD+C
Manager of Building Design & Construction
Corporate Enterprise Department
Sustainable Design & Development
City of Cambridge
50 Dickson Street, PO Box 669
Cambridge ON N1R 5W8

Dear Ms. Lekic,

**RE: Building Structural Condition Assessment - FINAL
30 Milling Road, Cambridge ON**

RJC No. TOR.130844.0001

1.0 INTRODUCTION

Read Jones Christoffersen Ltd. (RJC) was retained by the City of Cambridge to conduct a structural condition assessment of the building located at 30 Milling Road, in Cambridge, Ontario.

In particular, the scope of the investigation undertaken by RJC included the following:

- .1 Detailed review of past reports describing the structural systems and previous evaluation/repair programs undertaken at this facility.
- .2 Discussion with facility staff regarding known history, problems or other items of concern with respect to the building structure.
- .3 A comprehensive visual walk-through examination of the exterior and interior building from grade in order to identify and measure visually obvious signs of distress and/or deterioration in the various systems. The visual review was limited to the accessible, exposed

The date and weather condition at the time of our visit was as follows:

Exterior and Interior Review: July 13, 2021 Cloudy, 24°C

Tim Van Zwol, P.Eng., Ryan Dougherty, E.I.T., and Kyle Brown, E.I.T. performed RJC's site review services.

This report was prepared in accordance with generally accepted engineering practices. No other warranties, either expressed or implied, are made as to the professional services provided under the terms of our contract and included in this report.

Services performed and outlined in this report were based, in part, upon visual observations of the site and

attendant structures. Our opinion cannot be extended to portions of the building that were not reviewed by RJC.

This report is exclusively for the use and benefit of the client identified in the first paragraph of this report and is not for the use and benefit of, nor may it be relied upon by, any other person or entity. The contents of this report may not be quoted in whole or in part or distributed to any person or entity other than the client.

2.0 BACKGROUND INFORMATION

2.1 Building Description

It is our understanding the original building at 30 Milling Road was constructed prior to 1929. Since its original construction, it appears multiple additions and renovations have occurred at the site at various periods throughout its history. The current structure appears to consist of four primary sections (*Refer to Figure 1 and Photos 1 & 2 in Appendix A*). The different sections of the building (S.1 - West, S.2, S.3, S.4 - East) utilize a combination of structural systems and materials. In general, the structure is a one story building with no basement level. The property is bound by Milling Road to the north and the Speed River to the south.



Figure 1: Plan view of 30 Milling Road various building sections

2.2 Document Review

The following document was available for our review:

- .1 Corporate Facilities Building Condition Assessment report for the service depot at 30 Milling Road prepared by WalterFedy dated June 18, 2018.

2.3 Interview with Building Maintenance Personnel

The following history was obtained during our site visit with Mac Robertson, with the City of Cambridge.

- .1 The existing building was originally constructed as a water pumping and electric lighting station by the City of Cambridge. Currently the building is partially occupied by a contractor tenant under Contract with the City.



- .2 The earliest known record of the building is a fire insurance claim dated in 1929, found on the Region of Waterloo Public Building Inventory.
- .3 The roofing systems were reportedly replaced in approximately 2010 (flat and shingled roofs). The roof metal flashings were reportedly replaced in approximately 2019 due to reported pest ingress.

3.0 OBSERVATIONS

RJC's review of the building was undertaken by means of a visual review from the interior and exterior from grade. The following observations were identified during our visual review.

- .1 Building Section S.1 generally appears to be constructed of a low-slope wood roof structure with wood joists spanning in the north-south direction supported by load-bearing perimeter masonry walls and intermediate built-up wood beams spanning in the east-west direction. The roof wood beams are supported by the east and west perimeter masonry walls and interior steel columns at third points. The steel columns extend below the interior concrete slab-on-grade. Three overhead doors are located along the north elevation of section S.1. The load bearing masonry walls are a combination of stone rubble walls at the south elevation, masonry block at the west elevation and brick masonry at the north elevation. The exposed foundation wall along the south elevation (adjacent the Speed River) appears to be of stone rubble construction (*Refer to Photos 3 to 5 in Appendix A*).
- .2 Where accessible, the building Section S.1 visible wood roof structure appears to be in good condition with no significant evidence of deterioration. Visual review of the roof structure in all areas of the building was limited due to the presence of ceiling finishes and insulation. Localized corrosion related deterioration was observed at the base of the four interior steel columns supporting the wood roof beams. The load bearing perimeter masonry walls appear to be of various ages and conditions. The south stone rubble walls appear to be in poor condition exhibiting widespread mortar loss and loose rubble stone. The west masonry block wall appears to be in fair condition with localized step cracking and evidence of previous mortar repairs. The north brick masonry appears to be in good to fair condition with localized evidence of step cracking and spalling brick (*Refer to Photos 6 to 8 in Appendix A*).
- .3 The construction of building Section S.2 appears to be similar to Section S.1 generally consisting of a low-slope wood roof structure with joists spanning in the north-south direction and wood beams below spanning east-west supported by intermediate steel columns and load bearing masonry walls. The exterior masonry walls appear to be a combination of rubble and brick masonry. The lower 10 feet of the north elevation masonry wall is constructed of stone rubble. Brick masonry infills the existing swing-door openings and extends the rubble wall vertically approximately 8 feet. The south foundation and above-grade wall appears to be of stone rubble construction (*Refer to Photo 9 in Appendix A*).
- .4 The building Section S.2 wood roof, columns and interior concrete slab-on-grade elements



appear to be in good condition given their ages, with no significant evidence of structural deterioration observed. The brick masonry walls appear to be in fair to poor condition with widespread evidence of mortar loss, step cracking and brick spalling. The exposed stone rubble masonry foundation and above grade walls appear to be in poor condition with widespread evidence of mortar loss and loose rubble stones (*Refer to Photo 10 in Appendix A*).

- .5 Building Section S.3 appears to be of wood and brick masonry construction. The low-sloped roof is constructed of wood planks spanning in the north-south direction supported by wood joists spanning in the east-west direction. The wood joists bear in pockets in the east and west perimeter brick masonry walls. Section S.3 includes an attic storage space similarly framed to the roof above with a wood plank floor spanning north-south and joists spanning in the east-west direction. The ground floor appears to be a concrete slab-on-grade. The exposed sections of the perimeter masonry walls appear to be double-wythe brick. At the roof level, the north and south corners of the east and west perimeter walls step out for a height of approximately 6 feet forming architectural corbels (*Refer to Photos 11 and 12 in Appendix A*).
- .6 The Section S.3 roof structure appears to be in fair condition with localized evidence of previous water ingress in the form of water staining on the wood joists and ceiling finishes below. The concrete slab-on-grade appears to be in good condition. The brick masonry generally appears to be in poor condition with widespread evidence of spalling brick, mortar loss and step cracking. In addition, the northeast corner of the perimeter masonry walls at grade has a significant area of missing brick. Further, the four masonry corbels are in poor condition exhibiting mortar loss, step cracking and appear to present a potential fall hazard to the public accessible areas below (*Refer to Photos 13 and 14 in Appendix A*).
- .7 Building Section S.4 appears to be the newest addition to the structure. The gable roof is constructed of wood planks spanning east-west with roof trusses below spanning in the north-south direction. The trusses bear on the brick masonry perimeter walls. A brick masonry chimney is constructed at the centre of the west elevation, exterior of the perimeter wall. Two window openings are present on the south building elevation. The exposed section of foundation at the north and south sides of the structure appear to be of cast-in-place concrete construction. The interior slab appears to be a concrete slab-on-grade (*Refer to Photo 15 in Appendix A*).
- .8 The S.4 roof structure appears to be in good condition given its age. The brick masonry chimney was observed to be in very poor condition above the roof line. With the exception of the chimney, the remaining brick masonry appears to be in fair condition with localized evidence of brick spalling, mortar loss and step cracking. The two window sills on the south elevation were noted to be in poor condition. The exposed cast-in-place concrete foundation walls appear to be in fair condition given their age (*Refer to Photos 16 and 17 in Appendix A*).



4.0 CONCLUSIONS/DISCUSSION

Based on the findings of this report, the areas of the structure at 30 Milling Road appear to be in fair to poor condition. The primary concern with regard to the condition of the building structure is related to the perimeter load bearing masonry walls and foundation walls. Repairs are required in the immediate term to address current structural integrity and potential fall hazard concerns. A larger rehabilitation project is required in the short term to address the remaining masonry and structural deterioration present throughout the structure. Based on the findings of this review, the deterioration is expected to progress at an accelerated rate in the current deterioration is not addressed.

Based on the information obtained from our survey, we provide the following discussion and conclusions:

1. **The area of missing brick near grade at the northeast corner of building section S.3 presents a significant concern for the structural integrity of the adjacent load bearing masonry walls. Immediate masonry repairs are required to address the area of missing brick and reinstate the integrity of the wall structure.**
2. **The Section S.3 roof level brick masonry corbels are in poor condition and currently present a potential fall hazard to the public below. In addition, the cantilever of the corbels appears to exceed the requirements of the current masonry design standards.** Reconstruction of the existing masonry corbels in the immediate term is required address the present fall hazards. Consideration to reconstruct the corbels plumb with the masonry wall below may be required to bring the structure into conformance with current design standards. Prior to implementing this change, it is recommended the stakeholders of the building review the impact with the heritage committee having jurisdiction to determine if changes are permitted.
3. **The brick masonry chimney in building Section S.4 is in poor condition. Reconstruction of the chimney is required in the immediate term to address the current fall hazard presented by the chimney.** As an alternative course of action, the areas below the brick chimney and corbels may be blocked off from public access until such time the recommended repairs can be implemented.
4. **The stone rubble foundation and above-grade walls along the south elevation are exhibiting evidence of widespread mortar loss and loose stone rubble.** The location of the wall adjacent the Speed River poses significant challenges for access during rehabilitation. Wholesale masonry repointing is required to reinstate the existing capacity of the stone rubble walls.
5. **The remaining brick masonry is in fair to poor condition with widespread evidence of spalling brick, mortar loss and step cracking.** Localized repair of the brick masonry is required in the short term to address the current areas of deterioration.



6. **Localized reinforcement of the corroded interior steel columns in building Section S.1 is required to address the current levels of corrosion.** In addition, it is recommended a protective coating is installed on the surface of the columns to prevent further corrosion related deterioration.
7. **The window sills on the south elevation of building Section S.4 are in poor condition and require replacement.**
8. **While no significant deterioration was observed related to the wood roof structure, due to the presence of previous water staining it is recommended an allowance is carried during future roof replacement projects include for localized repair of deteriorated areas of the wood roof deck and/or wood joists.**



5.0 RECOMMENDED COURSES OF ACTION

We recommend the following repairs be performed to rehabilitate the current deterioration observed to date. Immediate, short and long term repair recommendations are presented below:

5.1 Immediate Term (0-1 Years) – Immediate Masonry Repairs

In general, we recommend the following repairs be performed in the immediate term (0 – 1 years) to address the immediate structural and fall hazard concerns observed at the time of our review.

- Reconstruction of the deteriorated masonry at the northeast corner of Section S.3
- Reconstruction of the building Section S.3 roof masonry corbels flush with the masonry walls below
- Reconstruction of the brick masonry chimney

This repair option will address the immediate deterioration concerns noted during our review and reinstate the original load carrying capacity of the structure. We recommend that these repairs be completed to prevent further deterioration and address current safety concerns.

5.2 Short Term (1-2 Years) – Masonry Repairs

In general, we recommend the following repairs be performed in the short term (1 – 2 years) to address the observed deterioration and to protect the structure against further deterioration in the long term.

- Wholesale repointing of the exposed stone rubble foundation and above-grade masonry walls
- Localized brick masonry replacement at deteriorated and/or spalled bricks
- Localized brick masonry repointing at deteriorated mortar joints and locations of step cracking
- Localized reinforcing of the interior steel columns and installation of a protective coating
- Replacement of the deteriorated window sills at the south elevation of building section S.4

This repair option will address the remainder of the deterioration noted during our review. We recommend that these repairs be implemented over the next 1 to 2 years. Delays in undertaking the required repairs will result in on-going deterioration of the identified items at an accelerated rate, greater repair costs and potential further loss of the load carrying capacity of the structure.



5.3 Long Term (5-10 Years) – Roof Structure Repair Allowance

In general, we recommend the following repair allowance be carried during any future roof replacement project to address possible deterioration of the existing roof structure.

- Localized replacement of deteriorated areas of the existing wood roof deck
- Localized sistering and/or replacement of deteriorated roof joists/trusses



6.0 OPINION OF PROBABLE CONSTRUCTION COSTS

The following costs for the repair of the structure at 30 Milling Road represent our opinion of the probable construction costs and are based on the information obtained during this condition survey assessment. The final costs will not be known until such time that the work is tendered and completed and the final quantities of required concrete repairs are known. It is not possible to accurately forecast the final bid unit-prices that may be tendered for the work because they are directly related to the construction climate at the time of tendering. The following cost estimates should be treated as "ball park" or Class "C" figures only and cannot be guaranteed accurate (*Class "C" probable costs – a statement of general requirements and an outline of a solution (degree of accuracy +/- 25%)*).

Probable Construction Costs are in 2021 dollars and do not include soft costs or H.S.T. unless noted otherwise. Soft Costs include engineering fees (including disbursements), material testing, and permits. Typically these costs are in the range of 8 – 10% of the Probable Construction Cost.

It should also be noted that the probable construction costs do not include any direct or indirect costs associated with the global COVID-19 pandemic, as the impact the pandemic will have on the costs for this project cannot be accurately forecasted.

Based on the construction review experience we have had in the field of structural repair and rehabilitation, we advise that it is reasonable to assume that the repair quantities - as compared to those deteriorated quantities observed during the condition survey - will be larger. Different items for repair characteristically have exhibited different increases in size during the repair program. Our following summaries which outline the probable construction costs, has considered this increase from the observed deteriorated quantities.

Consideration to complete various repairs outlined below into a fewer number of projects is recommended to take advantage of efficiencies and economies of scale.

6.1 Immediate Term (0-1 Years) – Immediate Masonry Repairs

The probable construction cost for the repairs described in Chapter 5.1 of this report excluding H.S.T., engineering fees, material testing costs and assuming all the work is undertaken in one program in 2021 dollars is as follows:

.1	Immediate Masonry Repairs	\$80,000
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6.2 Short Term (1-2 Years) – Masonry Repairs

The probable construction cost for the repairs described in Chapter 5.2 of this report excluding H.S.T., engineering fees, material testing costs and assuming all the work is undertaken in one program in 2021 dollars is as follows:

.1	Masonry Repairs	\$500,000
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6.3 Long Term (5-10 Years) – Roof Structure Repair Allowance

The probable construction cost for the repairs described in Chapter 5.3 of this report excluding H.S.T., engineering fees, material testing costs and assuming all the work is undertaken in one program in 2021 dollars is as follows:

.1	Roof Structure Repair Allowance	\$20,000
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7.0 CLOSING COMMENTS

Thank you for selecting Read Jones Christoffersen Ltd. for this project. RJC would be pleased to assist you with the implementation of our recommendations. Should you have any questions or concerns, please do not hesitate to contact this office.

Yours truly,

READ JONES CHRISTOFFERSEN LTD.

A handwritten signature in black ink, appearing to read 'Kyle Brown'.

Kyle Brown, BEng, EIT
Engineering Intern
Building Science and Restoration

Reviewed by:

A handwritten signature in blue ink, appearing to read 'R. Dougherty'.

Ryan Dougherty, BEng, EIT
Engineering Intern
Building Science and Restoration

Reviewed by:

A handwritten signature in black ink, appearing to read 'T. Van Zwol'.

Tim Van Zwol, M.S.c, P.Eng
Regional Manager/Associate
Building Science and Restoration

Encl.: Appendix A – Photographs



Appendix A

Photographs

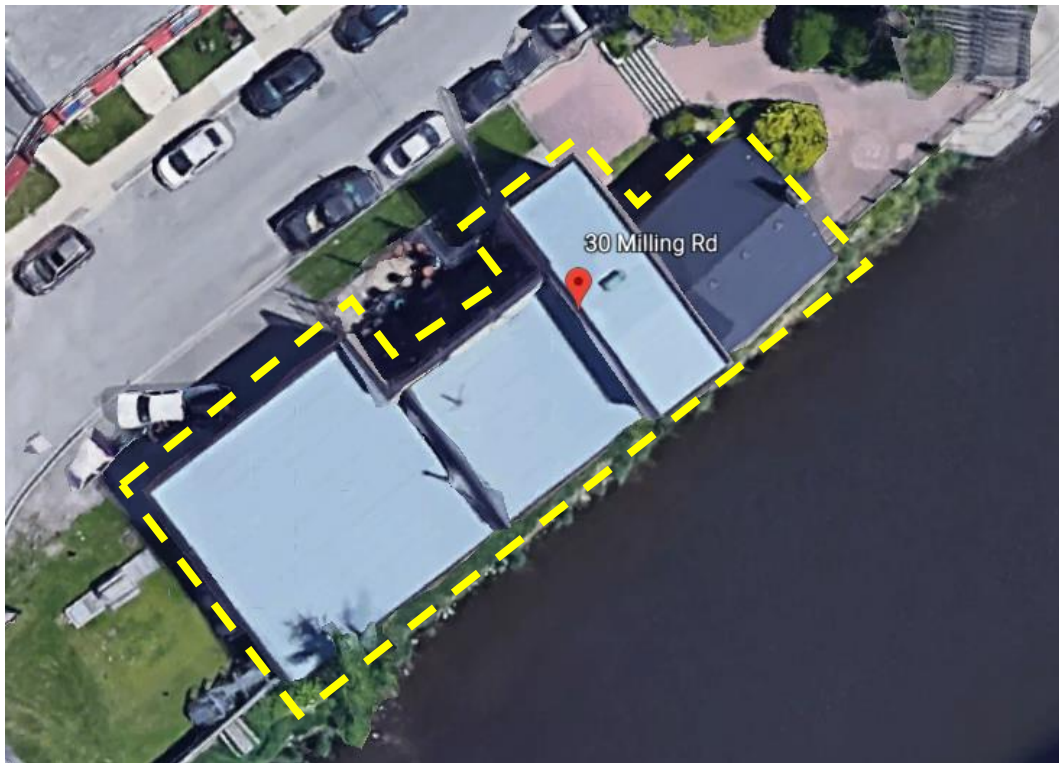


Photo 1 – Overview of the building located at 30 Milling Road, Cambridge Ontario.

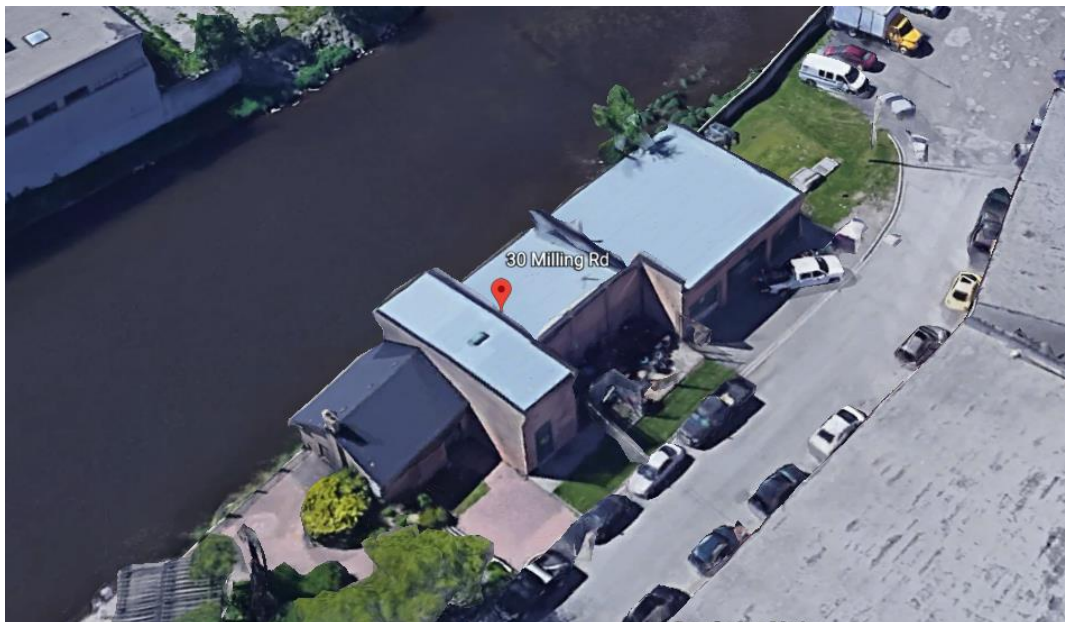


Photo 2 – Overview of the building at 30 Milling Road, Cambridge, Ontario.



Photo 3 – View of typical interior steel column located in Section S.1 of the building.



Photo 4 – View of the north elevation of Section S.1



Photo 5 – Overview of the south elevation of 30 Milling Road



Photo 6 – View of typical localized steel corrosion at base of interior support column located in Section S.1.



Photo 7 – View of typical stone rubble masonry deterioration



Photo 8 – View of the typical condition of the interior concrete slab-on-grade



Photo 9 – View of the north elevation of Section S.2



Photo 10 – View of typical localized spalling of the perimeter brick masonry walls.



Photo 11 – View of the masonry corbels at the north elevation of Section S.3



Photo 12 –View of the wood roof structure of building Section S.3



Photo 13 – View of the brick masonry deterioration at grade at the northeast corner of building Section S.3



Photo 14 – View of brick masonry wall and corbel deterioration at the north elevation of building Section S.3



Photo 15 –Overview of building Section S.4



Photo 16 – View of brick masonry spalling at the chimney on the east elevation of building Section S.4



Photo 17 – View of the window sill deterioration on the south elevation of building Section S.4



Corporate Enterprise Department

Sustainable Design & Development Division

Project: 30 Milling Street Building

COST ESTIMATES 'D'

The existing building, which was built in 1929, is in very poor condition. Originally it was a water & electrical lighting station.

Foundations, all bearing walls, steel columns, brick and masonry walls are deteriorated and some walls present a hazard to pedestrians. The east end of the building is unsafe and closed for any usage.

Our intention is to provide a realistic cost estimate of the construction costs based on the current condition of the building. Cost estimates in this report are preliminary class 'D' estimates with a degree of accuracy of +/- 25%. These cost estimates are based on:

- the Building Condition Assessment done by WalterFedy in 2018
- the structural engineer report done by Read Jones Christofferson Engineers in 2021
- visual examination of the current condition of the building
- current costs of similar work
- costing data from industry standard reference 'Hanscomb's Yardstick for Costing'
- lump sum allowances
- one-time construction work, not in phases

Cost estimates for 30 Milling Street renovation include:

- structural work to provide safe structure of the building
- building envelope renovation
- 2 new accessible WC
- Accessible entrances to the building
- New ceiling
- Interior renovation to current building & fire codes

- Repair existing concrete floor
- Interior doors' replacement
- Repair and paint inside walls
- Heating, ventilation & air condition
- Lighting
- Exit signage
- Building permit & testing
- Interior demolition
- Replacement of end of life equipment
- health & safety improvements
- security system
- fire alarm
- communication/data
- construction contingencies
- consulting fees
- building permit & testing contingencies
- design & pricing contingencies
- escalation contingencies
- general requirements
- contractor's overhead & profit

Contingencies are included to cover the accuracy risk. Since there is no design, we made assumptions based on our experience with similar projects. Design & pricing contingencies of 15% are included for design items that are not known at this time. With more detailed design these contingencies will be smaller. Escalation contingencies or changes in market conditions are for increases in construction costs between the time of estimate and time of construction. Construction contingencies of 10% covers unknown work during construction.

Exclusions from cost estimates:

- non-refundable HST
- remediation of designated substances
- furniture, furnishing & equipment
- impact of COVID or another pandemic
- after regular work hours
- exterior site work
- new transformer
- new water supply pipes outside of the building
- GRCA requirements

OPTION 1: BUILDING RENOVATION

Description	Cost Estimate	Sub-total	Total
<u>A. Foundation</u>		\$289,000	
- Repair damaged masonry foundation	\$289,000		
<u>B. Shell</u>		\$557,000	
-Structural walls	\$289,000		
-Restoration of deteriorated masonry & repointing	\$150,000		
-Windows	\$46,000		
-Exterior man doors & automatic door openers	\$21,000		
-Overhead doors	\$21,000		
-Localizes replacement of deteriorated wood roof deck, joists & trusses	\$30,000		
<u>C. Interiors</u>		\$448,000	
- Doors	\$7,000		
- Reinforcing steel columns & apply protective coating	\$25,000		
- Repair and paint wall finishes	\$189,000		
- New tiles	\$12,000		
- Repair and stain concrete floor	\$15,000		
- Ceiling including replacement of damaged insulation and vapor barrier	\$185,000		
- Partial demolition & cleaning	\$15,000		
<u>D. Mechanical</u>		\$151,400	
- plumbing fixtures	\$17,900		
- domestic water pipes	\$8,000		
- sanitary waste & vents	\$9,000		
- domestic water heater	\$5,000		
- heating, ventilation & air conditioning units with ductwork	\$110,000		
- fire extinguishers	\$1,500		
<u>E. Electrical</u>		\$75,000	
- lighting equipment with wiring	\$45,000		
- additional outlets with wiring	\$10,000		
- exit signage	\$5,000		
- fire alarm system	\$15,000		
<u>F. Building permit & testing contingency</u>	\$12,000	\$12,000	
<u>G. Subtotal Construction Cost</u>		\$1,532,400	
<u>H. General Contractor Requirements 10%</u>	\$153,200	\$153,200	

<u>I. Contractor's Overhead & Profit 5%</u>	\$76,600	\$76,600	
<u>J. Design & Pricing Contingency 15%</u>	\$229,800	\$229,800	
<u>K. Subtotal</u>		\$1,992,000	
<u>L. Escalation Contingency 4%</u>	\$79,700		
<u>M. Subtotal</u>		\$2,071,700	
<u>N. Construction Contingency 10%</u>	\$207,200	\$207,200	
<u>O. Total Estimated Hard Construction Cost</u>			\$2,278,900
<u>P. Consulting Fee 10%</u>	\$227,900	\$227,900	
<u>Q. TOTAL PROJECT COST</u>			<u>\$2,506,800</u>

OPTION 2: BUILDING DECOMMISSIONING

Cost estimates for building decommissioning include:

- Demolition the building down to grade
- Excavate & remove the concrete slabs on grade
- Excavate & remove 4 foot frost wall depth foundation walls & standard footings, other than back foundation wall and portions of side foundation walls 45 degree slope to back wall
- Cap sanitary & water lines
- general requirements
- contractor's overhead & profit
- construction contingencies
- consulting fees
- building permit

Exclusions from cost estimates:

- non-refundable HST
- remediation of designated substances
- GRCA requirements
- Imported granular and or topsoil/seeding if required

OPTION 2: BUILDING DECOMMISSIONING

Description	Cost Estimate	Sub-total	Total
<u>A. Decommissioning</u>		\$68,800	
- demolition	\$53,500		
- general requirements 10%	\$5,300		
- contractor's overhead & profit 5%	\$2,700		
- construction contingency 10%	\$5,300		
- building permit	\$2,000		
<i>B. Total Estimated Hard Construction Costs</i>			\$68,800
<i>C. Consulting Fee 10%</i>	\$6,900	\$6,900	
<u>D. TOTAL PROJECT COSTS</u>			<u>\$75,700</u>